

Detailed Site Plan

# Revised Design Standards

## Konterra Town Center East Prince George's County, MD



April 2010

FORESTCITY  
WASHINGTON  
KONTERRA DEVELOPMENT



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# Konterra Town Center East - Design Standards

Approval Sheet:



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# THE PRINCE GEORGE'S COUNTY GOVERNMENT

Office of the Clerk of the Council  
(301) 952-3600

July 29, 2009

RE: DSP 08011 Konterra Town Center East  
1325 G Street Associates LLP, Applicant

## NOTICE OF FINAL DECISION OF THE DISTRICT COUNCIL

Pursuant to the provisions of Section 27-134 of the Zoning Ordinance of Prince George's County, Maryland requiring notice of decision of the District Council, you will find enclosed herewith a copy of the Council Order setting forth the action taken by the District Council in this case on July 21, 2009.

## CERTIFICATE OF SERVICE

This is to certify that on July 29, 2009 this notice and attached Council Order were mailed, postage prepaid, to all persons of record.

Redis C. Floyd  
Clerk of the Council

(10/97)

County Administration Building – Upper Marlboro, Maryland 20772

Case No. SP-08011

Applicant: 1325 G Street Associates, LLP

## COUNTY COUNCIL OF PRINCE GEORGE'S COUNTY, MARYLAND, SITTING AS THE DISTRICT COUNCIL

## ORDER AFFIRMING PLANNING BOARD DECISION, WITH CONDITIONS

IT IS HEREBY ORDERED, after review of the administrative record, that the decision of the Planning Board in PGCPB No. 09-33, to approve with conditions a special purpose detailed site plan for the development character for the 132-acre downtown core area of Konterra Town Center East, and for the supporting infrastructure such as stormwater management ponds in the surrounding environs, for a project referred to as Konterra Town Center East, on property described as approximately 402.5 acres of land in the M-X-T Zone, with the larger Konterra Town Center site located on the east side of Interstate 95, south of Van Dusen Road, north of Muirkirk Road, west of Virginia Manor Road, and the core area surrounded by Perimeter Drive East, Perimeter Drive West, South End Road, and North End Road, Laurel, is:

AFFIRMED, for the reasons stated by the Planning Board, whose decision is hereby adopted as the findings of fact and conclusions of law of the District Council in this case, with the following changes:

A. The following language on page 3 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

[A comprehensive set of design standards (a complete design standards book can be found at [www.Konterra.com](http://www.Konterra.com)) illustrated with photos and graphics has been provided with this DSP. The standards are prepared based on the physical layout of the downtown core area as envisioned in the previously approved conceptual site plan, and are related specifically to each developable block of the DSP. The illustrations provided with the



design standards show the character and quality of the proposed development in the downtown core area. The standards have been divided into seven chapters and cover almost every aspect of a built town center environment, from the building design, streetscape, materials, lighting pattern and fixtures, signage, and landscaping to pavement patterns. The standards are in essence the codification of the physical development envisioned by the Conceptual Site Plan, CSP-07003, which will be further defined by this DSP. The standards have been proposed with the intent that they will be utilized as criteria for future permit plan review. The approved design standards will be used by Planning Department staff to review each specific project as the designee of the Planning Board. This arrangement, whereby each proposed permit plan must conform to the design parameters approved in this DSP, will provide the developer with enough flexibility to accommodate the changing market. The design standards are intended to provide a minimum quality assurance for the proposed downtown core development of Konterra Town Center East. The final construction of the downtown core area must be equal to or better than the quality as presented by the proposed standards in this DSP. Konterra Town Center East will have an additional private covenant/leasing agreement, imposed by the owners, to achieve a high-quality development and efficient operation of the downtown core.]

Add:

A comprehensive set of design standards (a complete design standards book can be found at [www.Konterra.com](http://www.Konterra.com)), illustrated with photos and graphics, has been filed for this DSP. The standards are based on the physical layout of the downtown core area as envisioned in the previously approved conceptual site plan, and are related specifically to each developable block of this special purpose DSP. The illustrations with the design standards show the character and quality of the development in the downtown core area that the applicant has committed to. The standards have been divided into seven chapters: they cover almost every aspect of a built town center environment, from the building design, streetscape, materials, lighting pattern and fixtures, signage, and landscaping to pavement patterns. The applicant proposes these standards as the codification of the physical development envisioned by the Conceptual Site Plan, CSP-07003, which is further defined in this special purpose DSP. The standards will be the criteria for future permit plan review, under this special purpose DSP. Under this applicant's proposal, the approved design standards are to be used by Planning Department staff to review each specific project, as the designee of the Planning Board. This arrangement, whereby each proposed permit plan must conform to the design parameters approved in this special purpose DSP, will provide the developer with enough flexibility to accommodate the changing market. The design standards provide a minimum quality level for the proposed downtown core development of Konterra Town Center East. The final downtown core area must equal or exceed the quality as presented by the proposed standards in this special purpose DSP. Konterra Town Center East

will have an additional private covenant/leasing agreement, imposed by the owners, to achieve a high-quality development and efficient operation of the downtown core. Such additional private covenant/leasing agreement does not affect the findings and conditions of CSP-07003 nor this special purpose DSP.

B. The following language on page 4 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

[The DSP also includes a summary of the approaches and strategies that will be applied to achieve sustainability in the development of the larger Konterra Town Center East. The summary reiterates the applicant's commitment to sustainable urban development. The approaches and strategies to be employed are as follows: to use an integrated design process; to establish compact, complete, and connected neighborhoods; to protect local watersheds and respect wildlife habitat; to minimize carbon dioxide emissions; and to ensure public health and safety in the design and development of Konterra Town Center East.

DSP-08011 allows development solely within the 132-acre downtown core area and allows supportive infrastructure, such as roads and erosion and sediment control ponds, which will be converted to stormwater management ponds after construction, on the remainder of the property shown on the DSP. Any development outside the 132-acre downtown core area shall require either an approval of a revision to DSP-08011 or approval of a separate DSP for that area.]

Add:

This special purpose DSP also includes a summary of the approaches and strategies that will be applied to achieve sustainability in the development of the larger Konterra Town Center East. The summary reiterates the applicant's commitment to sustainable urban development. The approaches and strategies to be employed are as follows: to use an integrated design process; to establish compact, complete, and connected neighborhoods; to protect local watersheds and respect wildlife habitat; to minimize carbon dioxide emissions; and to ensure public health and safety in the design and development of Konterra Town Center East. The sustainability doesn't however, fully address issues of transit and Traffic Demand Management (TDM). The 1990 Subregion I Master Plan reviews these issues at length, in recognition of the long-term transportation inadequacies of the Subregion I area, and proposes the implementation of TDM measures. The Planning Board has not addressed this issue, either, and the applicant must address sustainability. The revised Subregion I Master Plan should also address the issue, particularly with respect to Konterra.



DSP-08011 allows development solely within the 132-acre downtown core area, and allows supportive infrastructure, such as roads and erosion and sediment control ponds, that will be converted to stormwater management ponds after construction, on the remainder of the property shown on the DSP. Any development outside the 132-acre downtown core area shall require approval of a separate DSP for that area utilizing the standard DSP approval process in Part 3, Division 9 of the Zoning Ordinance. The extraordinary use of this special purpose DSP approval process shall not be used outside the 132-acre downtown. Therefore, additional land shall not be incorporated by means of a revision to special purpose DSP-08011.

C. The following language on pages 6 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

[Specifically, this DSP includes 2,161 multifamily dwelling units, a 300-room hotel, approximately 2.2 million square feet of various uses, and is in a town center layout that implements many New Urbanism principles and best urban design practices to promote the orderly development of the subject site. This DSP is a further refinement of the land use vision of the downtown core area as approved in the Conceptual Site Plan, CSP-07003. Given its scale, quality of the development, and wide range of uses, Konterra Town Center East will enhance the economic status of the County and provide desirable employment and living opportunities for citizens of the county.]

Add:

Specifically, this DSP includes 2,161 multifamily dwelling units, a 300-room hotel, approximately 2.2 million square feet of various uses, and is in a town center layout that implements New Urbanism principles and best urban design practices, to promote the orderly development of the subject site. This special purpose DSP serves as a refinement of the land use vision of the downtown core area as approved in the Conceptual Site Plan, CSP-07003. Given its scale, development quality, and wide range of uses, Konterra Town Center East has the potential to enhance the economic status of the County and provide desirable employment and living opportunities for citizens.

D. The following language on pages 6-7 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

**[(4) To promote the effective and optimum use of transit and other major transportation systems;**

The proposed downtown core area is located within the geographical center of Konterra Town Center East, which is located between the metropolitan areas of Baltimore and Washington, D.C. and is accessible to major regional highways and the Maryland Area Regional Commuter (MARC) commuter train system. The proposed downtown core area is located close to the intersection of the Intercounty Connector and I-95. Because of the nature of the development, various forms of transportation including pedestrian, bicycle, automobiles, and mass transit are necessary to support the planned downtown core. This area is designed in an urban grid street pattern to maximize accessibility. Sidewalks, bike lanes, and trails have been proposed with the approved CSP for the entire Konterra Town Center East and within this DSP for the downtown core area, which are consistent with the approved CSP. Additional transportation improvements are envisioned in the later stages of the development. These improvements may include bus connections, transit stops from the MARC station at Muirkirk Road and US 1, and a potential Metro station within the office complex in the environs of Konterra Town Center East between the downtown core area and I-95. The success of the proposed development hinges on the effective use of various major transportation systems.]

Add:

**(4) To promote the effective and optimum use of transit and other major transportation systems;**

The proposed downtown core area is located within the geographical center of Konterra Town Center East, which is located between the metropolitan areas of Baltimore and Washington, D.C. and is accessible to major regional highways and the Maryland Area Regional Commuter (MARC) commuter train system. The proposed downtown core area is located close to the intersection of the Intercounty Connector and I-95. Because of the nature of the development, various forms of transportation including pedestrian, bicycle, automobile, and mass transit are necessary to support the planned downtown core. This area is designed in an urban grid street pattern to maximize accessibility. Sidewalks, bike lanes, and trails have been proposed with the approved CSP for the entire Konterra Town Center East and within this DSP for the downtown core area,



SP-08011

SP-08011

which are consistent with the approved CSP. The Planning Board Resolution envisions additional transportation improvements in the later stages of the development. These improvements may include bus connections, transit stops from the MARC station at Muirkirk Road and US 1, and a potential Metro station within the office complex in the environs of Konterra Town Center East between the downtown core area and I-95. However, the Planning Board Resolution does not fully evaluate these issues even though they are identified as a significant concern in the 1990 Subregion I Master Plan. Further, as noted above, the Planning Board has not taken into account the need for TDM measures. The Planning Board has not determined how these issues are addressed in the Master Plan; its analysis is narrowly focused on the APF issue for the preliminary plan of subdivision, and it is adequate. As noted above, the applicant needs to address transit and TDM measures, and the revised Subregion I Master Plan should also address these issues with respect to Konterra.

E. The following language on pages 8-9 the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

**[(9) To permit a flexible response to the market; and**

The scope of the project and its multiple uses will allow Konterra the flexibility to be responsive to market forces in a variety of sectors. In addition, through approval of detailed design standards as proposed in this DSP, parameters for the overall character of the downtown core area are assured. The Planning Board believes, pursuant to the approved design standards, that permit plans can be reviewed administratively according to the approved design standards if those permit plans meet certain criteria as defined by this DSP. Konterra represents a well-integrated set of complementary uses, which together create a unified sense of place. Phasing development in a realistic and orderly way that establishes the design quality and character of the town center at its beginning will provide a framework that will build market demand and credibility necessary to fulfill the town center's ultimate vision. Approval of this development character in the DSP with its detailed design standards will provide appropriate flexibility to respond to the changing needs of public and private market sectors, while establishing an attractive, harmonious physical framework for development. The mixed-use and multiphase development with diverse products, as shown in the previously approved conceptual site plan for Konterra, and the detailed design standards in this DSP will permit a flexible response to the market. This DSP is in conformance with this purpose of the M-X-T Zone.

**(10) To allow freedom of architectural design in order to provide an opportunity and incentive to the developer to achieve excellence in physical, social, and economic planning.**

Detailed design standards in this DSP are intended to provide a framework for future administrative review of each permit plan, and are focused primarily on the urban character and the form of the public realms. The design standards specify detailed architectural design elements such as building height, solar orientation, fenestration, finishing materials, canopies, awnings, and building projections, but the standards do not lock in specific architectural styles. This design-oriented approach will provide an opportunity and incentive to allow freedom of architectural design that will create a unique character for Konterra Town Center East and greatly contribute toward the excellence envisioned in the 2002 approved General Plan for centers.]

Add:

**(9) To permit a flexible response to the market; and**

The scope of the project and its multiple uses will allow Konterra the flexibility to be responsive to market forces in a variety of sectors. In addition, through approval of detailed design standards as proposed in this special purpose DSP, parameters for the overall character of the downtown core area may be assured. The Planning Board believes, pursuant to the approved design standards, that it can allow for the use of an extraordinary process based on permit plans to be reviewed administratively according to the approved design standards if those permit plans meet certain criteria as defined by this special purpose DSP. Konterra presents the opportunity for a well-integrated set of complementary uses, which together may create a unified sense of place if the design guidelines are adhered to. Phasing development in a realistic and orderly way that establishes the design quality and character of the town center at its beginning will provide a framework that will build market demand and credibility necessary to fulfill the town center's ultimate vision. Approval of this development character in this special purpose DSP with its design standards will provide appropriate flexibility to respond to the changing needs of public and private market sectors, while establishing an attractive, harmonious physical framework for development. The mixed-use and multiphase development with diverse products, as shown in the previously approved conceptual site plan for Konterra, and the detailed design standards in this special purpose DSP will permit a flexible response to the market.



**(10) To allow freedom of architectural design in order to provide an opportunity and incentive to the developer to achieve excellence in physical, social, and economic planning.**

Detailed design standards in this special purpose DSP are intended to provide a framework for future administrative review of each permit plan (in lieu of a detailed site plan) and are focused primarily on the urban character and the form of the public realms. The design standards specify detailed architectural design elements such as building height, solar orientation, fenestration, finishing materials, canopies, awnings, and building projections, but the standards do not lock in specific architectural styles. This design-oriented approach will provide an opportunity and incentive to allow freedom of architectural design that will create a unique character for Konterra Town Center East. It is important that the architectural design emphasize traditional regional materials such as brick, stone and masonry as evidenced in most District Council decisions and that such materials and architectural design be of the highest quality. This Decision and Order does not specify any particular percentages required for particular materials; however, it is expected that the applicant and the Planning Department Staff and the Planning Board are aware of the need to emphasize such materials and obtain the highest quality design possible.

F. The following language on page 10-11 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

[ . . . The information provided on design principles and standards, proposed architectural elements, quality and mix of materials demonstrated in the DSP design standards, and the illustrative plan and renderings all ensure sustainable quality and reflect a cohesive development capable of sustaining an independent environment of continuing quality and stability. . . . ]

Add:

. . . . The information provided on design principles and standards, proposed architectural elements, quality and mix of materials demonstrated in the DSP design standards, and the illustrative plan and renderings provide the capability for sustainable quality and reflect a cohesive development capable of sustaining an independent environment of continuing quality and stability. Adherence to the design guidelines and the principles and text of the Subregion I Master Plan (including revisions) will be key elements in attaining the sustainable quality and stability. . . .

G. The following language on page 12 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

**[(10) On the Detailed Site Plan, if more than six (6) years have elapsed since a finding of adequacy was made at the time of rezoning through a Zoning Map Amendment, Conceptual Site Plan approval, or preliminary plat approval, whichever occurred last, the development will be adequately served within a reasonable period of time with existing or programmed public facilities shown in the adopted County Capital Improvement Program, within the current State Consolidated Transportation Program, or to be provided by the applicant.**

Conceptual Site Plan CSP-07003 and Preliminary Plan of Subdivision 4-07108 were approved by the Planning Board in 2008. Adequate public facility issues were evaluated in the course of reviewing both applications and were determined to be adequate by the Planning Board. The development contained in this DSP will be adequately served within a reasonable period of time with existing or programmed public facilities shown in the adopted County Capital Improvement Program, within the current State Consolidated Transportation Program, or to be provided by the applicant.]

Add:

**(10) On the Detailed Site Plan, if more than six (6) years have elapsed since a finding of adequacy was made at the time of rezoning through a Zoning Map Amendment, Conceptual Site Plan approval, or preliminary plat approval, whichever occurred last, the development will be adequately served within a reasonable period of time with existing or programmed public facilities shown in the adopted County Capital Improvement Program, within the current State Consolidated Transportation Program, or to be provided by the applicant.**

Conceptual Site Plan CSP-07003 and Preliminary Plan of Subdivision 4-07108 were approved by the Planning Board in 2008. Adequate public facility issues were evaluated in the course of reviewing both applications and were determined to be adequate by the Planning Board, although it failed to take into account and address concerns identified in the Subregion I Master Plan. The Planning Board continues to rely on its outdated guidelines that it has failed to update (and which are not well-suited for Conceptual Site Plan analysis), and it has ignored the need for more transit options and a plan for Transportation Demand Management for a



dense, urban project such as Konterra Town Center East. The need to address these issues are detailed in the Subregion I Master Plan, and should be addressed in the Sustainability analysis. A condition will be added to address these issues.

H. The following language on page 17 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

**[(i) The maximum height of multifamily buildings shall be one hundred and ten (110) feet. This height restriction shall not apply within any Transit District Overlay Zone, or a Mixed-Use Planned Community.**

The bulk regulations contained in the DSP do not show any building that is higher than 110 feet. Future permit review will also make sure that this requirement will be met.]

Add:

**(i) The maximum height of multifamily buildings shall be one hundred and ten (110) feet. This height restriction shall not apply within any Transit District Overlay Zone, or a Mixed-Use Planned Community.**

The bulk regulations contained in the DSP do not show any building that is higher than 110 feet. There is an expectation that in the future there may be buildings that exceed this height limit, and in many areas of Konterra Town Center East such taller buildings could be appropriate. For example, in the center of the core area or along I-95, buildings that exceed the height limit may be appropriate and desirable. This issue should be addressed in the revisions to the Subregion I Master Plan or in revisions to the Zoning Ordinance. Future permit review will address this requirement and ensure that building heights are kept within applicable limits.

I. The following language on page 20 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

[The subject DSP has been filed to fulfill this requirement. The DSP consists of site plans and design standards. The detailed site plan divides the downtown core area into 16 developable blocks and one open space block and shows the building envelope of each block with the possible uses. The design standards have been prepared in accordance with the above requirements to codify the development as

envisioned in the CSP. The standards are closely related to the specific physical layout of the downtown core and cover all aspects of a built town center environment as stated in this requirement. The design standards are also illustrated with graphics and photographs that provide information to define the development character and quality of the proposed downtown core area.

The subject DSP is similar in many ways to the Planning Board’s previous approvals of detailed site plans for community character, special purpose detailed site plans for signage, and “umbrella” architectural approvals. However, this DSP differs greatly from previous approvals in its scale and complexity. The approval of this DSP will establish parameters, design standards, and site plans for reviewing permit plans. Functionally, this will work by having applicants provide staff with interim permit plans during its permit design process, thus allowing staff to provide input on conformity to the design standards. Variation outside of the percentage guide must be shown to conform to the intent of the design standards or would be subject to review by the Planning Board. The Planning Board has also defined some additional instances which would require Planning Board review and are included in this resolution. The design- and character-driven standards and approval process will not only provide assurance that the desired development quality will be achieved, but also allows the applicant flexibility to respond to the market in a timely fashion.]

Add:

The subject special purpose DSP has been filed to fulfill this requirement. This special purpose DSP consists of site plans and design standards. The detailed site plan divides the downtown core area into 16 developable blocks and one open space block and shows the building envelope of each block with the possible uses. The design standards have been prepared in accordance with the above requirements to codify the development as envisioned in the CSP. The standards are closely related to the specific physical layout of the downtown core and cover all aspects of a built town center environment as stated in this requirement. The design standards are also illustrated with graphics and photographs that provide information to define the development character and quality of the proposed downtown core area.

The subject special purpose DSP is similar in many ways to the Planning Board’s previous approvals of detailed site plans for community character, special purpose detailed site plans for signage, and “umbrella” architectural approvals. However, this DSP differs greatly from previous approvals in its scale and complexity. The approval of this DSP will establish parameters, design standards, and site plans for reviewing permit plans. Functionally, this will work by having applicants provide staff with interim permit plans (similar to typical detailed site plans) during its permit design process, thus allowing staff to provide input on



conformity to the design standards. Variation outside of the percentage guide would be subject to review by the Planning Board. The Planning Board has also defined some additional instances which would require Planning Board review and are included in this resolution. The Planning Board review of permit plans under these circumstances will be treated similar to detailed site plan review, with the potential for further review by the District Council. The design- and character-driven standards and approval process is intended to provide assurance that the desired development quality will be achieved, and also allows the applicant flexibility to respond to the market in a timely fashion.

J. The following language on page 24 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

**[3. Development of this site shall be in conformance with Stormwater Management Concept Plan No. 19046-2007-00 and any subsequent revisions.**

Approved Stormwater Management Concept Plan 19046-2007-00 for the entire Konterra Town Center including the proposed development within the downtown core area covered in this DSP is valid through 2010. The DSP is consistent with the approved stormwater management concept plan.]

Add:

**3. Development of this site shall be in conformance with Stormwater Management Concept Plan No. 19046-2007-00 and any subsequent revisions.**

Approved Stormwater Management Concept Plan 19046-2007-00 for the entire Konterra Town Center, including the proposed development within the downtown core area covered in this special purpose DSP, is valid through 2010. The special purpose DSP generally is consistent with the approved stormwater management concept plan; however, the Department of Public Works & Transportation has refined the stormwater management techniques to be used, including bio-retention under sidewalks, in median strips of the roadways and in islands in the parking lots. The DPWT refinements are to be followed.

K. The following language on page 25 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

**[7. Development of the site shall conform to CSP-07003, or as subsequently revised.** The subject DSP is in conformance with approved CSP-07003. See Finding 9 above for a detailed discussion on each applicable condition attached to the CSP.]

Add:

**7. Development of the site shall conform to CSP-07003, or as subsequently revised.** The subject DSP is in conformance with approved CSP-07003. See Finding 9 above for a detailed discussion on each applicable condition attached to the CSP. The revisions to the Subregion I Master Plan could have an affect on CSP-07003 and such revisions will need to be taken into account. There is no expectation that such revisions would result in the DSP failing to be in conformance, but any adjustments required by the Master Plan revisions will need to be addressed.

L. The following language on page 28 of the Planning Board Resolution shall be stricken and replaced with the language below.

Strike:

**[30. Pursuant to the approval of VP-07108 the following minimum lot sizes are required for townhouse lots:**

- **A maximum of 36 percent of the lot sizes shall be between 1,000 and 1,799 square feet.**
- **A maximum of 46 percent of the lot sizes shall be between 850 and 999 square feet.**
- **A maximum of 10 percent of the lot sizes shall be between 630 and 849 square feet. The Applicant may submit, with any DSP that proposes townhouses, any new variance applicable to design standards, including a new variance for lot size.**

There are no townhouses proposed in this DSP. The only residential use proposed in this DSP is a total of 2,161 multifamily dwelling units.]



Add:

30. Pursuant to the approval of VP-07108 the following minimum lot sizes are required for townhouse lots:

- A maximum of 36 percent of the lot sizes shall be between 1,000 and 1,799 square feet.
- A maximum of 46 percent of the lot sizes shall be between 850 and 999 square feet.
- A maximum of 10 percent of the lot sizes shall be between 630 and 849 square feet.

The Applicant may submit, with any DSP that proposes townhouses, any new variance applicable to design standards, including a new variance for lot size.

The Planning Board had no authority to issue variances of this nature regarding lot sizes, and hence, its actions are void. The applicant must utilize the Detailed Site Plan process to obtain variances for lots sizes. In addition, the Planning Board failed to follow the legal criteria for granting variances. Thus, on a substantive basis, the applicant did not provide legitimate evidence of the need for the requested variances. In essence, the applicant is seeking lot size flexibility, which could be appropriate in the context of the overall Konterra Town Center East. This issue should be addressed during the revised Master Plan for Subregion I or by means of a revision to the Zoning Ordinance to address lot sizes in projects that have significant densities (such as in Centers).

Further, the Planning Board makes a number of findings under its “Planning Board Analysis” and “Additional Planning Board Analysis” on pages 29 through 35 of the Resolution. The following revisions shall supercede certain aspects of those findings.

First, the Planning Board finds that the application is consistent with the 2002 General Plan and the 1990 Subregion I Master Plan, and that the application conforms with the transportation conditions laid out in CSP-07002 and Preliminary Plan of Subdivision 4-07108. As noted above, to be

consistent with the foregoing plans, the applicant needs to address the issues of TDM and transit.

A condition will be added to address this.

Second, the Planning Board is rather expansive in finding that the applicant’s proposed permit review process meets the general intent of “this type of DSP for development character and quality except for certain procedural and design standard issues.” Because this proposed permit review process is an extraordinary process, a number of additional conditions are imposed to further satisfy the detailed site plan criteria, and this process is limited to the 16-block area that is the subject of this special purpose DSP. This process shall not be used for additional land in Konterra Town Center East outside of the 16-block area.

Third, with respect to permit plan application and the review process discussed under “Design elements,” this issue will be addressed in the conditions.

Fourth, with respect to the Design elements discussion regarding lighting standards and full cut-off lighting features, the Planning Board comments and condition on full cut-off lights needs to be placed in context. The full cut-off feature is particularly important on rights-of-way of main roads (such as the perimeter roads) and in parking lots or parking garages. However, along areas that are more pedestrian-oriented, the focus should be on quality design and the harmony of the lighting fixture with the distinct architectural components within each district, street, park and plaza. The general concern with the need for full cut-off lighting shall not be used to discourage the use of decorative traditional lighting fixtures that create a distinct sense of place in a portion of the site.

Affirmance of the Planning Board’s decision is subject to the following conditions:

- I. Prior to certificate approval of this detailed site plan, the applicant shall:
  - a. Obtain certification of Conceptual Site Plan CSP-07003. Any additional conditions attached to the final approval of CSP-07003 that impact the physical layout of the downtown core area as shown on the subject DSP shall be reflected on the site plans.



- b. Obtain signature approval of Preliminary Plan of Subdivision 4-07108; however, the approval of VP-07108 must be stricken from the approval.
- c. Revise the design standards in accordance with the recommended revisions in this report, and include the design standards book in the certification package for final certification approval.
- d. Expand the existing section on The Promenade in the design standards book to include additional design standards that establish design parameters for the architecture surrounding the central park, including, but not limited to, design, materials, color, street furniture, signage, lighting, shadow and wind analysis, detailed planting schedule, water features, if any, and hardscape; or obtain a separate DSP approval for the central park.
- e. Provide evidence demonstrating that the Health Department approves the applicant's environmental site assessment and testing that is required because of prior sand and gravel mining on this site.
- f. Provide evidence from the utility companies that have jurisdiction over the subject site that all utilities in the core area will be provided within the right-of-way of streets.
- g. Revise the design standards book as follows:
  - (1) Pedestrian pathways or connections through permanent surface parking lots, for which the farthest point exceeds four hundred (400) feet, shall be designed in "Allee" style. A typical design detail shall also be provided in Chapter 5, Landscape Standards.
  - (2) Provide a note in Chapter 7, Lighting Standards, indicating that pole mounted perimeter street and interior parking area fixtures, excepting light fixtures provided by and subject to BG&E requirements, are to be light fixtures with full cut-off optics. All light fixtures located within service court areas are also to be fixtures with full cut-off optics to the extent practicable. Decorative internal street and plaza poles, decorative building-mounted fixtures, and other fixtures used for aesthetic effect or security, including those that may be submerged in water features, will be light fixtures with cut-off optics, to the extent practicable and consistent with the District Council finding above.

On page 103, the Design Intent for Lighting Architecture shall be restated as follows: Light will enhance, feature and complement the distinct architectural components within each district, street, park and plaza to create landmarks.

- (3) Provide an infrastructure chapter to demonstrate how utilities serving the downtown core area will be provided and where the utility easements will be located. All utilities shall be underground. A utility exhibit to scale shall be provided.
- (4) Provide a section on transit access and transportation-friendly initiatives, which shall include:
  - (a) Provision of two or more proposed bus stops within one-quarter mile of the project and provide a discussion on transit and TDM measures in the Sustainability Chapter;
  - (b) Implementation of infrastructure within appropriate areas of the site for multiple support programs promoting shared vehicle usage such as car pool drop off areas, designated parking for vanpools or car share services, ride boards, and shuttle service to mass transit;
  - (c) Options for preferred parking for car share programs;
  - (d) Shared parking;
  - (e) A discussion of how the Applicant will implement the statements on page 22 with respect to the provision of bike lockers and showers for bike users; and
  - (f) Provision for secure bicycle racks in accordance with Leadership in Energy and Environmental Design (LEED) standards, to the extent possible or provide sufficient information on a more appropriate requirement for mixed-use developments. Usage of the bicycle racks shall be monitored throughout the course of development and the number or percentage may be increased or decreased as utility dictates. Modifications of the numbers and/or locations of bicycle racks should be documented with each permit application.
- (5) Revise the standards to state that Project Landmark Signs in excess of 50 feet and Vertical Beacon Signs in excess of 30 feet shall require approval of a separate detailed site plan.
- (6) Revise the standards to clarify that crosswalks shall have distinctive features such as pavers.
- (7) On Page 4 after the sentence discussing Metro extension, add: Transit connections to the MARC line and/or Metro are important aspects to successful urban development of the Town Center and to reducing vehicle dependency.



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- (8) On pages 12 and 14 the reference to “bike lanes” in the Recommendations section shall be changed to “bike demarcation” and the term “where appropriate” in the same references shall be changed to “except where infeasible”.
- (9) On page 29, Section 3.5.1 shall be restated as follows: Materials appropriate to the desired overall character of the development will be used, and there shall be an emphasis on traditional regional materials such as brick, stone and masonry.
- (10) Provide a new Sustainability Chapter of the Design Standards for the development and include the narrative submitted with the special purpose DSP in the new chapter. The Sustainability Chapter of the Design Standards, as submitted, shall be amended to include narratives of the following issues:
  - (a) The provision of sustainable roofing systems, inclusive of:
    - (1) Design technologies that incorporate vegetative material on roofs or portions thereof;
    - (2) The use of reflective / “white roof” technology; and
    - (3) The technologies used to collect and convey rain water for irrigation purposes.

The new chapter shall also specify the minimum solar reflectance index (SRI) value for sidewalks and building roofs. The applicant shall seek to incorporate sustainable roofing systems on the roofs or top levels of structures of no less than five percent of the roof area of residential and commercial buildings.

- (b) A narrative on the potential use of Photovoltaic and Geo-thermal installations. As this technology evolves, the applicant shall seek opportunities to incorporate this technology into the project where practicable, particularly seeking to utilize County based labor and materials.
- (c) A narrative on phasing the implementation of the collection of recyclable materials from commercial and multifamily properties. Collections shall commence not later than six months after opening of the initial phase.
- (d) A commitment to using energy efficient consumer appliances where practicable. Residential development shall include the

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- provision of energy efficient consumer appliances as standard or as optional features.
- (e) A narrative on transit and TDM measures consistent with the discussion of the 1990 Subregion I Master Plan. The known limitations of the regional and local transportation network make it clear that TDM measures in Konterra Town Center East are critical and need to be addressed at the beginning of the project, and not incorporated in later phases. Similarly, the adequate provision of transit opportunities shall be addressed, including the provision of shuttle busses, coordination with regional providers and key destinations. Konterra Town Center East is intended to be a high density regional center in the Baltimore Washington Corridor that is transit serviceable. To justify the density proposed, the Applicant bears some of the burden of addressing the transit needs and TDM measures to reduce traffic impacts. This may include provision of a broader shuttle bus service or subsidy to regional providers to support the level of service that is warranted.
- (11) Provide a note in each chapter indicating that the first permit plans for sites located within the adjoining blocks that front on the main streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of street trees, street furniture, and lighting fixtures for that street on which the project site is fronting.
- h. Modify the plan to make provision for the “Boulevard” standard for the access westward from Perimeter Drive West along the roadway connecting Konterra East and Konterra West.
- i. Revise the entrances to the lots north and south of Fashion Place shown on page 16 of the site plan to align these driveways at the proposed median break.
- j. Revise the Type II tree conservation plan as follows:
  - (1) Remove the proposed reforestation from the rare, threatened, and endangered species (RTE) buffer as it is no longer part of the site, and revise the worksheet accordingly.
  - (2) Revise the worksheet as needed to address other comments.
  - (3) Revise the legend to include any additional lines or symbols as necessary.
  - (4) Revise the symbols and line weights on the plans so that they are reproducible in black and white.

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- (5) Remove all duplicate information from the plan including, but not limited to, existing contours.
  - (6) Revise the line type used to depict the 65 dBA Ldn noise contour to be distinct from other features shown on the plan and provide additional labels as necessary.
  - (7) Show all existing woodland located within proposed or existing easements that are to remain as cleared, and revise the worksheet accordingly.
  - (8) Provide a TCP II signature approval block on all sheets of the plan set.
  - (9) Revise the limit of disturbance (LOD) to account for only the disturbance necessary for the proposed grading and structures shown on the plan.
  - (10) Remove proposed reforestation from property that is not part of the subject site and revise the worksheet accordingly.
  - (11) Revise the forest conservation detail to include the following note: "The signs are to remain in place."
  - (12) Revise the Type II tree conservation notes to include optional Note 3 and to replace all references to the "DER inspector" with "county inspector."
  - (13) Provide a specimen tree table on the plan containing the tree number, size, species, condition, proposed disposition, comments and/or special reservation treatments recommended, as well as a note below the table indicating how the specimen trees were located.
  - (14) Have the revised plan signed and dated by the qualified professional who prepared the plan.
2. At the time of permit plans, or site- or project-specific detailed site plans, whichever come first, the applicant shall provide the following with each application:
- a. A statement of justification to provide a detailed description of all applicable design standards and how the proposed permit plan satisfies them.
  - b. A parking tracking table showing the total cumulative required parking spaces to date and the cumulative number of approved parking spaces.
  - c. A development accumulation table showing the approved development including each type of use, the number of the dwelling units, and square footage.

- d. Additional outdoor open spaces, including green area, associated with multifamily residential buildings, except for residential over retail, to the extent practicable. Multifamily residential buildings shall provide a minimum of one fitness room and club room per 300 dwelling units to be used by all residents. The size of the rooms shall be in proportion to the number of residents, to be determined at time of review of each project.
  - e. An on-site recreational facility tracking table for the multifamily residential component.
  - f. A tracking table showing cumulative sustainability features. The Applicant will utilize programs as a guide and tracking tool that will serve to evidence the commitment to the sustainability initiative. Such criteria to be used may include the National Green Building Standard, Leadership in Energy and Environmental Design Green Building Rating System, Green Globes or similar organizations or criteria. This Condition is intended to complement Condition 10, below.
- A copy of these documents shall be provided to the Clerk of the District Council prior to submission.
3. Any detailed site plan or permit plan exhibiting one or more of the following conditions shall be reviewed by the Planning Board (in accordance with Part 3, Division 9 of the Zoning Ordinance):
- a. Deviations from the approved design standards such as height, setbacks, size of the sign face (except landmark signs), etc. quantitatively by more than fifteen percent;
  - b. Increases to the development density by more than ten percent from the overall development quantity in terms of the number of residential dwelling units and square footage for commercial and other uses above the maximums approved in this DSP;
  - c. Significant modifications to the area, quantity, or location of the open spaces or the plazas as shown on DSP-08011 in a manner that adversely impacts the aesthetic quality, design intent, or usefulness of the open spaces or plaza;
  - d. Exemptions from the approved design standards such as partial or complete waiver of parking and loading requirements of the developable blocks, reduction in green open spaces or tree canopy coverage;
  - e. Location of private structures within the air space above, or in the ground below, public rights-of-way (not including decorative structures or utilities approved by the Department of Public Works and Transportation); and



- f. Any other situation which, in the opinion of Planning Department staff, requires the approval of the Planning Board, such as when agreement cannot be reached between the review staff and the applicant, or any unforeseen conditions not included in this report which will have a significant impact on the physical layout of the downtown core area that adversely impacts the design intent.
- 4. The applicant and its successors or assignees shall provide the following:
  - a. The streetscape for roadways that are to remain publicly accessible, per Exhibit 3 in the public use easement(s);
  - b. Sidewalk connections and/or pedestrian walkways as applicable from all garages and surface lots to adjacent buildings;
  - c. The following walkways or the functional equivalent, if feasible in the event of subsequent layout modification, shall be provided:
    - (1) Sidewalk connections from the parking garages to Center Street on Sheet 24.
    - (2) Sidewalks along both sides of the entrance road to the parking garage from North End Road on Sheet 31.
    - (3) Sidewalks along both sides of the main drive aisle from North End Road to the parking garage on Sheet 34.
    - (4) Two pedestrian walkways or sidewalks through the parking area from North End Road to the planned buildings on Sheet 32.
    - (5) Two pedestrian walkways or sidewalks through the large parking area from North End Road to the planned building on Sheet 33.
    - (6) One pedestrian walkway or sidewalk through the parking lot between the office buildings on Sheet 28.
    - (7) One pedestrian walkway or sidewalk through the parking lot between the condominium buildings on Sheet 23.
- 5.
  - a. At the time of detailed site plan for the portion of the site including the stream valley trail, the trail alignment shall be finalized to ensure that it does not create an impact to the regulated buffers.
  - b. The applicant’s Recreation Facilities Agreement shall establish trail easement(s) that allow for the construction of a trail connecting the area of the Town Center north of the Pepco land to the southern area of the Town Center. This trail shall be on the east side of the Town Center Core. The applicant shall retain the sole

- unrestricted right to vacate, delete, move or reestablish the easement (including removal and replacement of any portion of a trail constructed within the easement) when deemed, in the applicant’s sole discretion, such change is necessary for construction of any other portion of the Town Center development, inclusive of infrastructure or some other amenity. The trail shall be constructed no later than completion of 50% of the Town Center’s approved residential development.
- 6. At the time of permit plans including the three roundabouts and all nonstandard roadway sections shown on the site plan, the applicant shall obtain approval from the Department of Public Works and Transportation. The relevant evidence shall be submitted with the permit plan.
- 7. Noise, stormwater management, and rare, threatened, and endangered species issues shall be addressed during the review of permit plans or site- or project-specific detailed site plans, whichever come first, in which that issue appears. The stormwater management shall adhere to the details developed by the Department of Public Works and Transportation, as revised to include stormwater management techniques for water quality and infiltration for the Perimeter Roads East and West, the South End Road and additional parking lots. These techniques include implementation of bio-retention medians and parking lot capture areas.
- 8. During the building permit process, the District Council shall establish an advisory “Focus Group” of seven persons, to be comprised of the representatives of area civic associations or local governments from the Laurel, Beltsville and Calverton communities. At least five days prior to any formal building permit submission, the Applicant shall provide the Focus Group with a presentation on the permit plans or preliminary drafts of the permit plans that are the subject of the DSP Design Standards. The applicant shall make available to the Focus Group members the information required to be submitted under Condition 2. Subsequently, the Focus Group may provide written comments to the MNCPPC permits review staff with respect to this special purpose DSP and the DSP Design Standards.
- 9. The applicant shall implement an internal shuttle that will serve Konterra bus stop locations for regional providers such as CTC, WMATA, and THE BUS. At such time as 50% of the Town Center’s residential dwelling units are occupied, the applicant shall have coordinated with the transit providers to establish bus routes or other mass transit from the Town Center to the MARC and/or a Metro Station, which service shall be available during appropriate AM and PM periods.



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Frequent service shall be provided to the Muirkirk MARC station for commuters either by regional service providers or by the applicant. If the availability of transit opportunities does materialize, during the review of subsequent detailed site plans for Konterra, the District Council may consider revising these Conditions to ensure the provision of an adequate level of transit service.

Vote: 8-0

This Condition 9 is intended to complement Condition 1.g.(10)(e).

10. No less than 18 months after final action on this special purpose DSP, the applicant and the applicant's heirs, successors and/or assignees shall provide a briefing to the Planning Board and the District Council on the status of the development in regard to permitting, construction, leasing, sustainability and storm water management, compliance with conditions and adherence to the design standards.

With respect to the Sustainability Chapter in the Design Standards, the applicant commits to creating a benefit/saving analysis that will verify the positive impact of various design, construction and operating elements and initiatives. While the applicant shall include a status update on sustainability in its reviews, due to the nature of the early phase development, it is recognized that progress may be more difficult to measure until such time that at least two million square feet of development has been constructed.

Subsequent to the initial briefings, if requested, annual briefings shall be presented at least through the initial stage. Additional or more frequent briefings may be requested by the Planning Board and the District Council. Upon such subsequent review, should the District Council determine that the applicant has not followed the Design Standards or has not been successful in achieving the sustainability goals, it may consider (as it is permitted to do pursuant to the Zoning Ordinance) revising these conditions for future development to ensure compliance with this approval.

Ordered this 21st day of July, 2009, by the following vote:

In Favor: Council Member Bland, Campos, Dean, Harrison, Knotts, Olson and Turner

Opposed:

Abstained:

Absent: Council Member Exum.

ATTEST:

*Redis C. Floyd*  
Redis C. Floyd  
Clerk of the Council

COUNTY COUNCIL OF PRINCE GEORGE'S  
COUNTY, MARYLAND, SITTING AS THE  
DISTRICT COUNCIL FOR THAT PART OF  
THE MARYLAND-WASHINGTON  
REGIONAL DISTRICT IN PRINCE GEORGE'S  
COUNTY, MARYLAND

By: *Marilynn M. Bland*  
Marilynn M. Bland, Chairperson



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# 1.0 Introduction/Overview

Konterra Town Center East is a planned mixed-use development to be located in Laurel, MD. It is ideally situated midway between Washington, D.C. and Baltimore, MD, and is accessible from and adjacent to, Interstate 95. The overall development will include retail, residential, office, public and recreational spaces, integrated horizontally and vertically, which will benefit both the local and greater Prince George's County community and the D.C./Baltimore region. As such, the type and magnitude of this development precludes it from following the usual processes of a detailed site plan requirement, one component of which is to provide 1:30 scaled drawings of every facade within the development. For this reason a set of design standards has been developed to provide direction and control for the physical design and appearance of the built environment ensuring the highest quality product and community is developed for Konterra Town Center East.

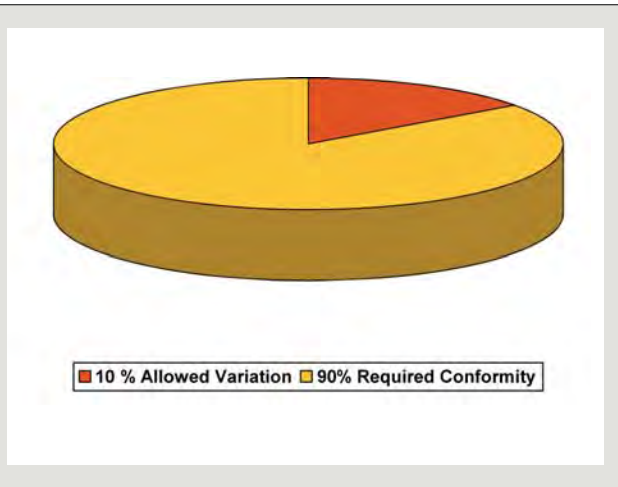
The intent of this document is to provide a well developed and workable set of parameters that maintain and reflect the overall character of Konterra Town Center East as a place and community. This document contains six major categories that are essential to define in order to ensure that development within Konterra Town Center East meets the desired standards and expectations of a successful mixed use environment. By creating a framework to govern buildings, streetscape, landscape, lighting and signage/graphics it is possible to control the built environment and ensure the integrity and character of Konterra Town Center East.

Although these standards serve to establish certain definable requirements that must be met, many of them are stated in terms of minimums, or given as a range or tolerance of acceptable measure. This approach is taken to provide a certain amount of room for diversity, flexibility, interpretation and variation that is essential to creating a place that is dynamic and engaging. As the project develops there may be instances where it will be appropriate to break with a requirement in order to achieve a better overall product/design; in the interest of allowing for those moments to occur we suggest a 15% deviation rule. This means that deviations within 15% of numerical development standards for: setbacks, building height, encroachments, easements, bridges, overhangs, canopies, balconies, projections, trellises, coverings, transparency at ground level, planter widths, and sidewalk widths are acceptable. In all cases code and safety regulations will take precedence over the allowance of aesthetic variation and in some cases will not permit a variation to take place. Variances should only be permitted where the resulting change adds value in some way to the overall development.

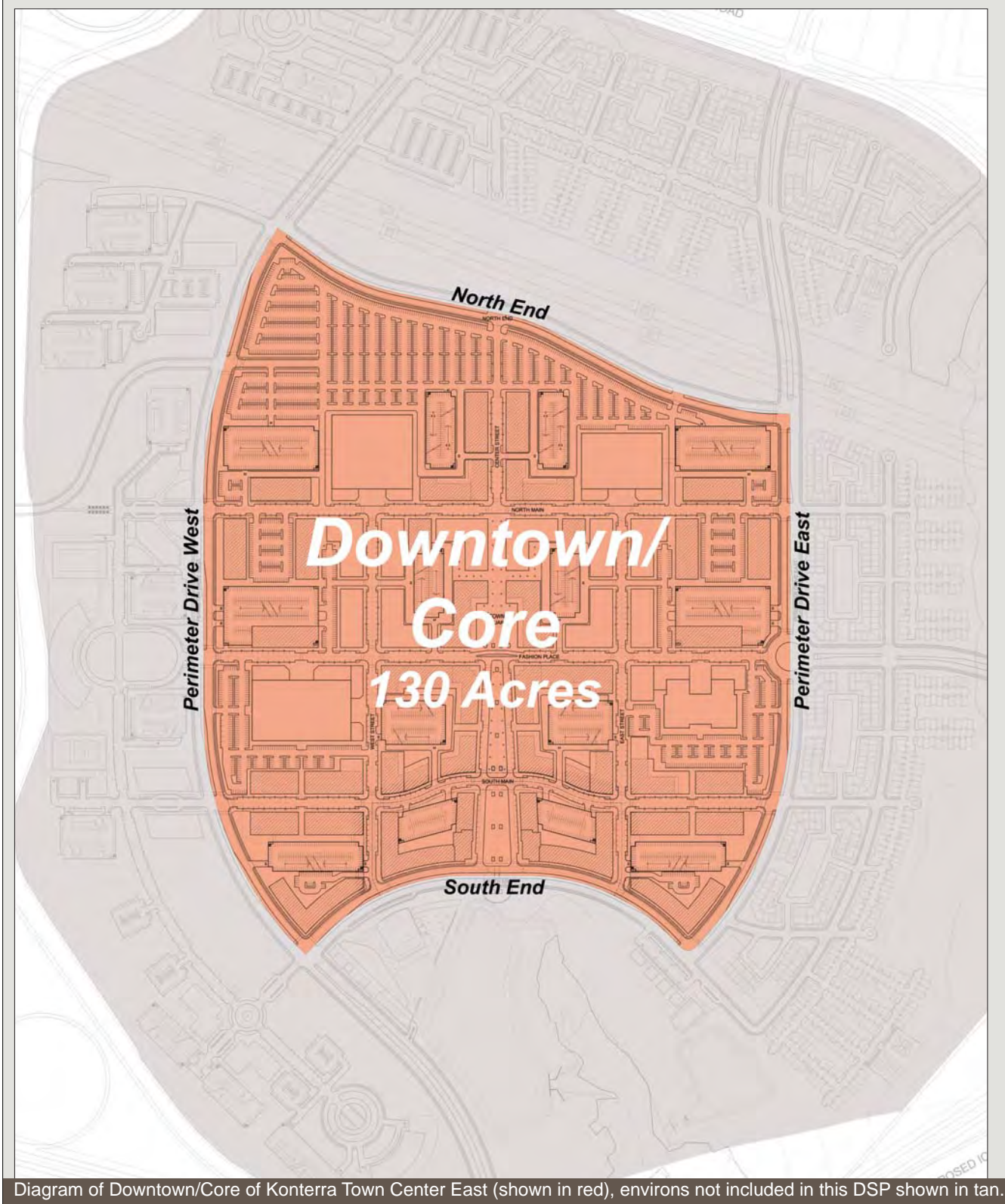
The design standards within this DSP submittal pertain only to the Downtown/Core of Konterra Town Center East. The diagram to the right highlights in red the 130 acres that make up the Downtown/Core of Konterra Town Center East and can be defined as the land located within the Perimeter Roads (Perimeter Drive West, South End, Perimeter Drive East and North End) measured from the center line of these roads inward. Although development may occur outside the downtown core it will be subject to a separate DSP submittal.

The downtown of Konterra Town Center East is planned to be a higher density, MXT-zoned, mixed-use project. As such it will contain a variety of uses including retail, residential, office and recreational spaces. In some instances these uses will be combined within one building integrated vertically, i.e. residential over retail; in other situations uses will be mixed horizontally meaning that each building will contain only one use but may be located adjacent to a building of a different use. This diversity will allow the character of a true mixed use environment to be built and expressed.

Konterra Town Center East is intended to be a twenty-four hour development which opens up opportunities for active uses to take place both during the day and at night, integrating possibilities like shared parking which can be more easily incorporated in this unique condition. Konterra Town Center East is also intended to be a multi-modal development encouraging visitors and residents to walk and bicycle as well as use their cars within the project. The incorporation of multiple uses and multiple modes of transportation will be unique to Konterra Town Center East and will provide a truly mixed use environment.

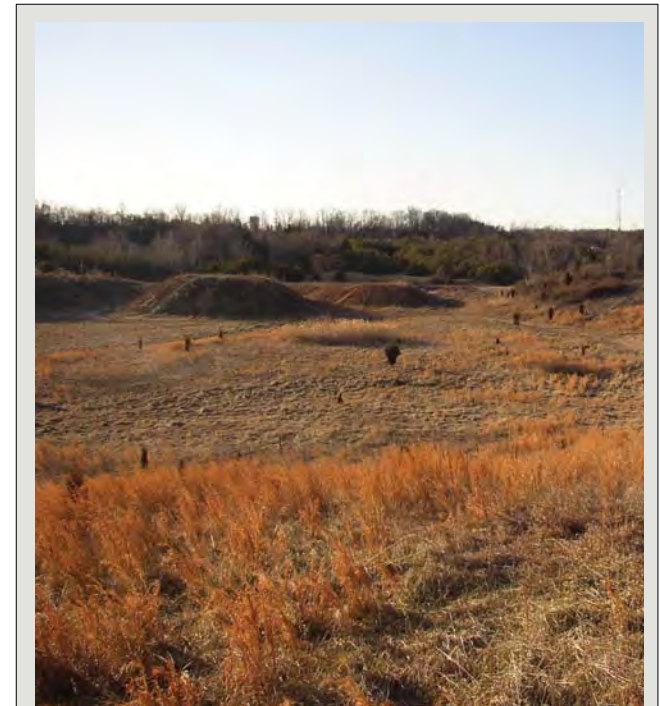
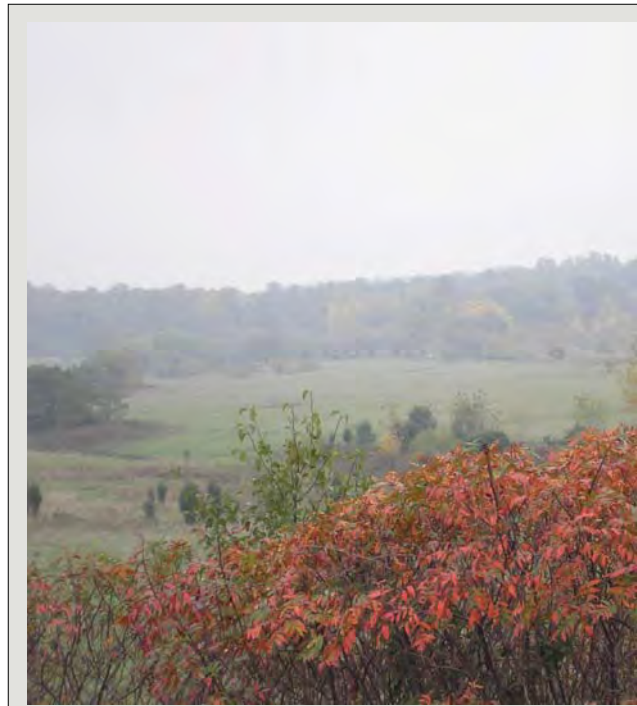


**Note:** The first permit plans for sites located within the adjoining blocks that front on the Main Streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of trees, street furniture and lighting fixtures for that street on which the project site is fronting.





## 1.1 Context of Konterra Town Center East



### The Context of Konterra Town Center East

The Konterra Town Center East site is ideally located midway between Washington, D.C. and Baltimore, Maryland off the I-95. The local and regional highway networks connect Konterra Town Center East directly to the entire Baltimore and Washington transportation network. The property is served directly by an excellent planned local and highway network, and nearly every road within the vicinity of Konterra Town Center is planned to be upgraded to arterial highway standards to further insure ease of accessibility to and from the project development.

The downtown core of Konterra Town Center East is planned to occupy

approximately 130 acres and contain roughly 4.25 million square feet of development at final build out. As a planned urban mixed-use development the increase and concentration of density will make it practical to offer a wider range of transportation options in lieu of the more automobile-dependent suburban solution. Consideration is being given at the state level to the possible extension of the Metro Green Line which would serve the National Security Administration, BWI Thurgood Marshall Airport and consequently Konterra Town Center East. Transit connections to the MARC line and/or Metro are important aspects to successful urban development of the Town Center and to reducing vehicle dependency.

To further encourage a multi-modal lifestyle within the project, Konterra Town Center East will incorporate a system of bike lanes and trails and connect to the existing recreational network.

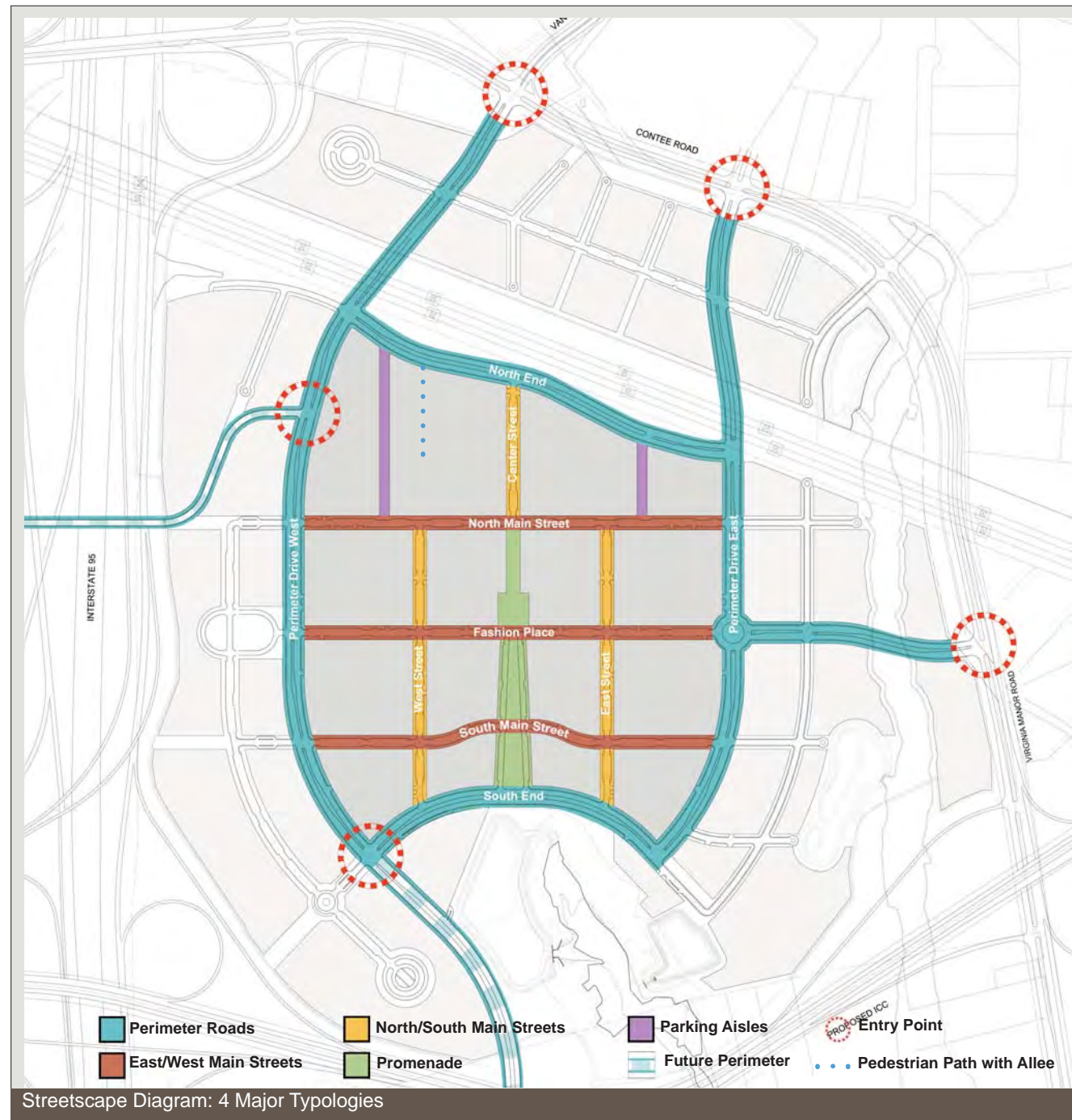
The land for the future site of Konterra Town Center East has been severely impacted by years of sand and gravel mining activities. The majority of the project site has been disturbed from its natural state and is devoid of tree cover. The general topography of the site is slightly sloping with isolated and relatively small areas of mounds, depressions and steep slopes. The high point, at an elevation of approximately 300 feet above sea level, is at the northern tip of the site. The low point, at approximately 190 feet above sea level, is at the southeastern corner of the site.

The area surrounding Konterra Town Center East can mostly be characterized as undeveloped and very lightly developed. Van Dusen road bounds the site on the north side and Virginia Manor Road bounds the site to the east. The I-95 creates the western site boundary and the future Inter-County connector will create the southern edge.

Konterra Town Center East will have a positive impact on the local community. With retail and office components there will be an increase in revenue and jobs created. The incorporation of residential and recreational uses into the development will offer housing alternatives and encourage community by creating a 24-hour environment.



## 1.2 Streetscape Network Overview



### Streetscape Network:

Four major road types make up the street network of Konterra Town Center East. Each typology is designed to accommodate the estimated traffic volumes as well as to enhance the overall character and function of the project. In general, roads that run east/west are level, and roads that run north/south are sloped to accommodate the topography of the site. Where the different typologies cross one another special conditions occur that reflect the nature of each street type. These intersections share a common framework of sidewalks, street trees, on-street parking, travel lanes, and landscaping that are sized and spaced per the guidelines. While details such as species of tree may change from street to street

offering variety, the framework that each street is governed by remains constant.

The intended use and character of the various street types will differ from type to type in scale and experience. For example, perimeter roads will be wider and contain a continuous median to aid in the safety and flow of traffic to and from the site, while the main streets will have a smaller more intimate urban feel, scaled to the pedestrian experience. The variety of street types offers a complete urban experience and allows opportunities for a range of different activities.



Perimeter Roads



East/West Main Streets



North/South Main Streets

### Perimeter Roads:

The roads diagramed in blue represent the 'perimeter roads' which are scaled to hold higher traffic volumes and to move traffic swiftly around the perimeter of the project. Because these roads are located further from the heart of the project, speeds will be faster and the scale of the roads will be sized more for the vehicle than the pedestrian. The main purpose of the perimeter roads will be to move large numbers of cars from the interstate, to the core of the project, and then back in a timely and efficient manner. (See section 2.3)

### East/West Main Streets:

The roads diagramed in red are the east/west Main Streets which are designed to be both pedestrian friendly and to move traffic efficiently through the site. These roads will contain two-way traffic as well as on-street parking. The major anchors and larger tenants in the project will line these streets creating a 'heavy' pedestrian zone. To accommodate this, large sidewalks, convenient crosswalks, street trees, street furniture and comfortable lighting will be implemented. These streets will be smaller in scale and slower in speed because they are intended to be more pedestrian in nature. (See section 2.4)

### North/South Main Streets:

The north/south Main Streets, diagramed in yellow, are gradually sloped to accommodate the site's topography. These streets are secondary to the east/west streets because they do not transect the entire site and therefore function as linkages between the east/west Main Streets. The north/south Main Streets will contain a variety of uses including retail, residential and office and will be designed to cater to both pedestrian and vehicular traffic. Wide sidewalks, convenient crosswalks, street trees and furniture and safe lighting will be characteristic of these streets. (See section 2.5)

### Promenade:

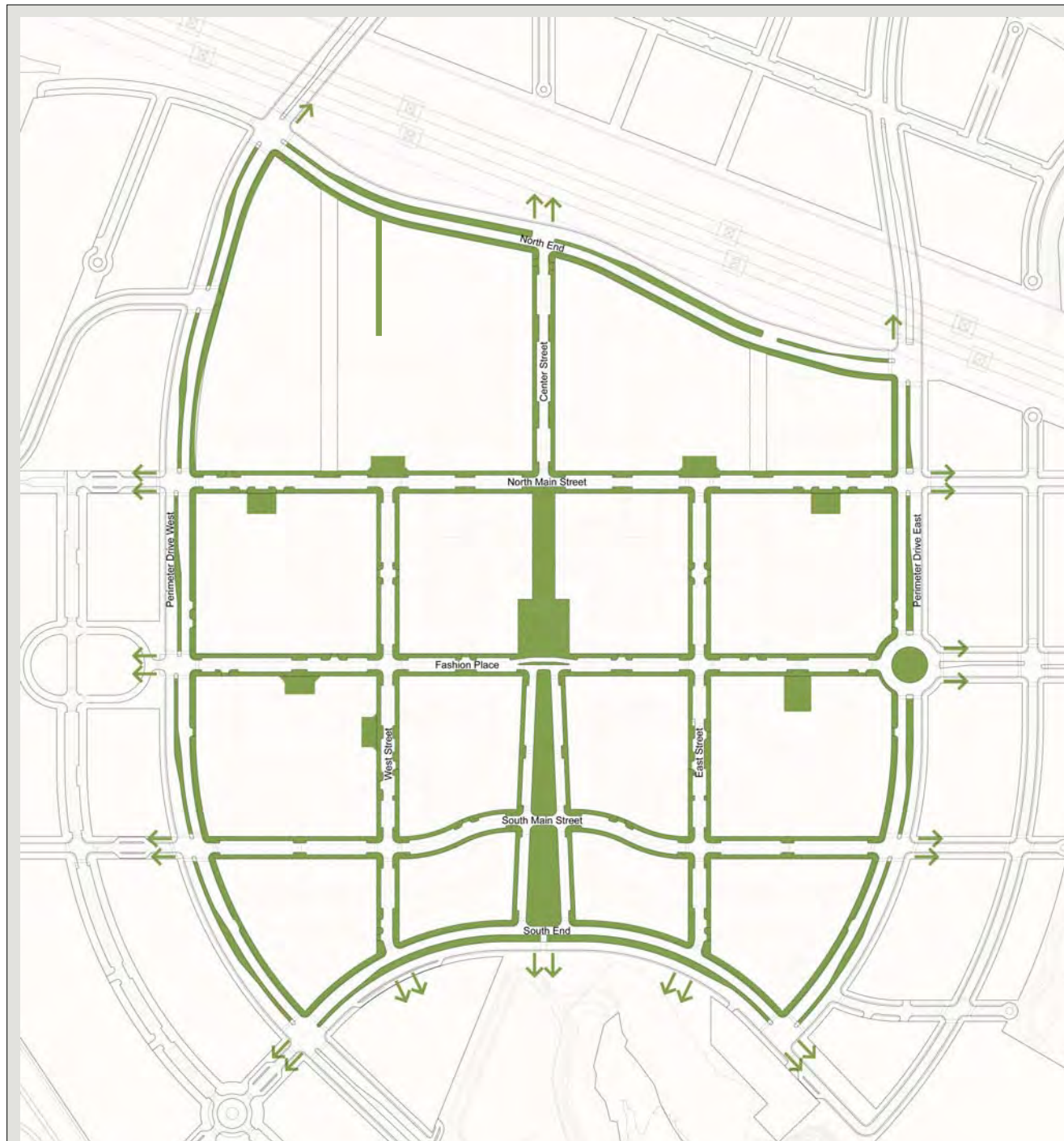
The promenade, diagramed in green, is a pedestrian friendly and open space focused circulation route. It is the primary north/south connection through the site and is comprised of a two-way traffic zone, a one-way couplet and the pedestrian passage. The Promenade is located in the heart of Konterra Town Center East and is an ideal place for entertainment, special events and gathering. As a major open space, the Promenade supplies the site with a valuable venue for alternative activities conducive to working, shopping and living. (See section 2.6.1)



Promenade



## 1.3 Open Space Network Overview



Open Space Diagram

### Open Space:

Open space is an essential part of Konterra Town Center East. As an urban mixed-use environment it is crucial to provide variety and relief within the built context and a forum for other recreational activities to occur enhancing the overall experience of the project. These open spaces will come in many forms including landscaped medians, wide sidewalks adjacent to landscape strips, pedestrian paths, outdoor rooms, plazas, open greens and town squares.

Because Konterra Town Center East is intended to be pedestrian oriented, it is vital that the pedestrian experience at street level be an enjoyable one. The provision of usable open space will therefore be essential to encourage pedestrians not simply to walk from place to place in the project but to pause and linger helping to make Konterra Town Center East not only functional relative to commerce but also a well-rounded community.

Open space will be carefully designed to be both functional and aesthetically appealing. Care will be given to ensure safety and comfort in order to capitalize on performance and use.



Promenade Green Space

### Promenade Open Space:

The promenade is a very distinct type of open space within Konterra Town Center East. Located between the one-way couplet at the southern end of Center Street this open space is designed to give relief to the urban context and to provide a place for leisure and gathering. As a large open space within the project, the Promenade can serve many uses from family picnics to an outdoor movie venue. The promenade is buffered by one-way streets and on-street parking, both of which act to slow traffic, making the area safer for pedestrians. The Promenade is also an ideal place for smaller retailers such as kiosks to operate and provide services that compliment the Promenade's uses. (See section 2.6.1)



Pedestrian Pathway



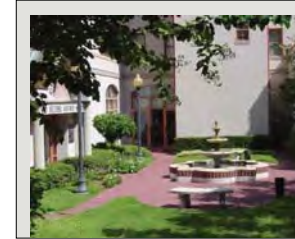
Wide Sidewalks



Streetscape Open Space

### Streetscape Open Space:

The predominant outdoor space in Konterra Town Center East is the streetscape. This typology, located between face of building to face of building, includes sidewalks, landscape strips, trees and street furniture and makes up the majority of the public open space. In most cases the streetscape follows a defined framework, between the street edge and building facade. At other times the buildings may be set further from the street edge allowing 'outdoor rooms' or plazas to occur. Covered walkways, awnings and tree canopies offer protection from the elements and encourage pedestrian use; while street furniture, such as benches, provide patrons a place to gather and relax within the realm of the streetscape.



Outdoor Room



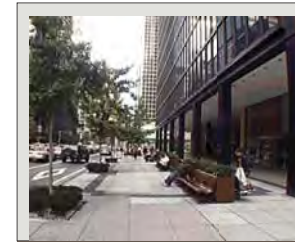
Landscaped Sidewalks



Town Square Open space

### Town Square Open Space:

Town Square is the heart of Konterra Town Center East. Located at the intersection of the north end of the Promenade and Fashion Place it is an ideal area for people to meet and gather, as well as a central location for special events to take place. Surrounded by all uses (retail, office and residential) with a view of the Promenade, it is the central point where all the features of Konterra Town Center East come together providing a space for all types of users and activities. The town square will likely be a mixture of hardscape and softscape surfaces, incorporating seating areas, cafe tables, open space and public art to encourage a mix of activities take place in this central community area. (See section 2.6.2)



Urban Plaza



Raised Plaza



# 1.4 Active Streets Overview



Pedestrian Street



East/West Main Streets

**Pedestrian Passage:**  
The portion of Center Street located between North Main and Fashion Place is the only permanent pedestrian street in Konterra Town Center East, although other streets can be closed temporarily for special occasions. The focal point of this street is the 'Town Square' and will serve as the heart of the project where pedestrians are encouraged to gather and relax. This area will also play host to any organized activities such as concerts or fairs. This would be an ideal place to locate restaurants and cafes because it is not directly adjacent to vehicular movement but is positioned centrally amidst the abundance of pedestrian activity. (See section 2.6.3)

**East/West Main Streets:**  
The east/west Main Streets in Konterra Town Center East contain the main entrances to the large anchor tenants, the majority of the retail tenants as well as residential and office uses. These streets will be the primary circulation throughout the site and will therefore likely host the greatest number of pedestrians, bicyclists and motorists, making them the busiest and most active streets in the project. As such, these streets will be designed to accommodate all types of users and will contain large sidewalks, landscaped streetscapes and essential signage to make navigation easy and intuitive. (See section 2.4)

**North/South Main Streets:**  
The north/south Main Streets contain a host of uses, from retail and of-office to residential and gathering spaces, and will serve as connectors between the streets running east/west. The north/south Main Streets are sloped to accommodate the topography of the site giving them a unique character within the development. While secondary to the east/west Main Streets because they do not completely transect the site, they too will be active in nature and host a variety of live, work, and shop experiences. Wide sidewalks and street furniture will be a vital and defining part of the character of these streets. (See section 2.5)

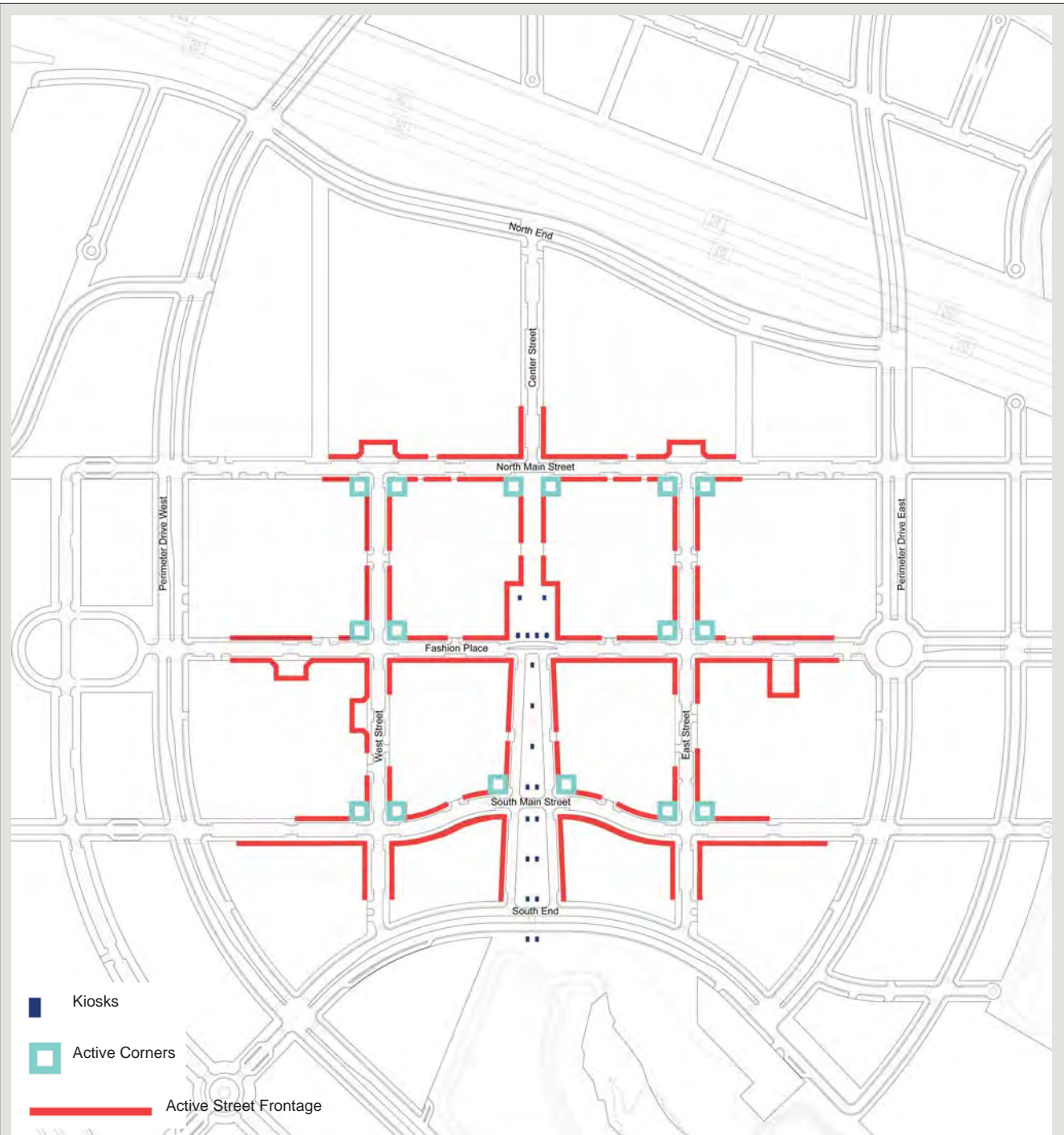
**Promenade:**  
Similar to the Pedestrian Street, a major component of the Promenade will be the open space that it provides its users. While the Pedestrian Street will primarily be a 'landscaped' hardscape the Promenade will likely have elements of both hard and softscape. The Promenade will provide an essential relief to the urban condition and afford people the opportunity to use the site for recreation and leisure activities as well. While cars do run the length of the Promenade the streets are arranged as one way couplets to better control the traffic flow and protect pedestrians. On-street parking will also acts as a buffer to control the interaction between vehicles and pedestrians. (See section 2.6.1)



North/South Main Streets



Promenade



Activity Diagram

**Intent:**

- To provide active urban streets which are pedestrian friendly and inviting.
- Active streets are those that contain retail, restaurant, office lobbies and entertainment uses.

**Required:**

- Active streets will be sized to accommodate large numbers of pedestrians to allow for efficient flow and ease of use.
- Tenants along active frontage streets are required to articulate their entry ways and front facades to enhance the pedestrian experience.
- Street furniture and landscaping will be provided to encourage pedestrians to activate the public realm in more ways than simply for navigation.



## 1.5 Vehicular Access to Parking Overview



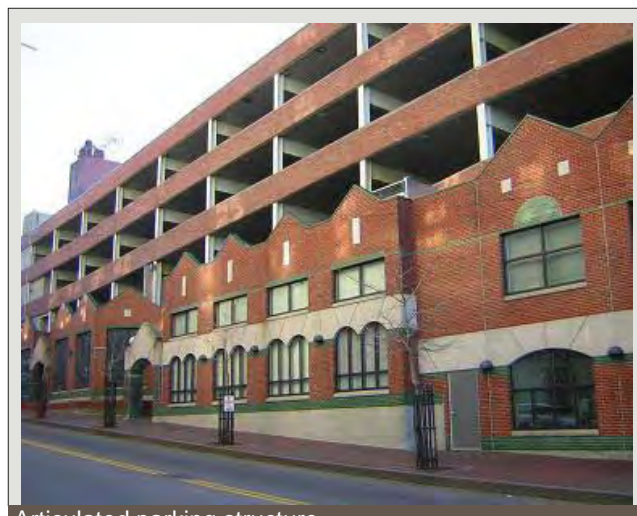
Articulated parking structure.



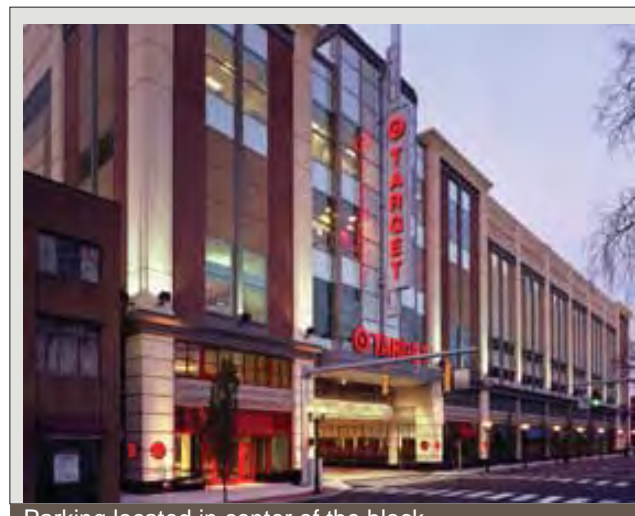
Parking structure with ground floor retail.



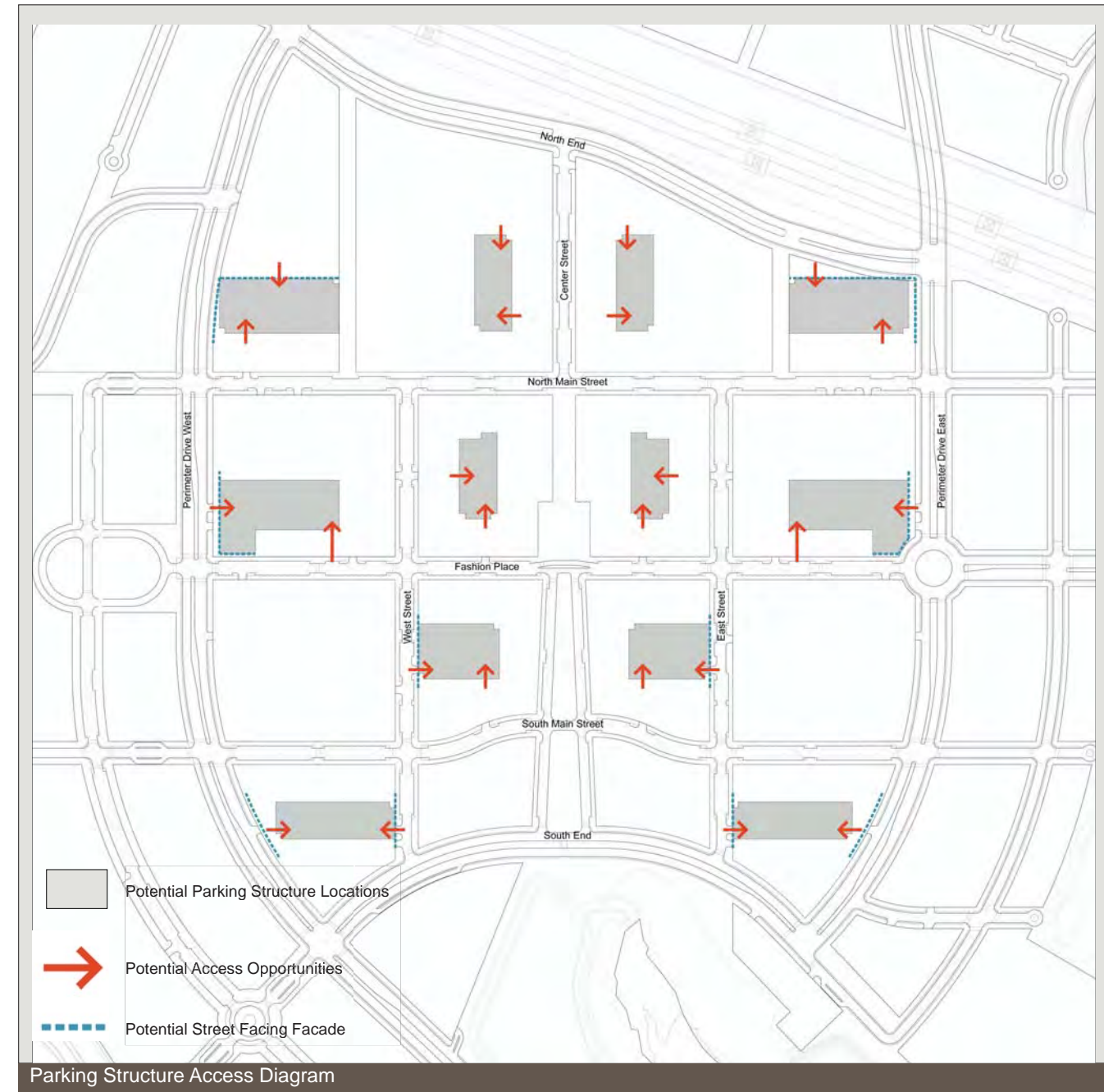
Mixed-use parking structure with retail at grade.



Articulated parking structure.



Parking located in center of the block



### Intent:

- To provide an easily understandable, accessible and navigable parking network within Konterra Town Center East.
- To locate parking to the rear of buildings / interior of blocks, and to articulate parking structures that are visible from the public realm where possible.
- To locate access drives discretely and on secondary streets where possible to maintain a hierarchy of circulation within Konterra Town Center East.
- Incorporate parking in a safe, convenient and aesthetically pleasing way.

### Required:

- All streets are sized to accommodate anticipated traffic volumes.
- Parking will be provided in various ways including surface parking lots, structured parking and on-street parking.
- Measures will be taken to screen parking lots, and detail parking structures, that can be seen from the street.



## 1.6 Street Level Views + Spatial Definition

### Intent:

To guide the visual and physical experience of Konterra Town Center East through the placement and definition of street walls, setbacks, building heights and landscaping to ensure the desired pedestrian atmosphere encourages people to use the site the way it is envisioned. These controls will guide the views within the site and the physical layout of the pedestrian realm and will provide for, and encourage pedestrian activity and use.

Building and streetscape criteria have been established to control the pedestrian's spatial experience within Konterra Town Center East. These parameters both establish a consistent experience throughout the project as well as allow for special moments to occur within established measures, eliminating monotony. (See section 3.0)

### Required:

#### 1.6.1 Street Walls

Provide a framework to control the way in which buildings address the street in terms of building set-backs and build-to lines for building facades fronting public streets, where it is important to maintain building continuity or to provide civic features such as plazas and/or pedestrian rooms. (See section 4.0)

#### 1.6.2 Setbacks

All street facing buildings are required to be setback no less than 15 feet from the face of curb to allow for wide pedestrian sidewalks, landscaping and awnings/canopies. Setbacks may not exceed 30 feet in depth from the face of curb. This distance allows for occasional plazas, but protects the overall continuity of the street wall. (See section 4.0)

#### 1.6.3 Building Heights

Building height requirements are based on building typology and are governed by minimum heights - there are no maximums. In general, single use buildings are required to be taller than vertically integrated mixed-use buildings. To ensure an urban street edge no building may be less than 22 feet from the ground to the top of the parapet. (See section 3.3 and 4.0)

#### 1.6.4 Variation in Plane

To prevent long expanses of monotonous facades it is required that the street facing facade walls vary in horizontal and vertical direction. The parapet wall must vary in height every 60 linear feet or less and must change in height by no less than 2 feet. The street facing facade of a building must change in plane every 30 linear feet or less at a depth of no less than 4 inches to add visual interest to the pedestrian realm. (See section 3.9 and section 4.0)

### Recommended:

#### 1.6.5 View and Vista Lines

Buildings that terminate vistas, or that hold prominent positions on the block, i.e. corner buildings, should be articulated with design features that acknowledge them as focal points. (See section 3.10)

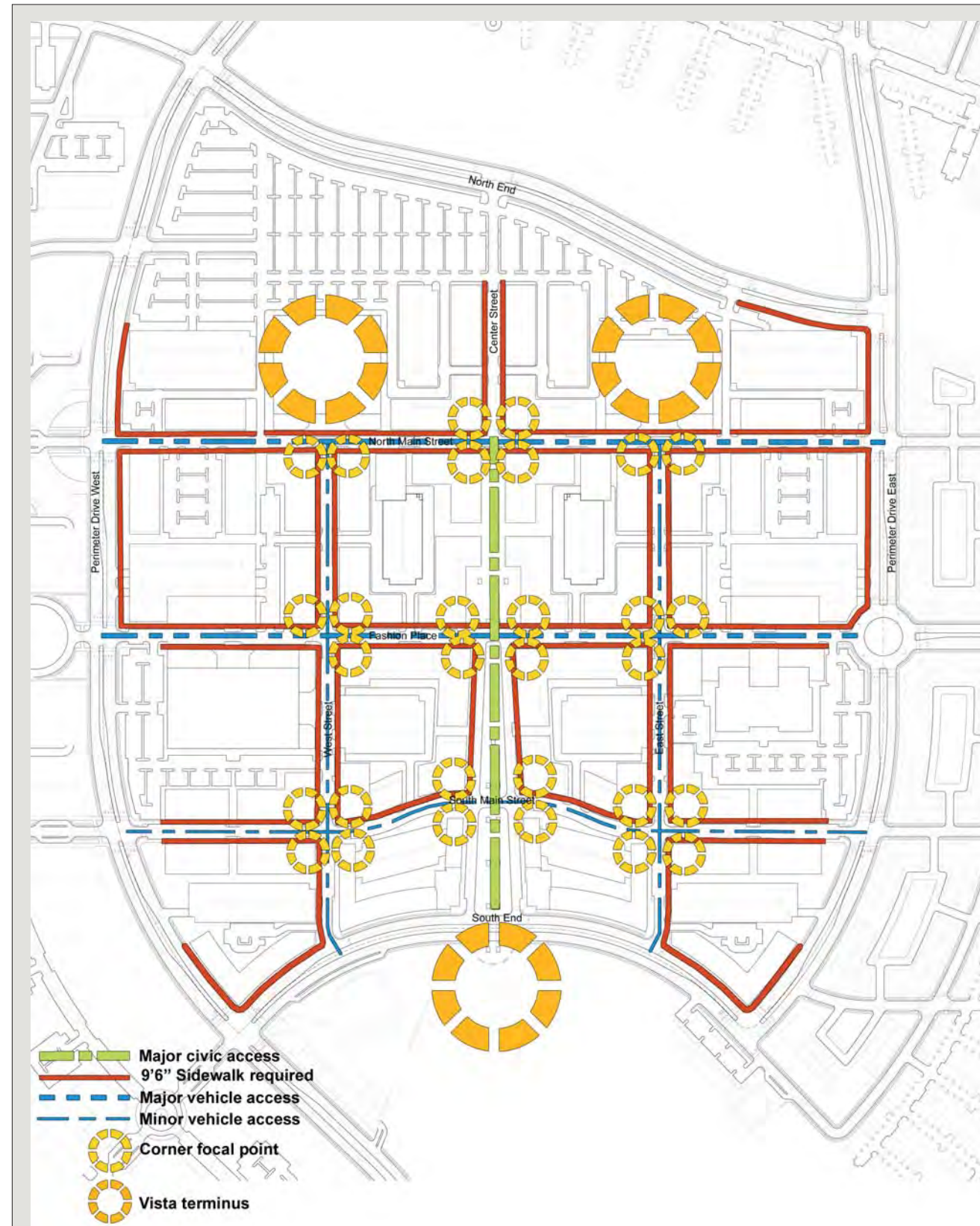


Diagram showing areas of spatial definition and opportunities for design features.



Rendering showing vertical and horizontal facade plane change.



Mandatory 10' sidewalk plus room for street furniture and landscaping.



# 1.7 Sustainability

## Konterra Town Center East Approach to Sustainability

### Overview

The Gould family, owners of the property since 1981, are well known for their commitment to the environment, historic preservation and long-term land stewardship. Konterra Town Center East is an example of that commitment by its adaptive reuse and reclamation of a former mined site. The Goulds' extensive experience in reclamation and adaptive redevelopment locally has resulted in more than a thousand acres of mine reclamation within which over 80 acres of wetland creation and 125,000 square feet of LEED certified office space has been constructed. Among the many examples of the family's commitment to sustainability here in Maryland, are the preservation of the Georgetown Branch/ Capital Crescent Trail, wind power and fluidized bed stone development and a national mine reclamation award. Sustainable stewardship is not only a family legacy but a long-term strategic core value.

Forest City has an 80-plus year history as a developer, owner, and manager of high-quality real estate. The company has a well known track record of inherently sustainable development, that includes adaptive reuse, historic preservation, urban revitalization, and development within close proximity to public transit. Capitalizing on these inherent characteristics and supplementing them with better design, construction, and management practices, the company has a growing roster of LEED-certified projects. Additionally, Forest City has recently launched a corporate-level department to further catalyze sustainability efforts based on the philosophy of balancing the "triple bottom-line" of people, planet and profit. Evidence of Forest City's commitment is found in our Sustainability Report Card for those projects being developed and for those recently completed.

For the Konterra Town Center East development, Konterra and Forest City Washington are committed to the principles of green design and smart growth. By emphasizing pedestrian and bicycle-oriented compact development, diverse uses and services, and energy efficiency in design, construction and in operations, Konterra Town Center East will be a healthy, vibrant, sustainable urban community that will serve as the premier gateway into Prince George's County, Maryland.

The principal sustainability goals for the Konterra Town Center East are compatible with Prince George's County's Master Plan for Subregion 1. Using an integrated design process, the developer commits to the following core principles of sustainability:

- Establishing compact, complete, and connected neighborhoods
- Protecting local watershed and respecting habitat
- Minimizing carbon dioxide emissions
- Ensuring public health and safety
- Ensuring that operating and maintenance procedures utilize sustainable and low-impact products

### Goals and Strategies

#### A. Use an Integrated Design Process

An integrated design approach is the key to designing a sustainable development and to achieving attractive buildings that use resources effectively and that fit the context of the built and natural environment. With this approach, the developer team, representing many different disciplines, is assembled early in the process to explore tradeoffs, synergies and to solve problems that may be over-looked or left unaddressed in a more linear process. An integrated approach allows for more advanced technologies to be considered and provides a building or neighborhood that operates as a comprehensive interactive system.

The Konterra and Forest City Washington team understands that treating the sustainability discussion as an "extra," adds expense and decreases effectiveness. Thus, for optimum benefit, green building awareness will be integrated into the design of each project phase. To this end, Sustainability consultants have been engaged to serve as a technical experts on green strategies and issues and to guide the project team throughout the project process to ensure the environmental goals for the project are achieved.

#### B. Establish Compact, Complete, and Connected Neighborhoods

By incorporating green planning strategies, Konterra Town Center East will promote the principles of smart growth and urbanism. A distinctive sense of place will be established through an integrated network of walkable streets, special sites reserved for public spaces, a mix of land uses, and easy access to amenities and services that will create a true live/work community and ultimately reduce vehicle trips. Additionally, bicycle racks and paths and pedestrian trails will be incorporated throughout the site to encourage pedestrian and bicycle uses. Other strategies and features may include:

- Compact development with "centralized density"
- Variety of housing types
- Community/public spaces: civic uses, water features, plazas, community areas, cultural facilities
- Access to open spaces and recreational areas: pocket parks, playgrounds, picnic areas
- Shopping and entertainment options: Destination anchor stores, national chains, regional shops, restaurants, theaters, and neighborhood-serving retail
- Pedestrian scaled and ground level retail
- Transportation Systems Management
- Multiple employment opportunities
- Outreach and education to raise awareness of the jobs and live/work opportunities at a local and regional level
- Outreach and education to residents and visitors on sustainable features

#### C. Protect Local Watershed and Respect Habitat

Formerly mined for sand and gravel, mining reclamation activities have recently been completed and the site has been restored to appropriate grades that stabilize the land area with adequate vegetation. As part of the mining reclamation process, four (4) sediment basins were constructed to control sediment runoff from the site. Additional action taken to control and treat storm water will be utilized to protect the Indian Creek Watershed and the environment. Konterra shall be allowed to use fee-in-lieu to satisfy up to 45% of its tree conservation requirements, provided such amount is used to plant trees or implement other beneficial improvements to the watershed. The fee-in-lieu funds shall be provided to and used by local advocacy organizations (such as the Anacostia Watershed Society, Patuxent Riverkeepers or Citizens to Conserve and Restore Indian Creek) that are engaged in projects and activities to protect and enhance the watersheds. Sensitive to Prince George's County's role as a leader in utilizing low impact development (LID), the developer team proposes a comprehensive storm water management plan to meet the State of Maryland and Prince George's County requirements for Water Quality Volume (Wqv), Channel Protection Volume (Cpv), Water Recharge Volume (Rev), and 100-year flood control. The existing sediment basins will be converted to storm water management ponds and will be sized to provide the remainder of the 100 year volume not achieved in the existing regional storm water management facility located just downstream of the site. In addition to the storm water management ponds, the developer will consider LID applications and upland infiltration devices (UID) where practicable and allowed by DPW&T in the development of the site to reduce erosion, surface runoff, to improve storm water quality, and to meet the storm water recharge volume (Rev) requirements of Prince George's County. The Konterra / Forest City Washington team, is seeking to implement LID techniques where practicable and permitted by applicable agencies. These may include the following:

- Bioretention areas
- LID as part of the project's street design: dual lane (divided) roadways designed with a depressed median and the integration of bioretention in the median
- Bioretention islands: widened islands in parking lots with bioretention integrated in surface parking lots
- Disconnect impervious areas/infiltration: Intercept roof or street runoff into perforated pipe system along the curb line with gravel trench below – where infiltration rates are acceptable per geotechnical investigation.
- Reduced pavement: 22' wide versus 24' wide roadways (non-standard)
- Rain barrels/cisterns: storage of roof runoff and re-use of stored water for irrigation
- Flow through planter boxes to intercept and slow flow close to the building footprint

Methods to minimize the use of potable water will also be considered at Konterra Town Center East through water efficient landscaping (i.e., xeriscaping, native/adapted species) and water-efficient fixtures (e.g., dual flush toilets).

To promote biodiversity and respect habitat, a hierarchy of vegetated open spaces will provide Konterra Town Center with a variety of outdoor amenities and recreation spaces. These outdoor spaces range from public spaces to semi-public and private spaces connected by pedestrian friendly streetscapes, sidewalks, and trails. Pocket parks, plazas, and gardens will be integrated to provide residents, workers, and visitors to the community a full range of outdoor pedestrian oriented activity options. Tree lined streets with attractive and comfortable sidewalks will provide a high level of connectivity within the community. In addition, integrated, well-designed exterior lighting to minimize light pollution will reduce the development's impact on distant nocturnal habitats.

#### D. Minimize Carbon Dioxide Emissions

Buildings and site development in the United States consume one-third of America's energy and two-third of its electricity and create one-third of all carbon dioxide emissions, a major cause of air pollution, smog, ozone depletion, and global climate change. Acknowledging this environmental impact, this development is planned to support smart growth principles that are accepted strategies for reducing the escalation of carbon dioxide emissions. The developer team commits to a comprehensive approach that addresses transportation, site, construction, and building energy use. Using an integrated design process, the team will consider the site, climate, building massing, building construction, systems, operations and maintenance, and occupant use patterns at the onset of the design process, with the goal of finding synergies and efficiencies inherent in the whole. As a result, the residences, shops, facilities, and open spaces at Konterra Town Center East will be developed with environmental-sensitivity, concern for energy- and resource-efficiency (reducing dependence on non-renewable fossil fuels), attention to indoor environmental quality and environmental benefits while maintaining functionality, durability, and cost-effectiveness.

Specific strategies for the development and buildings include:

- Pedestrian and bicycle access to public transportation
- Car-share and carpooling opportunities
- Reduce energy demand through:
  - Cool (light-colored, high emissivity and reflectivity) roofing that also mitigates the heat island effect
  - Tight and highly insulated envelope system
  - Exterior solar shading devices
  - Daylighting
  - Efficient heating, cooling, and air conditioning systems
  - High performance, low-e windows and glazing
  - Low mercury, efficient lighting: Compact fluorescents, LED street and exit lights, solar-powered exterior and landscape lighting

- Commitment to use Energy Star® appliances where practicable–
- Reduce transportation energy: Selecting construction materials harvested and/or manufactured locally, local contractors, and local businesses where practicable
- Construction and demolition waste recycling
- Green power purchase where cost effective

#### E. Ensure Public Health and Safety

Because there is an intrinsic link between environmental health and public health, the project's sustainability approach will address public health and safety as well. By creating a compact, pedestrian-scale, mixed-use design, Konterra Town Center East enables residents and visitors to walk, jog, or ride bicycles to access basic services and amenities. Designated recreational spaces, playgrounds, and fitness facilities allow for recreation and exercise. Trees, native plants, and other vegetation will be incorporated to help filter outdoor contaminants.

Americans spend up to 90% of their time indoors. As a result, the indoor environment has a significant impact on health and comfort as well. Konterra Town Center East aims to incorporate green materials and strategies where appropriate and practicable to ensure quality indoor environments, including:

- Materials that have low volatile organic compounds
- Materials that are formaldehyde-free
- Materials that do not emit volatile organic compounds or toxic gases
- Efficient, effective ventilation systems
- Access to natural daylight and views

In regard to safety and security, the project has incorporated crime prevention through environmental design (CPTED) principles into the site design. This includes creating defensible spaces, minimizing isolated areas, and giving residents visual access to common areas so they can keep an eye on activities. To emphasize safer pedestrian and vehicular interaction, cross-walks are proposed in key locations and the speed within interior areas of Konterra Town Center East will be no more than 25 miles per hour.

Exterior lighting will be used to create a sense of safety and security throughout the community with clear identification of circulation, gathering spaces, and parking facilities. In addition, lighting will help define the entrances to the community, the major streets and intersections, and office, retail, and residential pedestrian pathways. Lighting will also be used to enhance and feature the distinct architectural components and ambiance appropriate within each district, street, park and plaza to enhance landmarks.

#### F. Reduce heat islands and minimize impacts on humans and wildlife

The built environment is continually expanding, which causes an increase in temperature in urban areas. The projects approach to reducing the effects of heat islands through out the development will be a commitment to the use of high albedo materials for both roof and non-roof related surfaces. Konterra Town Center East will be a comfortable environment with abundant vegetation and tree-lined promenades and courtyards.

Courtyards with open grid pavers allow water to be absorbed through pervious material, while deciduous trees provide shade in the summer and allow sunlight to warm the area in winter. The majority of parking is stacked on site and the roof decks will have a high Solar Reflectance Index to reduce the heat emitted. Light colored materials for both roofs and paving reflect sun light and keep the solar heat gain of buildings low. The reduction of heat gain in buildings decreases the need for HVAC systems to cool indoor spaces, which in turn lessens the buildings energy consumption. Strategies that Konterra Town Center East will achieve include:



## 1.7 Sustainability

- Courtyard open grid paving out of highly impervious materials
- Concrete sidewalks with a Solar Reflectance Index of 29 or greater
- Gray or white concrete roads to reduce heat absorption
- Deciduous trees that shade 50% of the sites hardscape
- High Albedo roof materials to cover 75% of roof surfaces

It has been found that cities and urban areas are three to eight degrees warmer than surrounding areas due to the “heat island effect”. This temperature difference is attributed to materials with a lower reflective properties absorbing energy from the sun and reemitting that energy as heat. In an effort to reduce the heat island effect in the Konterra Town Center, materials to be used for sidewalks and building roofs will be highly reflective. The level of reflectivity, and therefore the temperature of a material, may be estimated by a composite index known as the solar reflectance index (SRI). The reflectivity of a material is in direct proportion to its SRI. For example, a black acrylic paint (low reflectivity) may have an SRI of 0, while white acrylic paint (high reflectivity) may have an SRI of 100. A minimum of 5% of roofing material will be either a white roofing membrane, green, or some other conventional roofing material topped with white acrylic coating. A minimum of 50% of sidewalks shall be white concrete, or asphalt topped with white portland cement “white topping”. Roofs and sidewalks shall have an SRI rating of finished material of 29 or greater.

Vegetated roof systems are an attractive solution to insulation and to reducing land needed for stormwater management. Vegetated roofs conserve energy by helping to keep the building cooler in the summer and warmer in the winter. Vegetated roofs also capture, retain and filter rainwater.

These roofing systems are composed of vegetation within a growth media, placed over a waterproof membrane. Vegetated roofs capture, retain and filter the rainwater until the growth media reaches its saturation point. At the point of saturation, the water is discharged through conventional roof drainage systems to continue the stormwater management and filtration process.

The type of vegetated roof system will depend on roof slope, roof membrane, load-bearing capacity of the buildings, and technology commercially available during the design of the project. In addition, appropriate technologies will be employed to collect and convey rainwater on site for landscaping and/or irrigation purposes. The equivalent of 5% of the roof area of residential and commercial buildings will be composed of sustainable roofing systems (defined to be those technologies that either incorporate vegetative material and/or provide for the collection and conveyance of rainwater for irrigation purposes). Konterra will provide a table at permit site plan to show how it will accomplish the 5% minimum roof coverage within Phase I of the development.

Unique shaded environments throughout the project provide enjoyable experiences for Konterra Town Center’s residents, employees, retail shop owners and visitors. Through sustainability, Konterra will be a desirable destination in Prince Georges County for shopping, living, working, and leisure.

### G. Photovoltaics

Photovoltaic is a renewable energy system that converts sunlight directly into electricity. PV panels equipped with PV modules are either mounted at a fixed angle on a roof facing south, or mounted on a tracking device that follows the sun. The potential energy demand of elements of the project and possible locations to utilize PV will be explored in the design phases of the project. Utilization of PV will depend on its economic viability at the time of design, the building use or tenant requirement and its impact on roof top views and aesthetics.

There are many types of photovoltaic energy systems. Photovoltaic cells have the potential for many uses through out the town center project including lighting an entry feature or parking lot, secondary pathway lighting, and providing renewable energy to a buildings electrical system.

An evaluation and explanation of the determination for use or non-use of this type of renewable energy system (based on the factors noted above) shall be provided during the permit site plan review process. If the systems are determined to be practicable and economically viable, Konterra will seek to develop photovoltaic cell projects with county based labor and materials where possible.

### H. Geo-thermal

Geothermal is a renewable energy system that can satisfy on-site electric power demand by offsetting conventional space heating, space cooling, or water heating energy consumption. The energy savings realized from the use on-site renewable energy can be determined by metering the on-site energy that is utilized. Geo-thermal installations will be evaluated on a building by building basis to determine if the installation is economically viable, taking into account locations of geo-thermal wells and tenant requirements.

An estimate of a building’s potential energy use will be calculated during the design phase so that the size of renewable energy system can be determined. The incorporation of a geothermal energy system into the project will be determined by geological factors of the building location, the size of system required, the land area needed to house the system, and the economic feasibility of the system at the point of design. An evaluation and explanation of the determination for use or non-use of this type of renewable energy system (based on the factors noted above) shall be provided during the permit site plan review process. Konterra will seek qualified county based labor and materials where possible.

### I. Collection of Recyclable Materials

A program that incorporates separation, storage, and collection of recyclable material on-site will be implemented to serve Konterra as a community. Materials that will be recycled include: paper, corrugated cardboard, glass, plastics and metals. Recyclables can either be separated on-site or commingled and sent to a Material Recycling Facility (MRF), where they will be sorted both mechanically and by MRF employees. Single-stream recycling is when all recyclable materials are placed in the same container. Single-stream recycling provides county businesses with many benefits that include: using compactors and bailers to consolidate recyclable materials; saving money on recycling and disposal fees; and using only one container for commercial property owners’ recyclables.

Recyclable material storage can be individually stored by residential properties and business owners and then collected at scheduled times to be stored in a central storage container on-site. A central location and storage area will provide easy access for both maintenance staff and collection vehicles. The method of collection and storage of recyclable materials will depend on economic factors during the design of the project.

The collections will start 6 months after opening each phase of construction.

### J. Transportation Demand Management (TDM). See discussion in 1.8

## Conclusion

The Konterra and Forest City Washington team is committed to sustainable urban development. The team will explore every opportunity available to utilize best sustainable practices in the design, construction, maintenance and operations of the development. By incorporating green and smart growth strategies, Konterra Town Center East will be a vibrant, safe, healthy, livable, sustainable 24-hour community with housing, employment, cultural, shopping, and entertainment options for its residents, workers, and visitors. The developer is committed to making Konterra Town Center East a premier gateway for Prince George’s County.



## 1.8 Transit Access and Transportation-friendly Initiatives

Consistent with the 1990 Subregion I Master Plan (“1990 Plan”), the DSP thru design and use implements an array—including some measures expressed in the Master Plan— of Transportation Demand Management (“TDM”) initiatives. A primary factor is the creation of the “downtown core area” and its redesign of the 1990 Plan. Konterra Town Center, as approved, transfers the dominant retail elements of 1990 Plan’s regional mall to a design of pedestrian oriented retail within the proximity of both office and residential uses. Designing real mixed use reduces peak hour trips and the proximity of jobs and retail to residences provides a more realistic potential for accessing venues via walking and cycling. The redesign and planning of mixed use becomes one of the best tools for implementing the objectives of TDM.

Specific initiatives are outlined in more detail below. Conceptually, the TDM program will include bus stops that will be accessible to the downtown core area making the entirety of that area convenient to walk. A shuttle will be implemented as demand requires to facilitate access within the downtown core and to regional transportation. Placement of a large component of retail development within the mixed-use downtown core results in a significant portion of the employee work force, as well as patrons of these businesses, traveling to and from the area at times outside of AM peak hour periods. Moreover, studies show a significant percentage of retail employees will utilize public transportation. Both of these effects are stated considerations of the 1990 Plan. Carpooling and vanpooling have increased and Konterra's TDM efforts include initiatives to further encourage this trend consistent with the objectives and considerations of the 1990 Plan. In part this is done by the placement of preferred parking and drop-off locations that will enhance the convenience of accessing employment locations by those taking advantage of car and van pools. Equally important, uses (residential, office, retail/dining and recreational) are being interspersed within the downtown core such that the parking space requirements in the downtown core area can be minimized and will consequently produce less off-site trips—particularly during peak hours—which considerations were a part of the 1990 Plan.

These measures will be complemented by the implementation of communications aimed at publicizing and promoting the various TDM initiatives in order to facilitate, encourage and increase their use. Walkable and cycle sensitive street/parking design are key elements to creation of urban place where car usage can become an option and not a necessity. The implementation of these elements into downtown core along with other TDM measures can, as proposed by the 1990 Plan, provide for substantive reductions in vehicle trips thereby allowing the capacity for development and creation of a true urban place to be realized. The TDM will help reduce off-site vehicle trips and the Town Center management will pursue programs to help promote the use of transit, create awareness of opportunities to share rides and/or cars and help to provide access to and stimulate the use of such programs whenever practicable. These programs will be monitored regularly to assess utilization and to make appropriate adjustments. The TDM programs will include the following:

A.) Bus and Pedestrian Routing Plan: Per the Final Phase Bus and Pedestrian Routing Plan, Konterra Town Center has more than two bus stops within a ¼ mile radius of the center of the Downtown core, the subject of the approved Detailed Site Plan (SP 08011). These stops are located along the perimeter roads of the downtown and can be accessed either by a pedestrian friendly walkway system or shuttle service thus making transit use and access convenient for the resident, patrons, and employees of the town center. Bus stops will be clearly marked and designed for pedestrian access and safety. Additionally, Konterra Town Center is within reasonable walking distance of nearby regional bus stops and public transportation hubs that are off the Konterra site. The transportation hubs such as stops Z29 and 89M for the WMATA bus service provide links to the suburban Washington D.C. area. Additionally, Laurel Mall serves as a hub for Howard County Transit, CTC and Connect-A-Ride services. Routes, possible subsidization of public transit by employees and stop times will be coordinated with the bus transportation providers to ensure proper connections at peak times. As development expands beyond the first phase and/or Town Center East, reevaluation and revisions of the Routing Plan may occur to further accommodate riders, employment areas or other sites that provide potential for increased ridership.

B.) Vehicle Share, Van and Car Pool: In an effort to encourage a reduction in daily vehicle trips, residential and office developments with the Town Center will pursue arrangements with companies that provide vehicles for sharing. This may require programs that provide payment of cab fare for carpool and vanpool users during an emergency. Also, dedicated drop off areas and parking for car pool participants including vanpools will be provided. Employer based transportation management programs will be encouraged and, if pursued diligently, can be among the most effective of transportation management programs as they can be tailored to the needs of the employee by alternating work schedule and or providing telecommuting options. Such options include allowing organizations, or some portion of employees, to start work early (before AM peak periods) or late (after AM peak period), such as flex-time arrangements or staggered work hours. This may include subsidizing the use of carpools/vanpools by employees. Subsidies can take many forms such as (partial) purchase of vehicles, gasoline, maintenance and the like, use of company-owned vehicles by carpoolers, cash payments to carpools and payment of parking fees.

C.) Ridesharing: Promoting ridesharing to reduce vehicle trips for school or work commutes through the use of ride boards that will include carpooling, vanpooling, ride matching and other techniques will be instituted with the initial phase of development. These techniques are geared towards commuters that either don't like to use mass transit or - because of location and/or work schedule - find it difficult to use mass transit. As such, ride boards will be placed in strategic locations within residential, office, and retail locations to make employees and residents aware of car pooling possibilities including car sharing companies and dedicated preferred parking.

D.) Retail employees will be directed to park in the remote area of the retail parking. Van pools for retail employees will have designated drop off areas within the retail complex. If warranted, employees using car share or car pooling will have preferred parking within the areas for retail employee parking.

E.) Shared Parking: Due to market forces, there will be a limited opportunity for sharing of parking for residential uses – sharing will more readily occur for office, hotel, and retail uses. We believe that with the mix and proximity of uses (including employment) combined the proximity of transit, the parking dedicated for residential uses can be reduced below what is currently required by code.

The key goal of a parking analysis is to find the “balance” between providing adequate parking to support a development from a commercial viewpoint and minimizing the negative aspects of excessive land area or resources devoted to parking. Mixed-use developments can result in greater density, better pedestrian connections, and, in turn, reduced reliance on driving, typically because multiple destinations can be accessed by walking.

The minimum recommendation by land use is as follows for the aforementioned parking analysis:

Hotel:	1 space/room
Retail/Restaurant:	4.25 sapces/100)SF
Office:	2.84/1000SF
Residential:	1.86/dwelling unit

\*minimums are subject to adjustment based on residential unit size, specific office or tenant requirements, transit developments and on-site monitoring of the utilization of parking provided.

A parking program will be provided for each phase of development. Parking will generally follow the guidelines provided in the analysis and may be adjusted to accommodate specific tenants or tenant mix requirements, new market or shared parking standards or the inclusion of other means of

shared transportation that reduces parking requirements. In the early phases of development, it is anticipated that excess parking will be developed to facilitate future phases of development without significant interruption or depleting existing parking.

A mixed–use development that incorporates retail, residential, hotel and office uses in close proximity to one another will reduce off-site vehicular trips and allow for the opportunity to provide fewer parking spaces than would be required for a single use development. Both the Institute of Traffic Engineers (ITE) and The Urban Land Institute (ULI) have published works on shared parking. Based on market analysis and guidelines provide by ITE and ULI, a parking program will be developed for each new phase of development and for the aggregated Town Center project that will ensure appropriate and sufficient parking for the combined uses. The methodology is intended to provide flexibility and will be adjusted as development densities and uses change and to respond to market conditions. The parking plan will be used to implement strategies to effectively manage on-site parking facilities and programs through the build-out process of the property and to ensure that an adequate parking supply is provided and utilized. The parking plan will consist of the following elements:

1.) Identification of land uses and densities within various area of the project;

2.) Identification of the market forces that may allow opportunities for shared parking. Due to market forces, there will be some opportunity for sharing of parking for residential uses – sharing, for example, will more readily occur between office, retail shops and hotel uses. Restaurants will generally require more parking than retail shops. Department stores will require a fixed, unshared parking count;

3.) Apply an industry accepted standard for hourly distribution by land-use to the non-shared parking demand;

4.) Incorporate restrictions that may affect a particular use such as dedicated spaces for residential;

5.) Incorporate ITE adjustments for multi-modal means of transportation (bus service and shuttle);

6.) Identify factors necessary to accommodate demand premised on the seasonality of certain uses, e.g. outdoor exhibition/entertainment areas;

7.) Adjust parking for “capture” among complementary uses;

8.) Provide a parking comparison of shared parking applicable to multi-use projects contrasted by parking required by the County Zoning Ordinance for single use projects;

9.) Apportion the parking on a block-by-block basis to ensure appropriate distribution;

10.) Identification of interim parking to facilitate phased construction.

A shared parking analysis prepared by the Traffic Group was included in the approved Detailed Site Plan. Should actual experience reflect parking shortages or a conflict, this parking analysis shall be revised to reflect actual experience and to address parking conflicts.

F.) The Konterra Town Center layout has been designed to accommodate bicycle movement in a bicycle friendly fashion. This includes well identified and generous bicycle passages within the streetscape of the downtown core area. Amenities for bicycles such as lockers and showers will be provided where needed and when practicable within office development or via health clubs within the retail development. The need and practicability will be addressed as part of the permit site plan. It is understood that those buildings with residential units will have the necessary secure locker facilities within the residential unit for their occupants who travel by bicycle. Retail spaces will have bicycle racks within reasonable walking distance along the rights of way or alleys.

G.) Cyclists have been provided for in the layout of Konterra Town Center as well. Bike lanes are found throughout the site, and bike racks will be provided to meet the needs of the cycling community. This would include provision of bike racks within 200 yards of a building for 5% of the building users to the extent possible. During the permit site plan review document stage for each phase of development the need and the extent of demand for bike racks and/or bike share options can be analyzed and adjusted based on users. The monitoring of the bike rack usage will be accomplished and documented with each permit application.



## 1.8 Transit Access and Transportation-friendly Initiatives



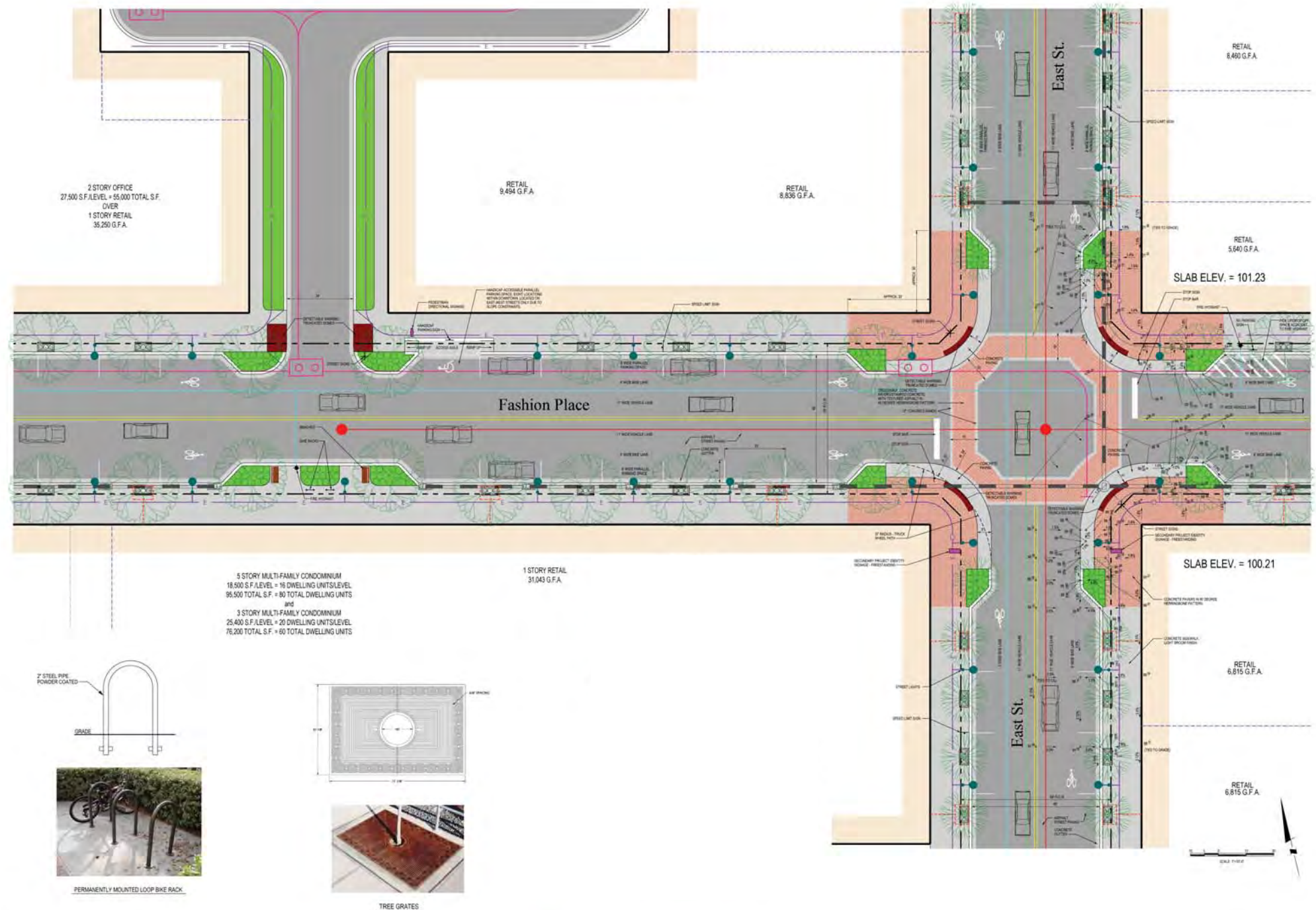
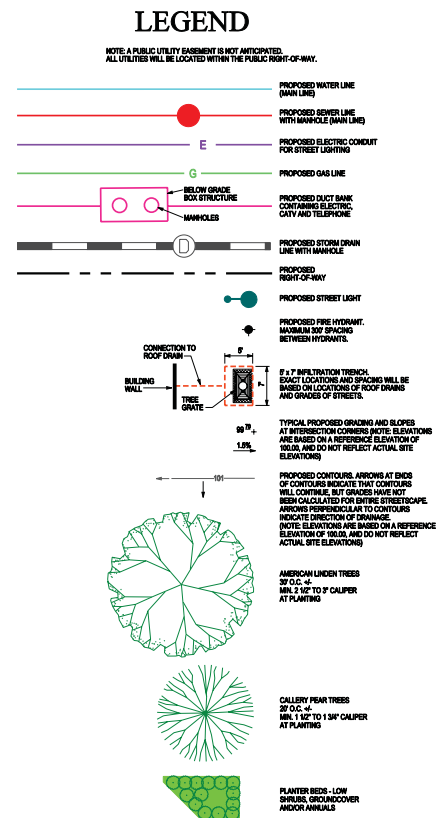


# 1.9 Infrastructure

There are two major components to providing utilities to the downtown core, onsite and utilities located in the public right of way. The utilities located in the public right of way will be installed in strategic location which meet or exceed the requirements of all of the utilities including but not limited to BG&E, WSSC, PG DPW&T, and Verizon. Utility easements will not be granted in the public right of way. In order to route all of the utilities and plan for additional capacity it was determined that a duct bank would need to be provided for the utilities to use. The concrete duct bank will contain multiple conduits. Each utility company will then receive separate manholes where required. A detailed plan will be prepared to establish the horizontal and vertical location of the shared duct bank as well as to coordinate with the "wet" utilities. The duct bank is to be constructed by Konterra for use by the various utility companies. Please refer to the exhibits on this page for a plan view and to the exhibits on page 16 for the cross-sections which depict the location of the utilities in the public road section.

The service will be routed from the public roads between buildings to the service areas and parking lots in the rear of the buildings to minimize the location of meter vaults, transformers, and gas regulators in the front public spaces.

The typical public utility easements will only be provided on the Perimeter Roads. Konterra will be providing a Duct Bank for the public utilities. An agreement will be signed with each individual Public Utility outlining where in the duct bank conduits have been provided. In addition a blanket easement will be provided for the onsite work.

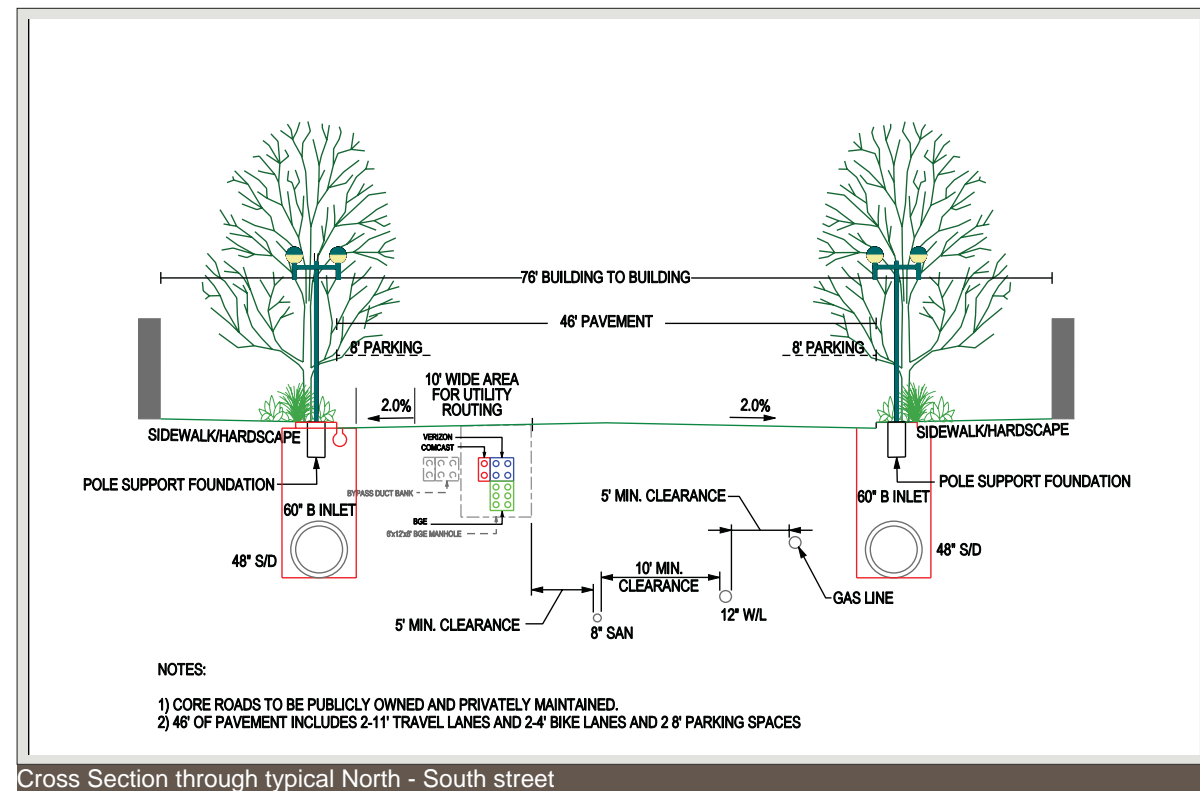


Typical Plan of Infrastructure Layout

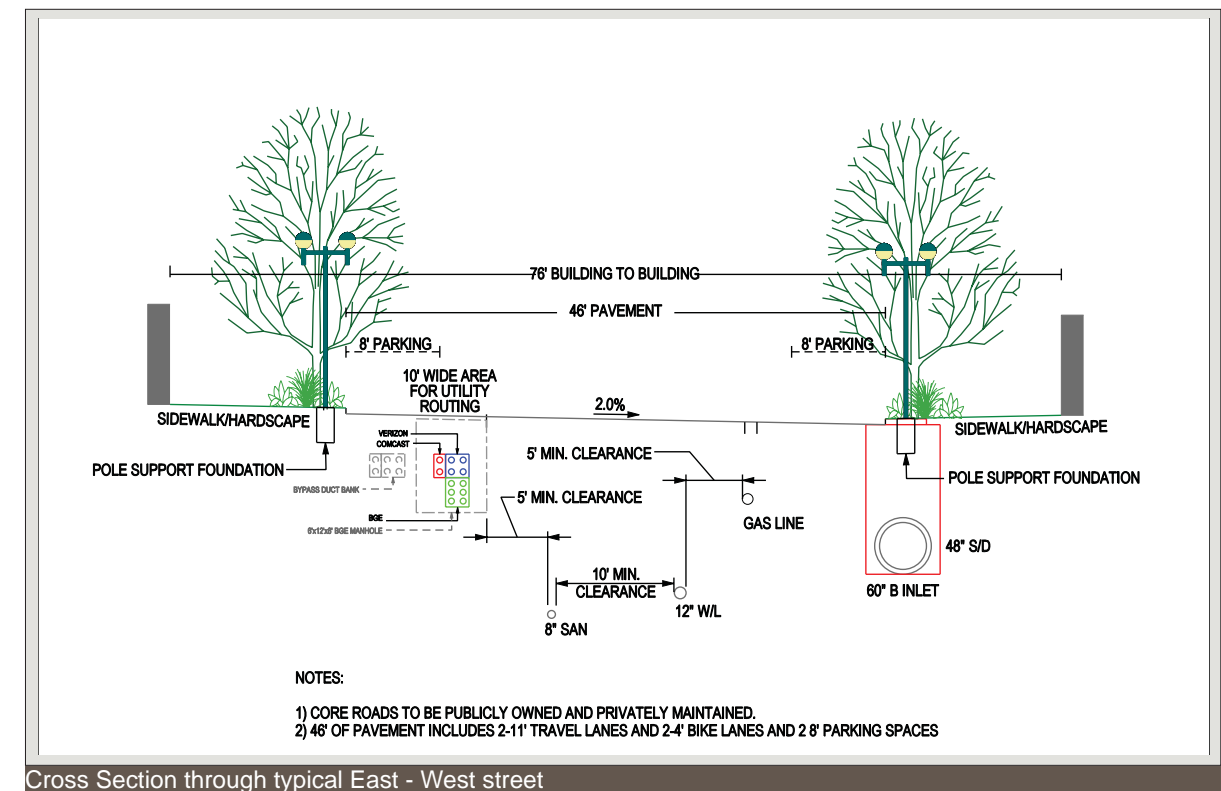
\* Please see enlarged sheet 3 of the DSP



## 1.9 Infrastructure



Cross Section through typical North - South street



Cross Section through typical East - West street



# 2.1 Human Comfort

## Intent:

The most essential element to successful place making is creating an environment that is comfortable, attractive and functional for the people using it. Key to this point is designing for human comfort. Several factors effect the way that people experience an environment from shading and thermal control to providing the appropriate amenities like street furniture to encourage multiple and varied use of the site. As Konterra Town Center East is intended to be a 24-hour community it is vital that human comfort be considered for all seasons and times of day while ranging in focus from visiting shoppers, weekday office workers and long term residents.

## Required:

### 2.1.1 Shading/Weather Protection

Prince George's County, MD has a temperate climate with temperatures ranging from highs in the 90's during summer to lows in the 20's during the winter months. Prince George's County also deals with a steady average monthly amount of precipitation ranging from 3 inches to 4.75 inches throughout the year. These factors need to be addressed in the design of both buildings and the streetscape of Konterra Town Center East.

Awnings and canopies serve a dual role of providing shade in the summer months as well as protection from rain and snow in the wetter months. Being able to provide somewhat continuous overhead coverage will be beneficial to the project making it easy to get from place to place regardless of the weather conditions outside.

Street trees will also play a role in providing shade and comfort to Konterra Town Center East. Not only will trees provide shade to pedestrians throughout the project, they will also help to control glare and the heat island effect on streets, helping to lessen areas of intense heat in the summer. The use of deciduous trees will also help to provide shade in the summer but allow sunlight to filter through to the street level more easily in the winter thus contributing to a better and more comfortable pedestrian climate. (See section 3.2, 3.4 and 3.6)

### 2.1.2 Scale and Proportion

Care will be given to create a street wall that is consistent and comfortable to the pedestrian experience. Wanting to maintain an urban character and feel throughout the project, bulk restrictions are stated primarily in terms of minimums to ensure that elements such as building heights and setbacks meet certain minimum standards. Building heights range from the lowest minimum height of 22 feet for stand alone retail, to the highest minimum of 48 feet in height for stand alone office. A minimum 15 foot setback from the face of curb is required for all buildings to provide a continuous pedestrian sidewalk, but may increase to as large as 30 feet to allow for periodic plazas or gathering spaces. The streets within the core are primarily single lane, two-way traffic with on-street parking and are a minimum of 76 feet from face of building to face of building (excluding building canopies, awning and projections). These governing proportions create a consistent framework for the pedestrian experience of Konterra Town Center East. (See section 4.0 for guidelines pertaining to specific building typologies)

### 2.1.3 Street Furniture and Amenities

Konterra Town Center East is intended to be a multi-modal development that is both pedestrian and vehicle friendly. Bike lanes will be incorporated into the streets, sidewalks will be designed to allow for comfortable movement from place to place by pedestrians, and streets are sized to accommodate cars and service vehicles comfortably. Within this framework it is important to incorporate the necessary amenities that will encourage visitors and residents to use the site to its fullest and provide areas that service the demands of all users. The pedestrian zones should incorporate street furniture such as seating areas and low walls, trash receptacles, planters, special paving etc. to enhance the physical, and visual value of the project. Bike racks and lockers should also be provided to those who choose to utilize the

site's connection to the larger recreational network. Parking areas should be clearly marked and zones where pedestrians and vehicles cross should be discernible making each aware of the possible interaction between them. (See section 2.2)

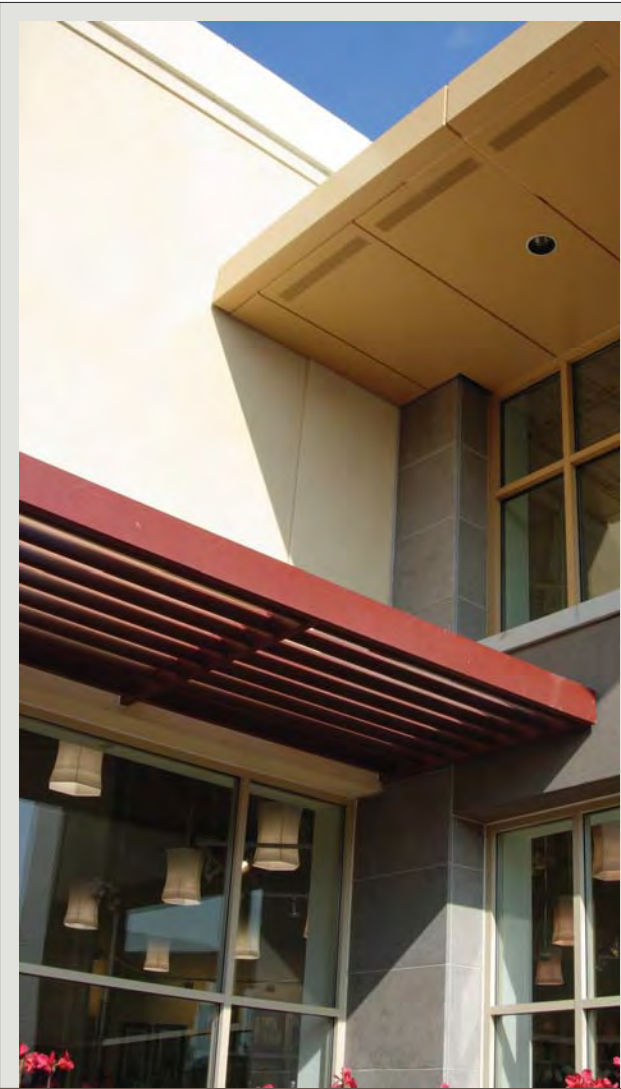
### 2.1.4 Lighting

Lighting is also a major factor in obtaining human comfort. As Konterra Town Center East is to function as a 24-hour development it is essential to use lighting to accentuate and enhance the buildings and landscaping as well as to provide a safe and well-lit environment in the evening hours. Certain areas such as parking structures and the areas between buildings that pedestrians use to access parking must be well lit and feel safe. Lighting must be sufficient for both pedestrians and vehicular traffic use. (See section 7.0)

### 2.1.5 Signage

Signage will also be instrumental in the effectiveness of way-finding and navigation of the site. Signage will need to be sized appropriately and located in designated areas for pedestrian and vehicular use. (See section 6.0)

**Note:** The first permit plans for sites located within the adjoining blocks that front on the Main Streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of trees, street furniture and lighting fixtures for that street on which the project site is fronting.



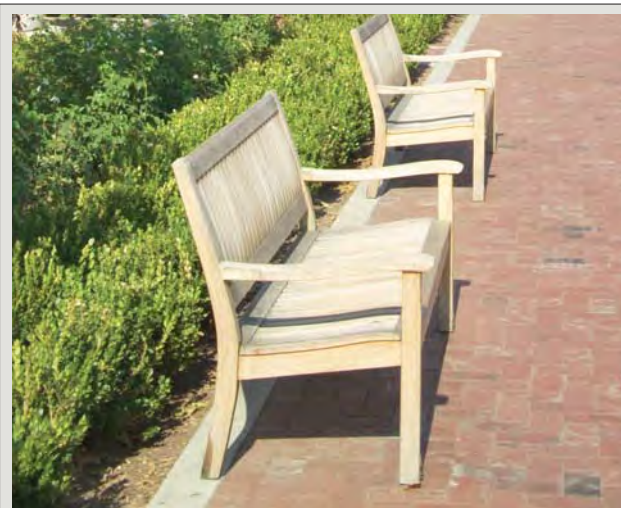
Shade structures provide protection and lower indoor temperatures.



Pavers provide signage and way finding details for pedestrians.



Continuous awnings provide shade and shelter from the elements.



Street furniture and special pedestrian paving materials.



Lighting between buildings leading to parking structure.



## 2.2 Streetscape Identity

### Intent:

Konterra Town Center East has a clear and defined streetscape framework that is repetitive and recognizable throughout the development, clearly defining the project's character. Details may vary from street to street within the project to further define an area within the development but the overall framework remains consistent throughout. For example the species of street tree or the sidewalk paving material may differ between Fashion Place and North Main as an indication to the user that they are on a different street with a slightly different look however, certain parameters, will stay the same throughout the entire development, such as minimum tree spacing, on-street parking stall size, travel lane width and sidewalk width making for a harmonious overall experience.

#### 2.2.1 Face of Building to Face of Building

The distance between street facing facades throughout Konterra Town Center East will be at minimum 76' feet from face of building to face of building along the downtown Main Streets. This distance is determined by various dimensional requirements that govern the necessary sizes of things such as travel lane width and on-street parking stall size.

#### 2.2.2 Travel Lanes

The width of travel lanes is governed by the county standards and requires that they be a minimum of 11 feet wide. The downtown core of Konterra Town Center East is comprised predominantly of single lane two-way traffic. There are exceptions to this, for example the perimeter roads which are designed to be two lanes of traffic in each direction separated by medians with left turn lanes at major intersections, as well as the southern end of Center Street, which is a one-way couplet.

#### 2.2.3 Bicycle Lanes

Konterra Town Center East is intended to be a multi-modal development and will incorporate bicycle lanes into its street network. Per county standards bike lanes are required to be no less than 4 feet in width.

#### 2.2.4 On-Street Parking

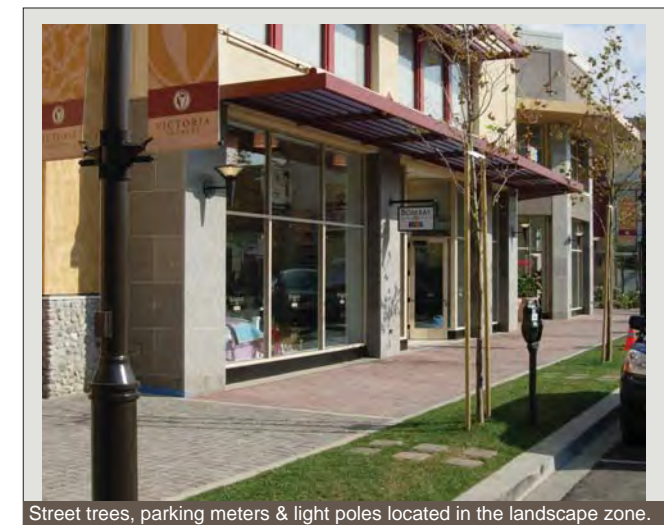
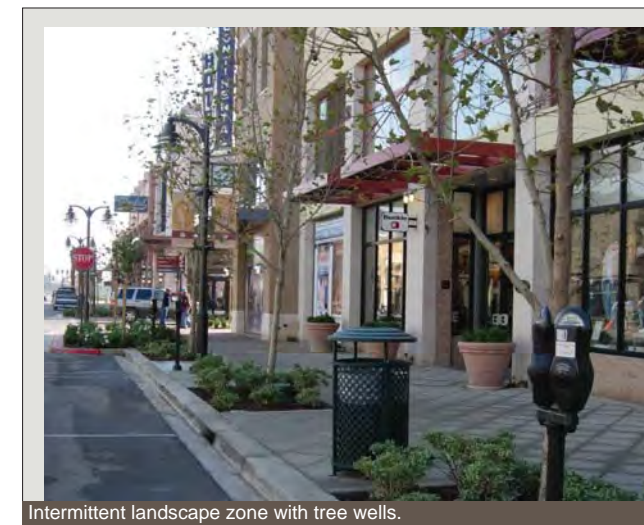
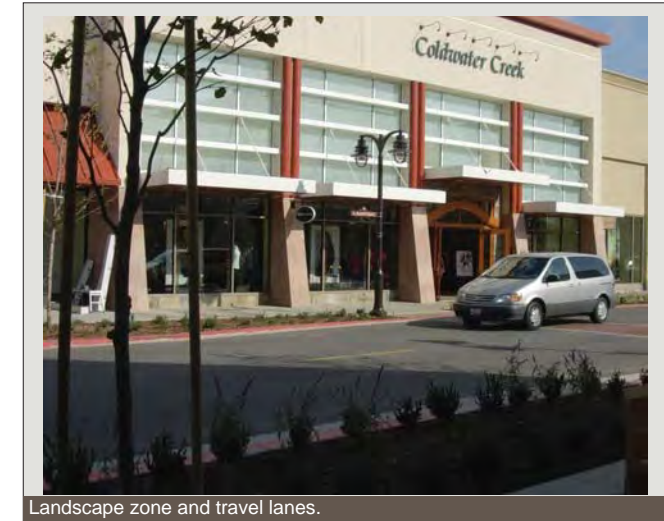
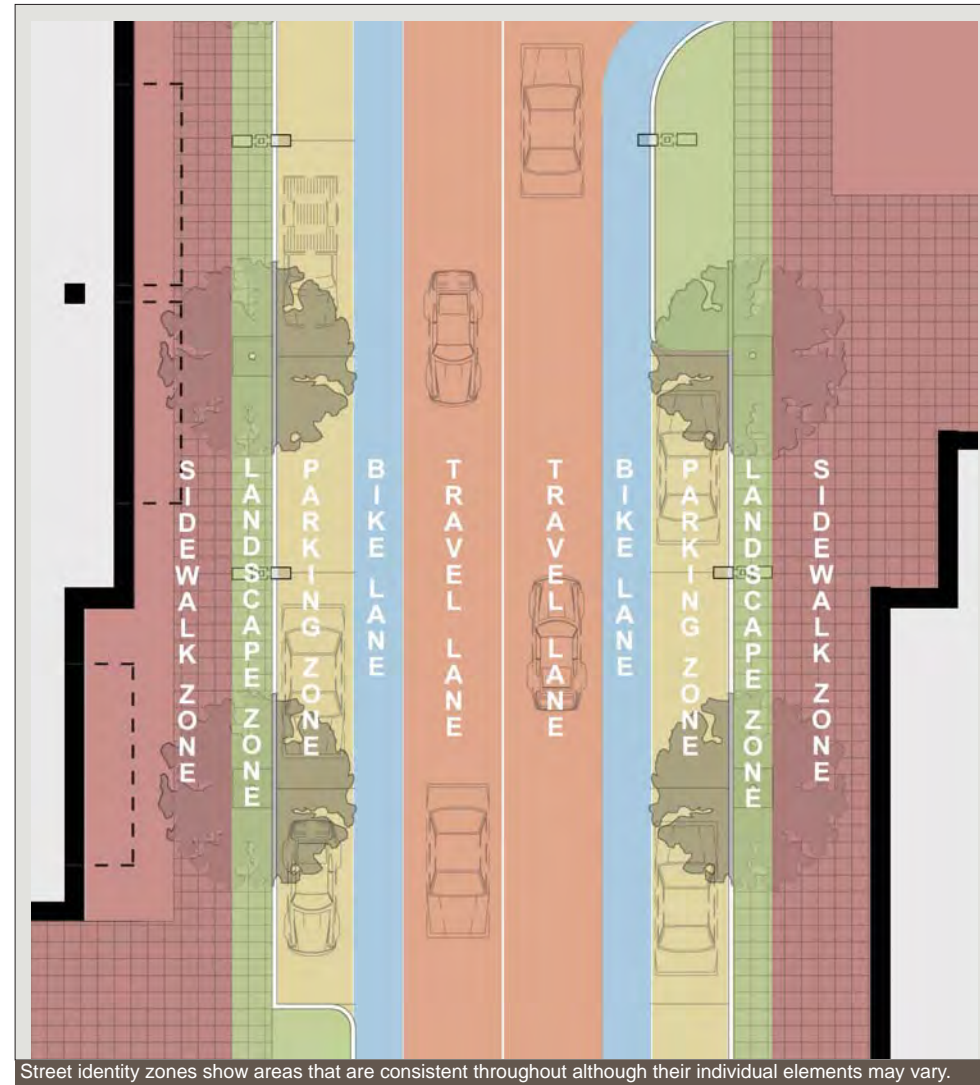
On-street parking is sized in compliance with the county requirement and requires each stall to be 8 feet by 22 feet. On-street parking is important to the project because it offers short term, one stop parking for those making quick trips to the site. On-street parking functions as a traffic calming device, forcing thru traffic to slow down to allow users to pull in and out of spaces. Finally on-street parking creates a buffer between vehicular movement and pedestrian movement making it more difficult for pedestrians to jaywalk and therefore encouraging them to cross in the designated crosswalk areas.

#### 2.2.5 Street Trees

Street trees play a large role in the character and identity of the streets they line in Konterra Town Center East. Required to be spaced no less than 30 - 45 feet apart on center, they offer shade in the hot summer months and in the winter months they shed their leaves and permit more light to filter to the street level. Street trees can also be used to differentiate between streets by placing varied species on different streets as location indicators. By varying the type of street tree on each street it is possible to build a rhythm and pattern throughout the site that is consistent in placement and function but visually dynamic.

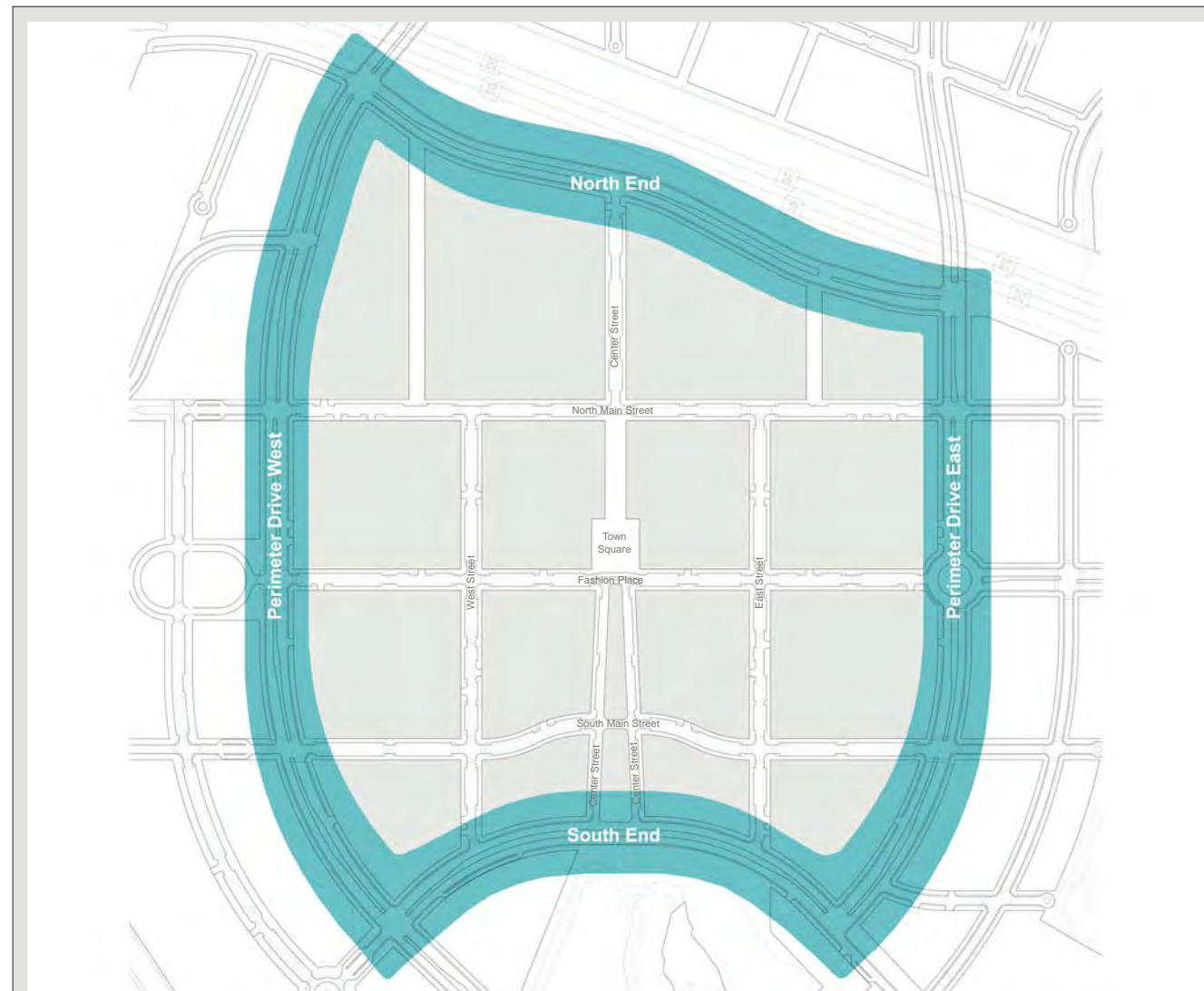
#### 2.2.6 Planters/Landscaping

Typically a four foot six inch landscaping zone is provided throughout Konterra Town Center East. This zone is located between the curb and the sidewalk providing a continuous landscaped strip in some areas and a more intermittent landscaping zone in other areas. This zone will house urban landscape features like tree grates, planters, lighting and signage, and creates a buffer between pedestrians and vehicles. This zone is critical for providing vegetation and open space throughout the town center and offers the opportunity to incorporate nature into the development.





## 2.3 Perimeter Roads



Overall plan showing perimeter road locations.

### Perimeter Roads:

The Perimeter Roads provide the major circulation to and from Konterra Town Center East forming essentially a ring road around the downtown, facilitating higher speed traffic and separating it from the lower speed traffic inside the core. Within the development these roads will accommodate the largest traffic volumes and therefore will be the largest in scale. The perimeter roads will contain multiple thru lanes in each direction as well as turning lanes to expedite left hand turns that would otherwise slow traffic. Bicycle lanes will be incorporated to encourage the multi-modal environment of downtown Konterra Town Center East and to connect the downtown to the larger recreational network.

### Intent: First Impression / Ease of Access

The perimeter roads are the front door to the downtown core and will give patrons their first impression of Konterra Town Center East and must therefore be designed with care. They will contain the nodes of access and the gateways into the project and must therefore be consistent with, and indicative of, the character of Konterra Town Center East.

The Perimeter Roads of Konterra Town Center East are: North End, South End, Perimeter Drive West and Perimeter Drive East. These roads cre-

ate essentially a bypass around the site allowing traffic to flow easily around as well as into the downtown at strategic points. The perimeter roads act as a conduit between the higher speeds of the interstate and ICC and the lower speed pedestrian oriented realm of the main streets in the downtown core; they therefore become a hybrid of the two different road types.

Medians will play a large role in the character of the perimeter roads giving them the feel of landscaped boulevards rather than purely access roads. These medians will not only serve as areas for landscaping and place making but also as safety devices to protect pedestrians crossing the street and to buffer vehicles traveling in opposite directions. Medians will vary in width to accommodate necessary turning lanes, and will contain various types of landscaping elements such as trees, shrubs, hard and softscape as well as signage and lighting.

### Recommendations:

- Typical R.O.W. will be 100'
- Medians will be incorporated and will vary in size but, will be no less than 13' in width at any given point.
- Left hand turning lanes will be provided where necessary to expedite traffic around the site. Turning lanes will be no less than 11' in width.
- Drive lanes will be no less than 11' in width.
- 4' minimum bike demarcations will be provided to encourage pedestrian and multi-modal use except where infeasible.
- Sidewalks will be no less than 5' in width at any given point.
- A minimum 4' planting area will be provided where possible and will contain urban elements such as trees, lighting, landscaping, signage/graphics etc.
- Trees will be spaced every 30' - 45' on center or less.
- Lighting will be spaced and sized to accommodate both pedestrians and vehicles.
- Crosswalks will be provided to encourage safe pedestrian movement across the perimeter roads.
- East/West access across the perimeter roads is encouraged to facilitate connections with development outside the downtown core.



Landscaped Boulevard Median



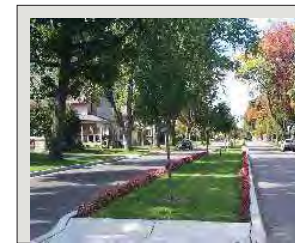
Boulevard Crosswalk



Landscaped Median



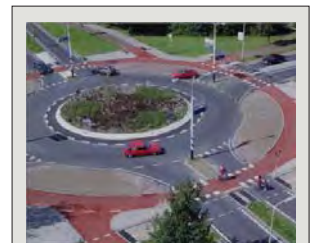
Landscaped Median



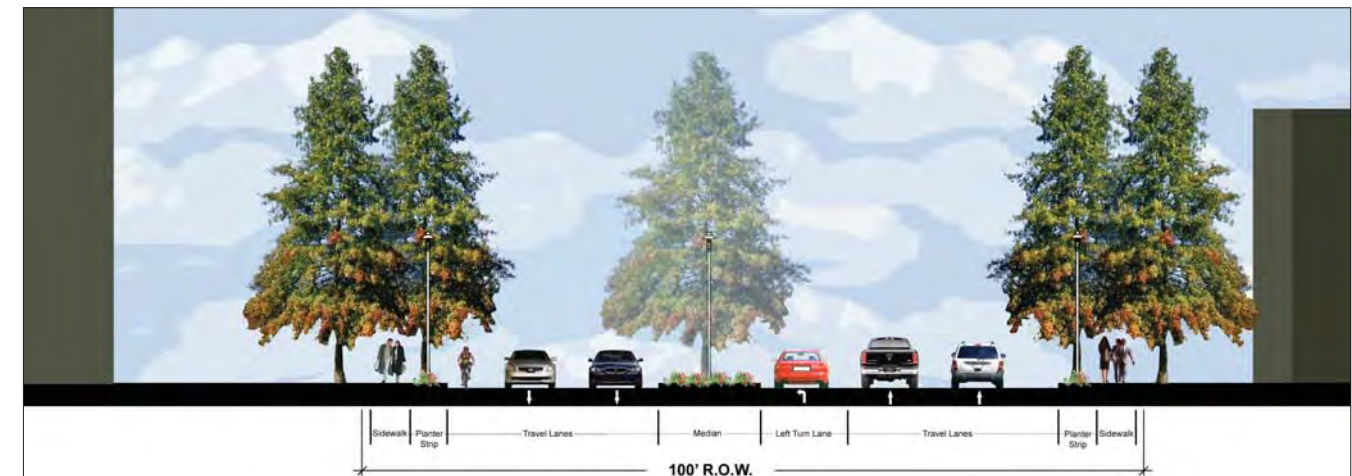
Landscaped Median



Boulevard



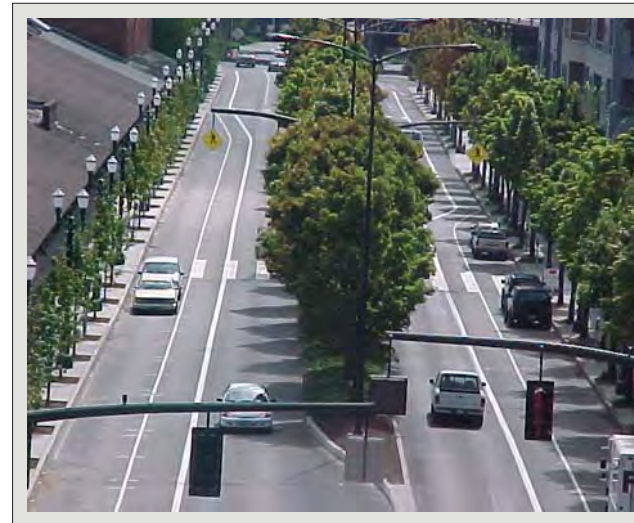
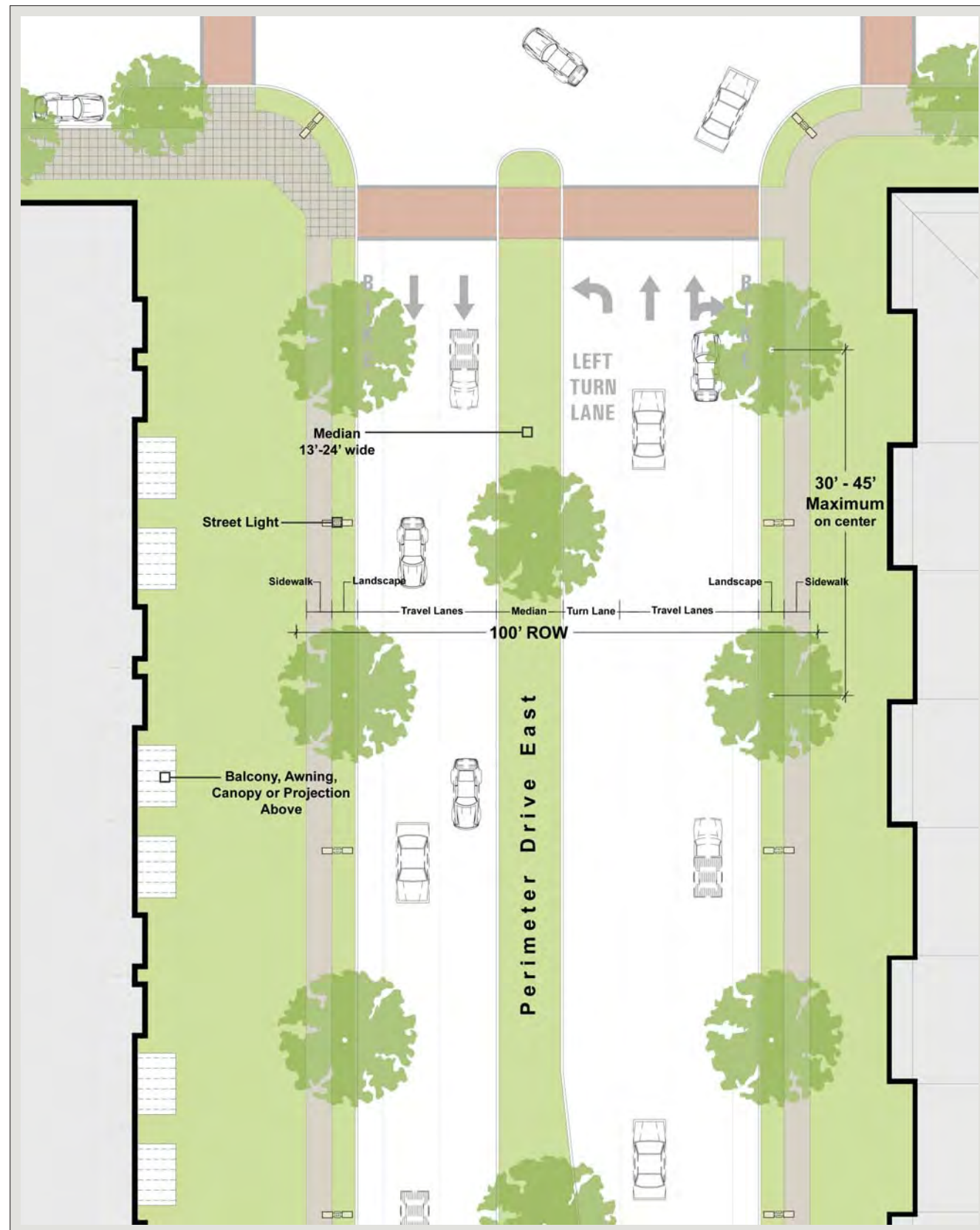
Roundabout



Perimeter Drive East Section



## 2.3 Perimeter Roads



There are four roads that make up the perimeter roads of Konterra Town Center East: North End, South End, Perimeter Drive East and Perimeter Drive West. All of these roads are in place to facilitate commuters to, from, and around the downtown core. These roads are sized to handle higher volumes of traffic at faster speeds than the downtown Main Streets.

The majority of cars on these roads will arrive from the I-95 and the Inter-county Connector and will be filtered into the site through 3 major entry points. The first entry point is located at the intersection of North End and Perimeter Drive West and feeds from Contee Road to the north of the site. This entry will likely be the most heavily used because it is convenient for both the people traveling to the downtown as well as those working in the office park planned to develop directly west of downtown.

The second entry point will occur at the intersection of North End and Perimeter Drive East and also feeds directly from Contee Road. This entry point will be secondary and will likely serve the local community as well as the majority of the new residential development both in the core and the area east of downtown.

Perimeter Drive East will also contain a roundabout where it intersects



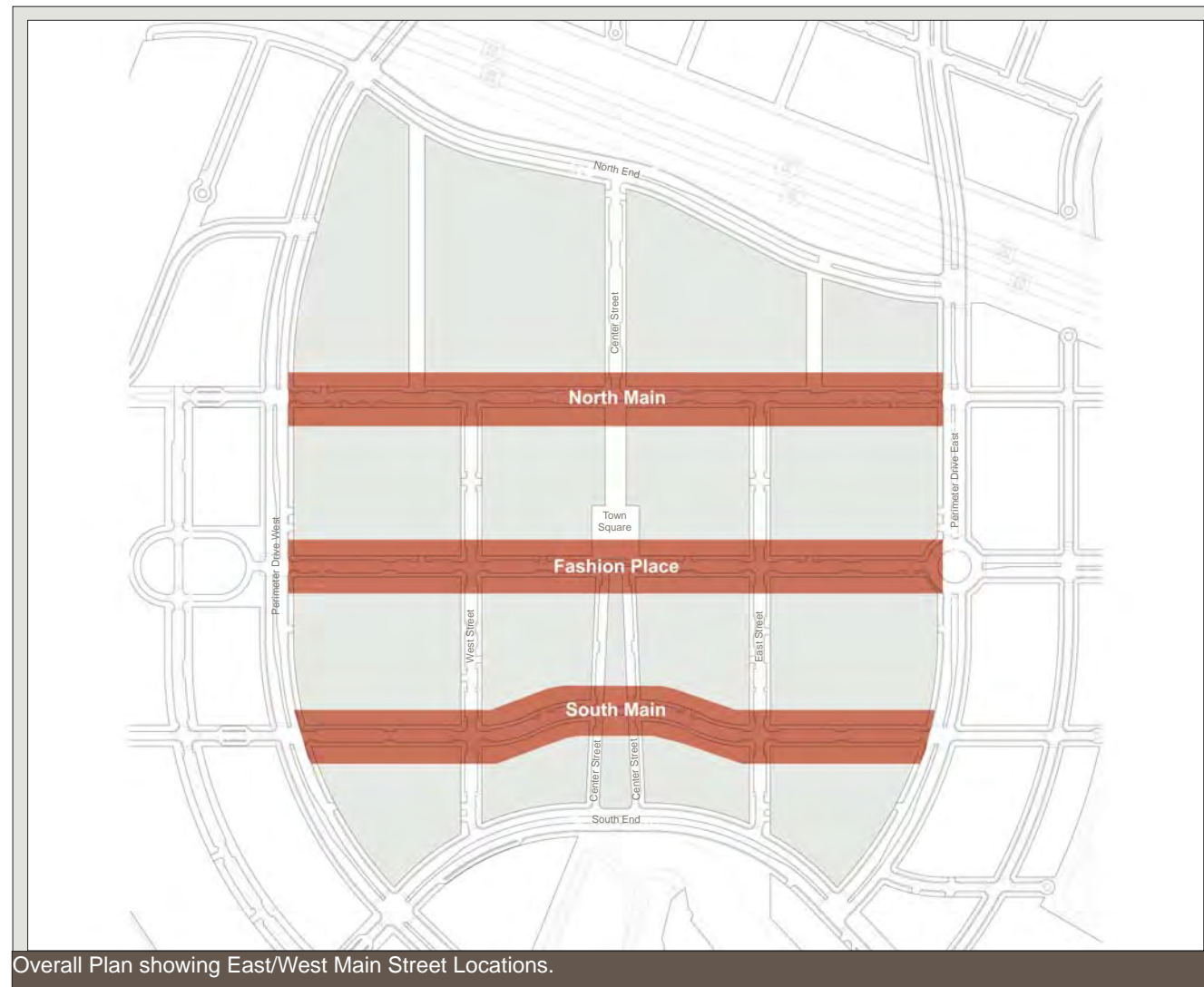
with Fashion Place. This marks the third major entry into the downtown. This third entry point on the east side of the site and will become integral when the Intercounty Connector is in place. This entry will accommodate all the traffic that arrives from the ICC and filters in from Kenilworth Avenue extended/route 201. The roundabout located at the entrance to the downtown is essential to gather/disperse cars effectively allowing traffic to continue moving while also serving as a landmark, gateway and way-finding device.

The perimeter roads serve two main functions: to move large numbers of cars around the perimeter of the site as well as into the downtown, and to create and mark the gateways into Konterra Town Center East. These roads must be both functional and attractive as they represent the initial impression of the character of Konterra Town Center East. Signage and lighting will play a vital role as far as navigation is concerned, and landscaping will be integral in expressing the visual character of the project.



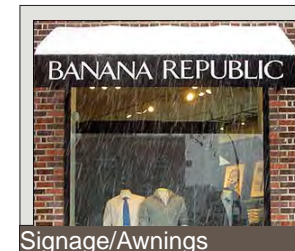
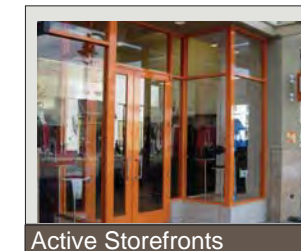
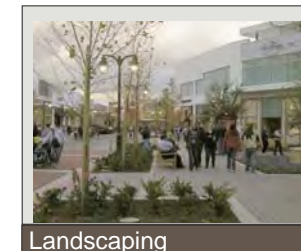
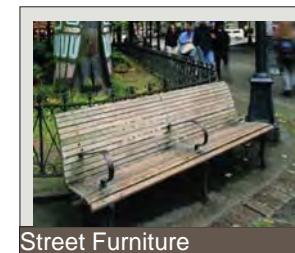
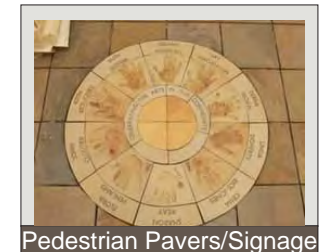
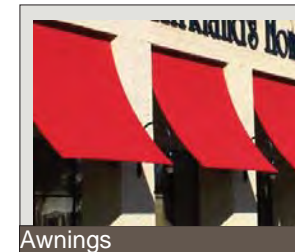
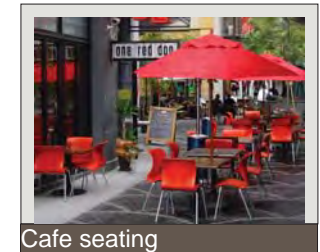


## 2.4 Main Streets - East / West



### Recommendations:

- Drive lanes will be no less than 11' in width
- 4' minimum bike demarcations will be provided except where infeasible.
- On street parallel parking will be provided where possible; minimum stall size: 8' x 22'
- Sidewalks including landscape strip will be no less than 15'-0" from the face of curb.
- A minimum 4' planting area will be provided intermittently and will contain planters, trees, street furniture, lighting, etc.
- Trees will be spaced every 30' - 45' Maximum on center where possible.
- Lighting will be spaced and sized to accommodate both pedestrians and vehicles.
- Canopies and awnings will overhang sidewalks to provide weather protection
- Building face to building face distance will be 76'-0" at minimum
- Bicycle racks will be provided at convenient locations



### Main Streets - East/West

Circulation through the Downtown Core of Konterra Town Center East will take place on a series of "Main Streets" that will be sized and designed to engage and facilitate both pedestrian and vehicular movement safely and efficiently throughout the development. Main Streets are characterized by their urban form, pedestrian oriented amenities and framework of design principles creating a dialogue between the streetscape and the buildings that front them. The major accesses into the downtown occur at the intersections of the East/West Main Streets and the Perimeter Roads. The East/West Main Streets that cross the site are level rather than sloped and host the major entrances to the anchor retail stores. For these reasons the East/West Main Streets are the primary circulation routes within Konterra Town Center East.

#### Intent: Comfortable Neighborhood Scale

Main Streets are intended to be very pedestrian friendly to reinforce the notion of 'parking once' then navigating the site by foot. Many amenities will be in place to ensure the safety and comfort of pedestrians such as wide sidewalks, awnings and canopies, crosswalks, bollards, lighting, trees etc. Where pedestrians and cars do interact, such as curb cuts and intersections, special care will be given to details like paving and

signage to ensure that each is aware and conscious of the other to create the safest environment possible for both.

Main Streets will be designed to a comfortable width for both vehicles and bicycles making navigation easy and enjoyable, while at the same time functional for emergency and service vehicles. Parallel on-street parking on both sides of the street will not only offer convenience to patrons but will also act as a street calming device and buffer between pedestrian and vehicular movement.

The character of the Main Streets will be defined not only by the buildings that line them but especially by the design and detailing of the streetscapes they front. Street furniture, lighting, landscaping and trees will play a vital role in giving Konterra Town Center East its identity and will provide the comforts needed to encourage an urban mixed-use sense of place.





## 2.4 Main Streets - East / West



- **Paving:** Special pavers should be used to indicate areas where high levels of pedestrian activity are expected and to guide pedestrians through areas that are multi-modal.
- **Tree Guards:** Trees placed in tree grates should have a vertical lip around the edge to protect trees in heavy traffic areas.
- **Newspaper Racks:** Should be located in a common area and should not interfere with pedestrian movement and access.



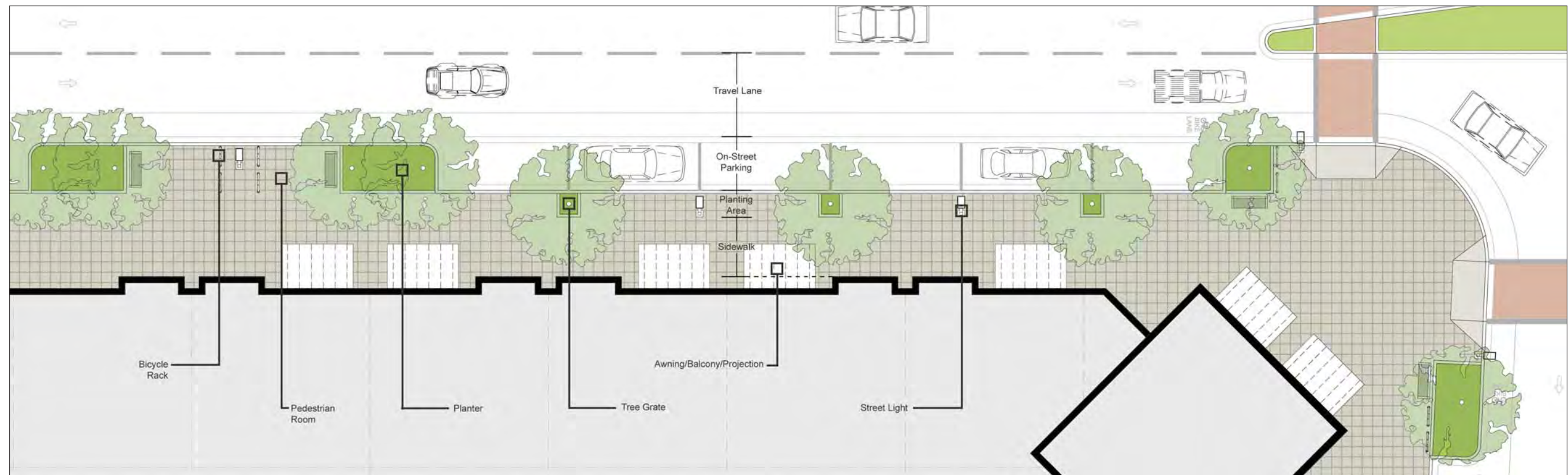
- **Bicycle Racks:** Should be located along street facing facades near entries so that they are convenient and visible to users but do not obstruct access or circulation.
- **Trash Receptacles:** Throughout the Core/Downtown Area, trash receptacles should be provided.
- **Bollards:** may be used to restrict vehicle access in locations where pedestrians and vehicles are expected to mix. Design of bollards should be consistent with other street furnishings.



- **Seating:** Should be provided where possible and may include benches, low walls, planter/bench combos etc. Cafe tables and seating should be allowed in the sidewalk and landscaping zones where appropriate.
- **Buffer and Screening:** Konterra Town Center should be landscaped with a variety of hard and softscape features that relate to the different street types. Trees and other screening devices should be used to buffer, enhance, and screen parking areas and structures as well as trash enclosures from public view.

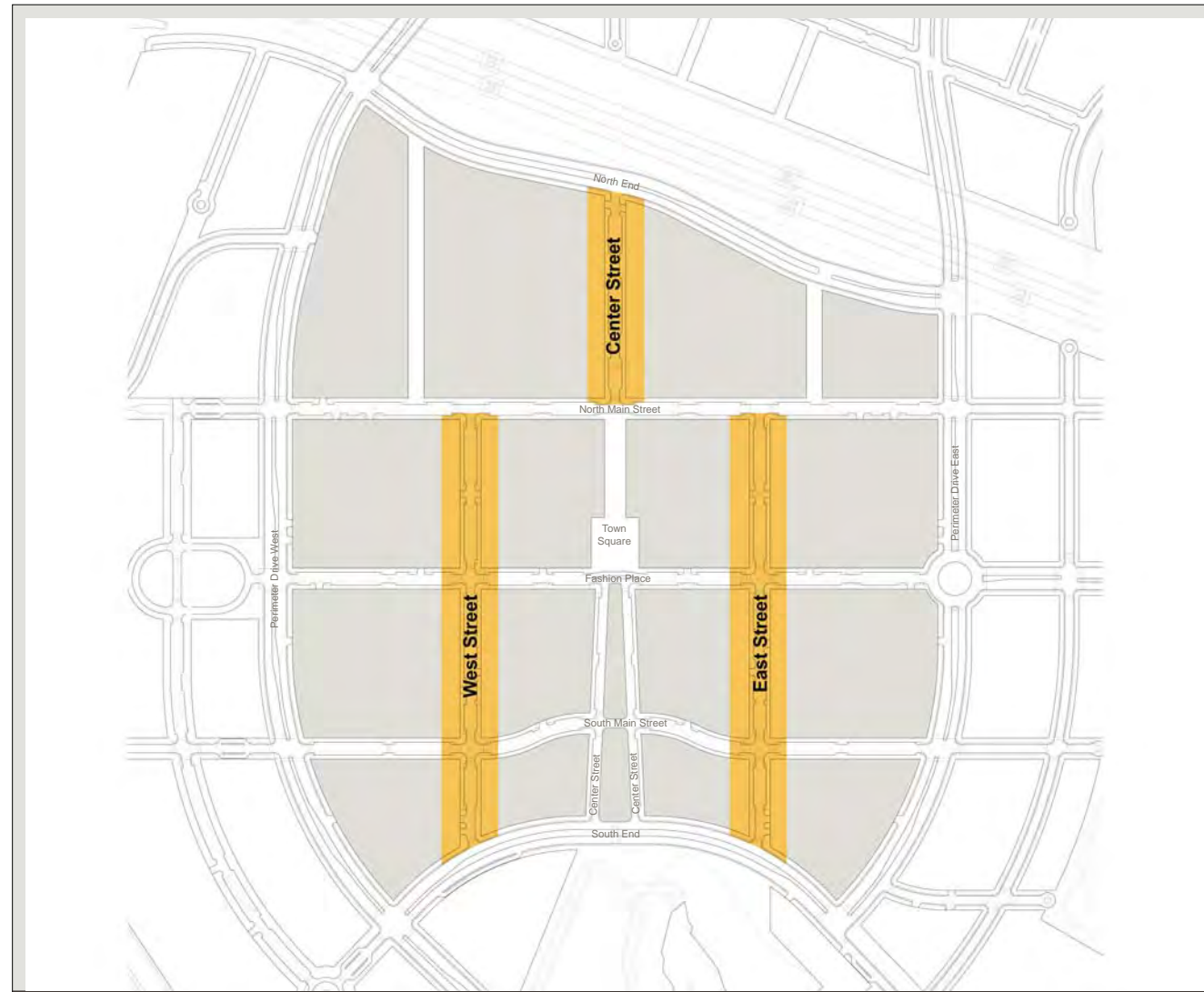


- **Treatment:** In commercial areas sidewalks are likely to be predominantly hardscape, with generous tree grates around street trees.
- **Tree/Landscape Variety:** One way to distinguish between streets is to use different varieties of trees and landscaping to enhance the character of each streetscape.





## 2.5 Main Streets - North/South



Overall Plan showing North/South Main Street Locations.

### Main Streets - North/South:

Circulation through the Downtown Core of Konterra Town Center East will take place on a series of "Main Streets" that will be sized and designed to engage and facilitate both pedestrian and vehicular movement safely and efficiently throughout the development. Main streets are characterized by their urban form, pedestrian oriented amenities and framework of design principles creating a dialogue between the streetscape and the buildings that front the streetscape. There are three north/south Main Streets in Konterra Town Center East: West Street, Center Street and East Street. The main streets running north/south differ from those going east/west in two fundamental ways. The most noticeable and visual difference is that the north/south main streets slope to accommodate the natural topography of the site. The second distinguishable characteristic of the north/south main streets is that they do not completely transect the downtown, but rather flow into the east/west main streets creating framed views at their termini.

#### Intent: Neighborhood Scale / Connection

The north/south Main Streets are intended to be very pedestrian friendly to reinforce the notion of 'parking once' then navigating the site by foot. Many amenities will be in place to ensure the safety and comfort of pedestrians such as wide sidewalks, covered walkways, crosswalks, bol-

lards, lighting, and trees to name a few. Where pedestrians and cars do interact, such as curb cuts and intersections, special care will be given to details such as paving and signage to ensure that each is aware and conscious of the other to create the safest environment possible for both.

The north/south main streets will be designed to a comfortable width for both vehicles and bicycles, making navigation easy and enjoyable while at the same time functional for emergency and service vehicles. Parallel on-street parking on both sides of the street will not only offer convenience to patrons but will also act as a street calming device and buffer between pedestrian and vehicular movement.

The character of the Main Streets will be defined not only by the buildings that line them but especially by the design and detailing of the streetscapes they front. Street furniture, lighting, landscaping and trees will play a vital role in giving Konterra Town Center East its identity and will provide the comforts needed to encourage an urban mixed-use environment.

### Recommendations:

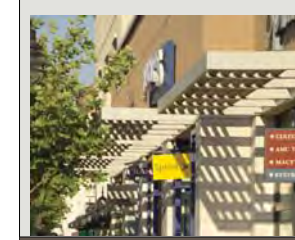
- Drive lanes will be no less than 11' in width
- 4' minimum bike demarcations will be provided except where infeasible.
- On street parallel parking will be provided where possible; minimum stall size: 8' x 22'
- Sidewalks with landscape strip will be no less than 15'-0" measured from the face of curb.
- A minimum 4' planting area will be provided intermittently and will contain planters, trees, street furniture, lighting, etc.
- Trees will be spaced at a minimum every 30' - 45' on center where possible.
- Lighting will be spaced and sized to accommodate both pedestrians and vehicles.
- Canopies and awnings will overhang sidewalks to provide weather protection
- Building face to building face will be at minimum 76'-0"
- Bicycle racks will be provided at convenient locations



Tree Grate



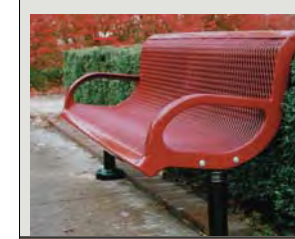
Stone Pavers



Awnings



Pedestrian Pavers/Signage



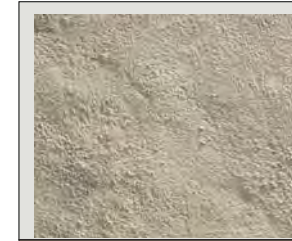
Street Furniture



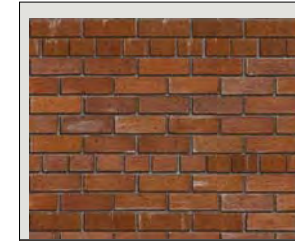
Street trees and planters



Rough stacked stone



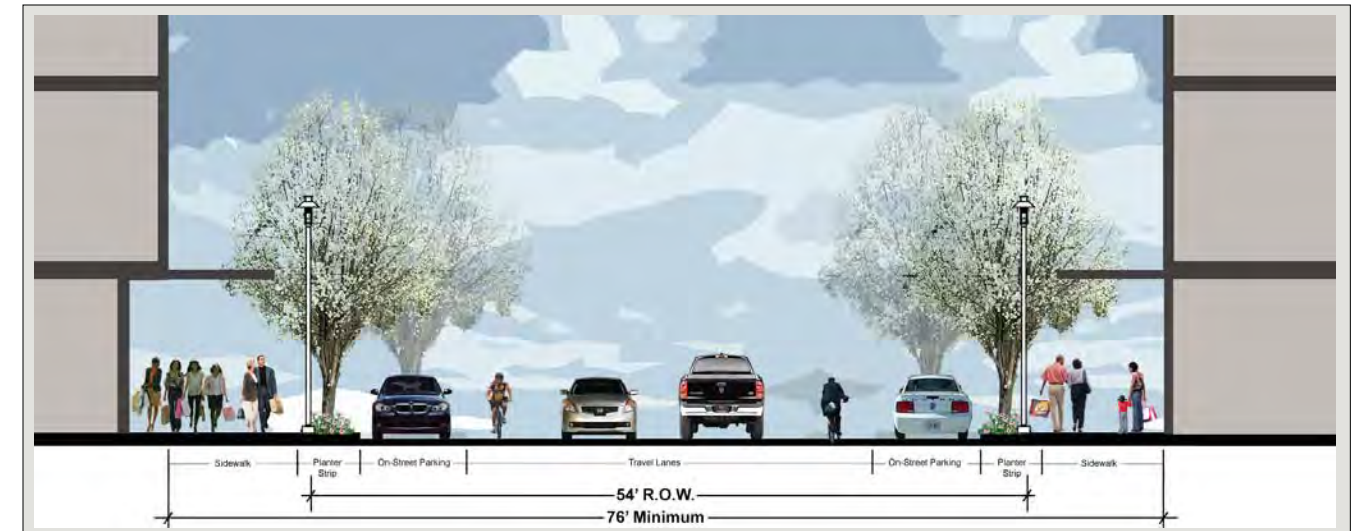
Stucco



Brick

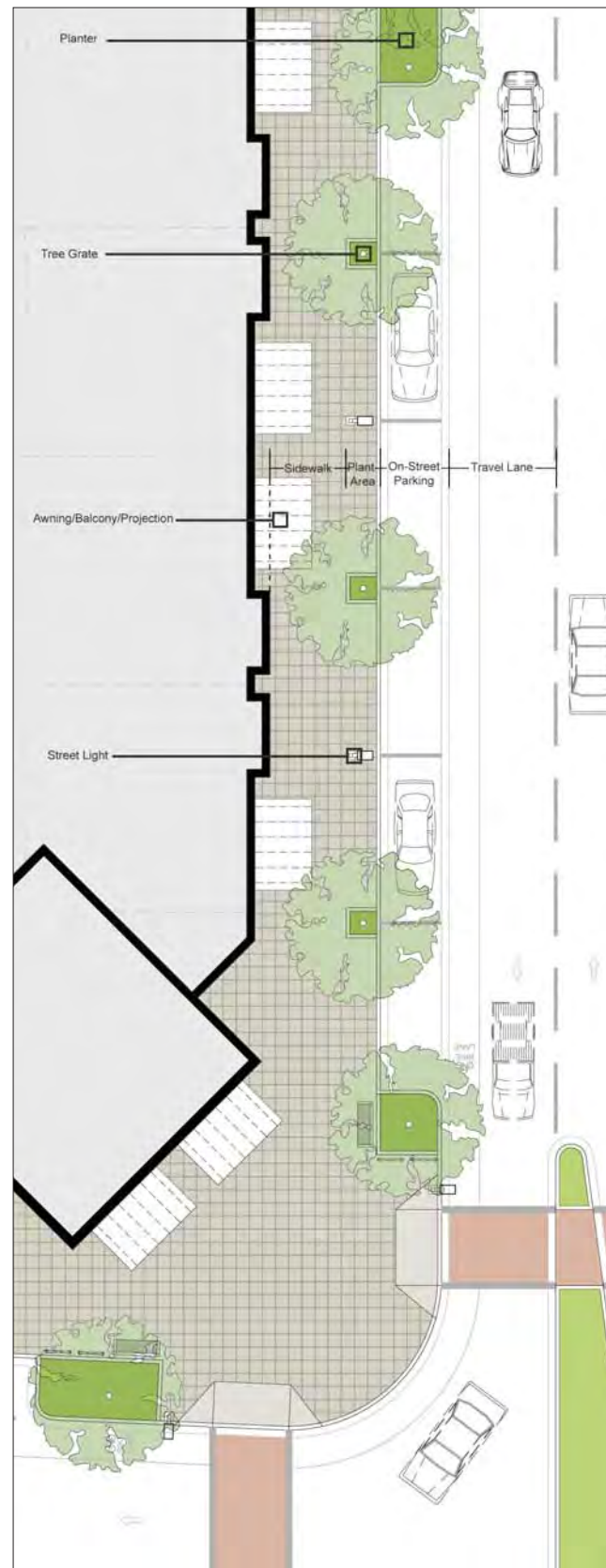


Stone





## 2.5 Main Streets - North/South



**North/South Main Streets:** The downtown of Konterra Town Center East contains two types of 'Main Streets', east/west and north/south. The north/south main streets are discernible by their slope which reacts to the natural topography of the site. The north/south main streets provide mainly secondary circulation as they do not traverse the downtown completely.

There are four opportunities to enter the site on the north/south streets from the perimeter roads. Three of these opportunities occur along South End where it intersects West Street, Center Street and East Street. The fourth opportunity, and the only site entry from the north of the site is where North End intersects Center Street. All north/south main streets terminate at North Main, for this reason the north/south main streets are secondary circulation and act as linkages between more primary streets.

Because the topography of the site dictates that the north/south streets be sloped it therefore follows that the buildings that line them will also need to react to the site's contours. It is likely that this sloping will provide natural breaks in the buildings' parapet lines creating interesting and dynamic facades.

The north/south main streets will be scaled to accommodate both pedestrians and vehicles. Bicycle lanes will be incorporated where possible to support a multi-modal lifestyle. Wide sidewalks with street furniture and canopy trees will facilitate pedestrian use and comfort. On-street parking

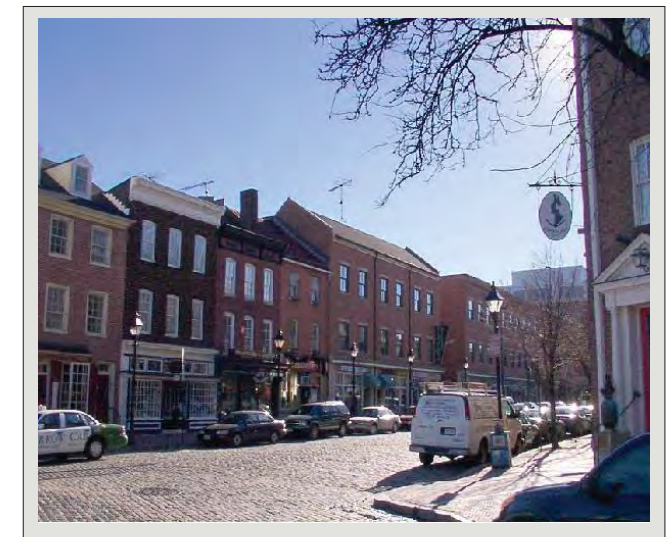


will also be provided to offer both shopping convenience as well as traffic calming benefits creating a safer, more favorable environment.

The majority of parking garage entrances will likely be located on the north/south main streets which means that the majority of people coming to the site by car will be routed along these streets. This will foster higher volumes of traffic but at slower speeds which is ideal for retailers as well as pedestrians. Locating the pedestrian and vehicular parking structure entrances along these streets encourages activity takes place here in addition to the more major east/west main streets.

The north/south streets will primarily contain smaller retailers and specialty shops. To create vistas and encourage pedestrian movement along the north/south streets, two of the large anchor stores are set at the northern terminus of East Street and West Street, while the other two are placed at the east and west ends of Fashion Place. By locating the major anchors in these locations the north/south streets become integral to the circulation between the anchors and encourage pedestrians to travel along them.

The north/south main streets will host all the different uses within the site: retail, residential, office and recreational. Similar to the east/west main streets the north/south main streets will be characterized by their own signature tree type. This will be one way of identifying different streets while also providing a means of way-finding to both pedestrians and motorists.



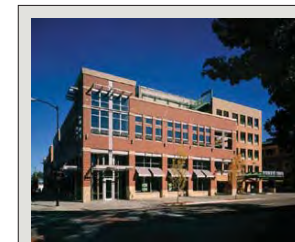
Street Furniture



Street trees and planters



Entryway Awning



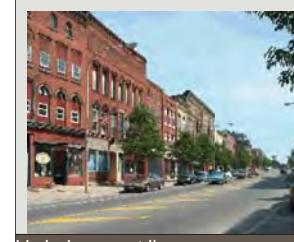
Parking Deck Entrance



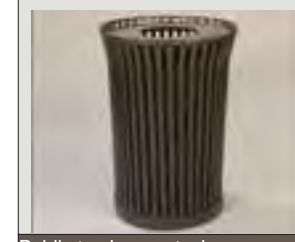
Brick



Stone



Varied parapet line



Public trash receptacles





## 2.6 Center Street Corridor



Overall plan showing Center Street corridor

### Center Street Corridor :

The Center Street Corridor comprises the central north/south axis of Konterra Town Center East connecting all east/west streets and linking the perimeter roads from North End to South End. As the only continuous north/south route through the project it is made up of the most diverse circulation modes including a typical main street, a one-way couplet, and a pedestrian only passage through Town Square. The Center Street Corridor visually and physically links the major open/civic spaces of the project creating a view corridor that links Konterra Town Center East to the surrounding community. The following pages of Section 2.6 establish design parameters for the Center Street Corridor, inclusive of the Promenade, Town Square and Pedestrian Passage. The various design elements and wind analysis if warranted, will be provided in specific detail at time of permit site plan. Additional information relative to signage and lighting along the Center Street Corridor can also be found in Sections 6.5 and 7.6 of these Design Standards. Weather protection (see also 3.4) for the Center Street Corridor is an integral element of building placement and design. Shadow analysis has been conceptually determined by a Sun Study (see 2.6.2) reviewing potential building placement around the Center Street Corridor allowed pursuant to the

DSP. Permit site plans will propose appropriate building heights which design has anticipated prevailing winds from available data. When combined with attention to building massing and street level landscaping, the design will negate the creation of substantial wind tunnel effects. In this manner, the Center Street Corridor will provide for a comfortable pedestrian environment that is both functional and enjoyable.

### Intent: Linkage, View, Gathering

The Center Street Corridor takes advantage of the natural sloped topography of the site using its wedge shape to create a vista linking the heart of Konterra Town Center East to the natural wetland to the south of the downtown core.

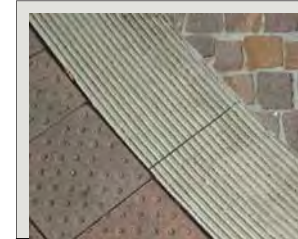
The Promenade, a one-way couplet located between South End and Fashion Place, incorporates open space with retail and residential uses that gestures both into the center of Konterra Town Center East as well



Kiosk



Benches



Paving



Bollards



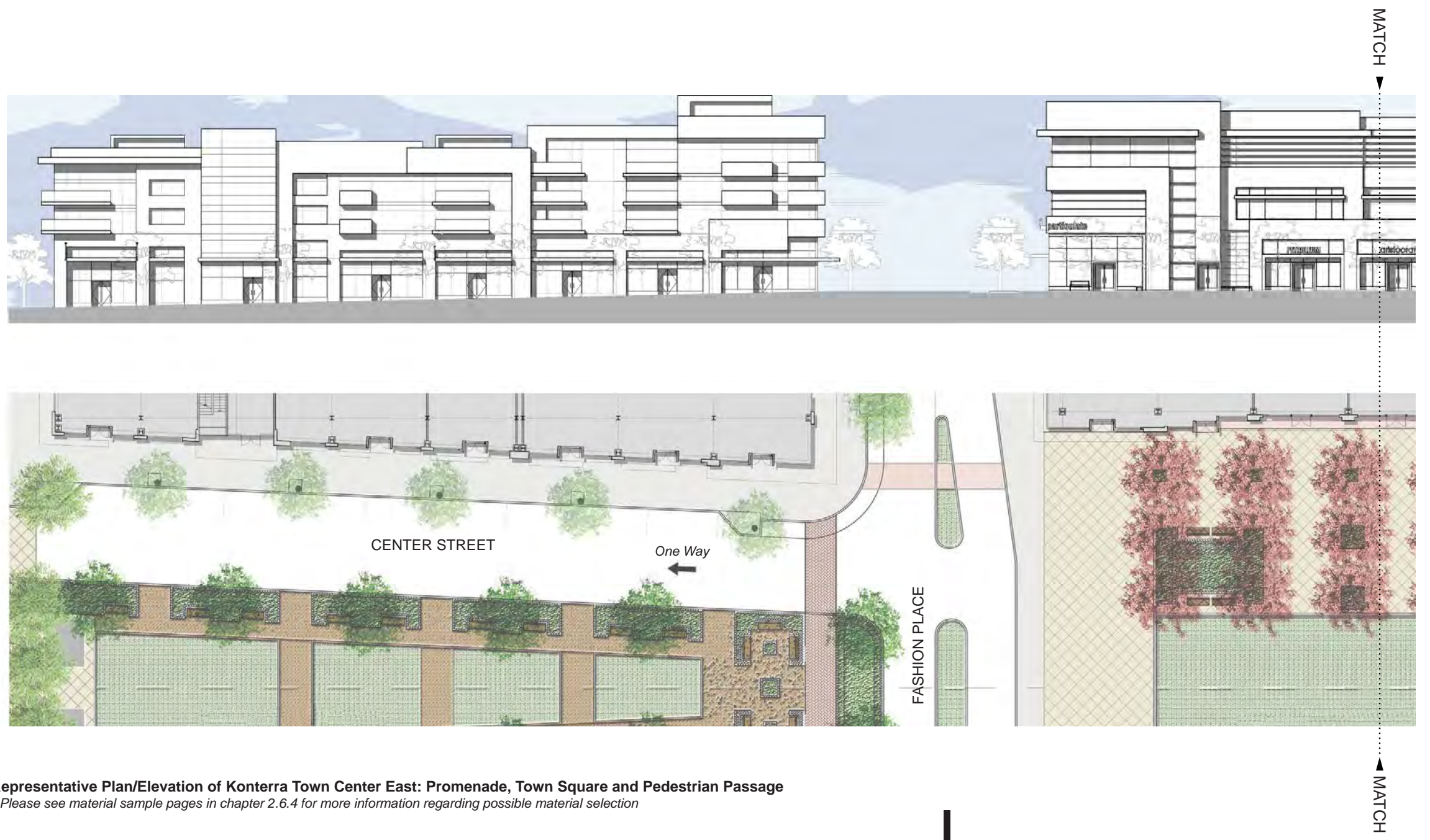
as out to the surrounding community. The wedge shaped open space of the promenade created by the one-way couplet provides an area for various activities from lounging and gathering to strolling and shopping.

As you continue north up the Center Street Corridor you come to Fashion Place where the Promenade ends and Town Square begins. Town Square is the geographical and civic center of Konterra Town Center East. Town Square provides a single location that comprises all the features of a civic plaza, community gathering space, central park, and shopping nexus into one space. Programmed to be an activity hub within Konterra Town Center East the Town Square will host a variety of program types as well as building typologies and landscape features to support its many uses.

The Pedestrian Passage connects Town Square to the south with North Main Street continuing the pedestrian only street within the project. This portion of the Center Street corridor hosts mainly retail shops with enhanced landscape features to engage and encourage walkability and activity.



2.6 Center Street Corridor



**Representative Plan/Elevation of Konterra Town Center East: Promenade, Town Square and Pedestrian Passage**  
*\* Please see material sample pages in chapter 2.6.4 for more information regarding possible material selection*

**PROMENADE**





**SQUARE**

**PEDESTRIAN PASSAGE**



## 2.6.1 Center Street Corridor - The Promenade



Overall plan showing Center Street promenade location.

### The Promenade:

The Center Street Promenade is a unique condition in Konterra Town Center East. It contains the only one-way couplet in the project as well as the only linear park, which connects the downtown of the project with the recreation and amenities located to the south of the site. The Promenade stretches from South End to Fashion Place where its vehicular access ends and it feeds into the Pedestrian Passage continuing the strong open space and pedestrian connection through the site.

### Intent: Open Space and Circulation

Center Street is composed of three different types of experiences.

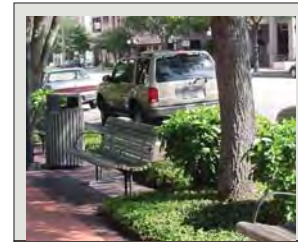
(1) The northern most section from North End to North Main is a typical north/south main street scaled to give an urban feel surrounded by multi-story office and residential over retail uses.

(2) From North Main to Fashion Place, Center Street transforms into an entirely pedestrian pathway, containing the "Town Square" at the very heart of the development. In this portion of Center Street the pathway is widened and the buildings are lower in scale offering a wide open space in the center of the urban fabric.

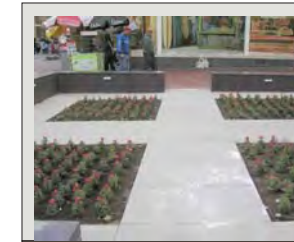
(3) The final stretch of Center Street from Fashion Place to South End is a third and different experience. The actual street is divided into a one-way couplet, each direction separated by a wedge of open space that forms a unique promenade condition opening it up to the land outside the downtown core and creating a formal gateway into the heart of the project.

The scale of the open space allows for a variety of activities to take place there, from normal shopping and circulating to larger events like festivals and outdoor concerts. The Promenade itself will be a combination of hard and softscaping lined with trees to provide both shade and protection from the elements and to frame the view into the Town Square.

Both the physical shape of the land and the designed landscaping of the promenade will reinforce its function of linking the downtown core of Konterra Town Center East with its adjacent neighbors. The Promenade creates a view corridor that frames both the Town Square and the larger open space south of the project reinforcing the idea that the Town Center is part of the larger Laurel Community not simply an enclave onto itself.



Street Furniture



Planters



Tree grates



Pedestrian pavers / signage



Seating



Street trees and planters



Stone Paver



Street Furniture

### Recommendations:

- The southern end of the Center Street Promenade will be a one-way couplet.
- Drive lanes will be no less than 11' in width
- 4' minimum bikes lanes will be provided where appropriate
- On street parallel parking will be provided where possible; minimum stall size: 8' x 22'
- Sidewalks including landscape strip will be at minimum 15'-0" measured from the face of curb
- Trees will be spaced at minimum every 30' - 45' on center where possible
- Lighting will be spaced and sized to accommodate both pedestrians and vehicles.
- Trees will be provided in the open space of the Promenade to provide shade and enhance the desired view corridor.
- Building face to building face will vary between Fashion Place and South End due to the wedge shape of the open space.
- Bicycle racks, street furniture and shopping kiosks will be provided at convenient locations



Street furniture

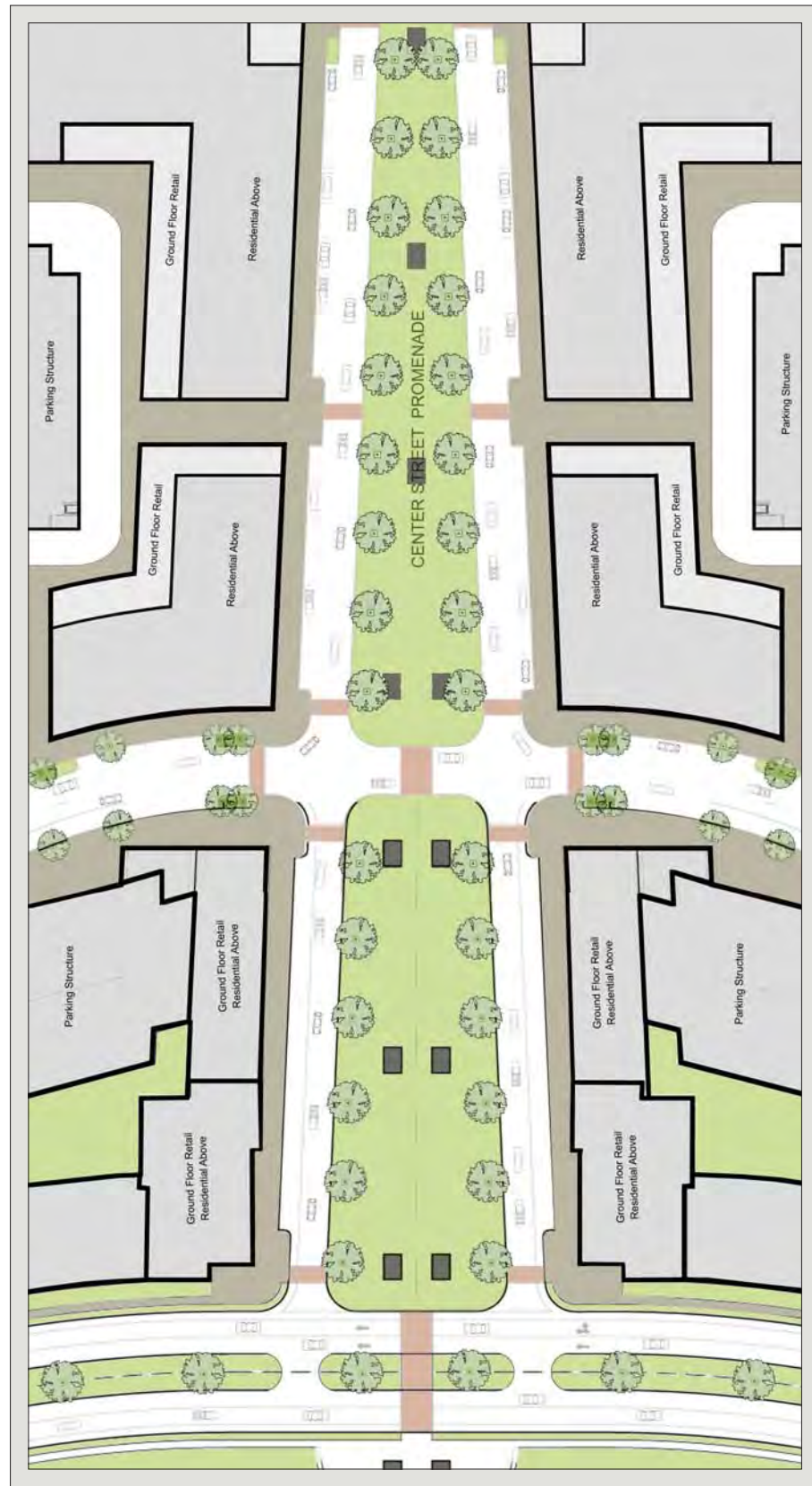


Kiosks





## 2.6.1 Center Street Corridor - The Promenade



### Center Street Promenade:

The Promenade will serve multiple functions in downtown Konterra Town Center East. As a larger open space the promenade will allow for a variety of activities to occur here from occasional fairs and concerts to daily gathering and relaxing. The promenade acts as both a circulation path, and a destination point within the core. Buffered on both the east and the west sides of the open space by one-way traffic lanes and on-street parking, the promenade is protected and centralized, a true focal point within the project. The images above represent similar situations of pedestrian circulation surrounded by vehicular movement.

Above on the left is an image of Bryant Park in New York City, a mixed softscape/hardscape open space that is used for relaxation and gathering. Above to the right an image of Las Ramblas in Barcelona shows a dominantly hardscape pedestrian open space that functions primarily for circulation and commercial use. In both cases the open spaces serve to connect two destinations through a unique experience.

The Center Street Promenade is likely to be a combination of the two examples above, melding the hardscape and softscape as well as the recreation and commercial uses.





## 2.6.1 Center Street Corridor - Promenade



**Residential over Retail Building Typology - Representative Pedestrian Passage Plan/Elevation**  
*\* Please see material sample pages in chapter 2.6.4 for more information regarding possible material selection*

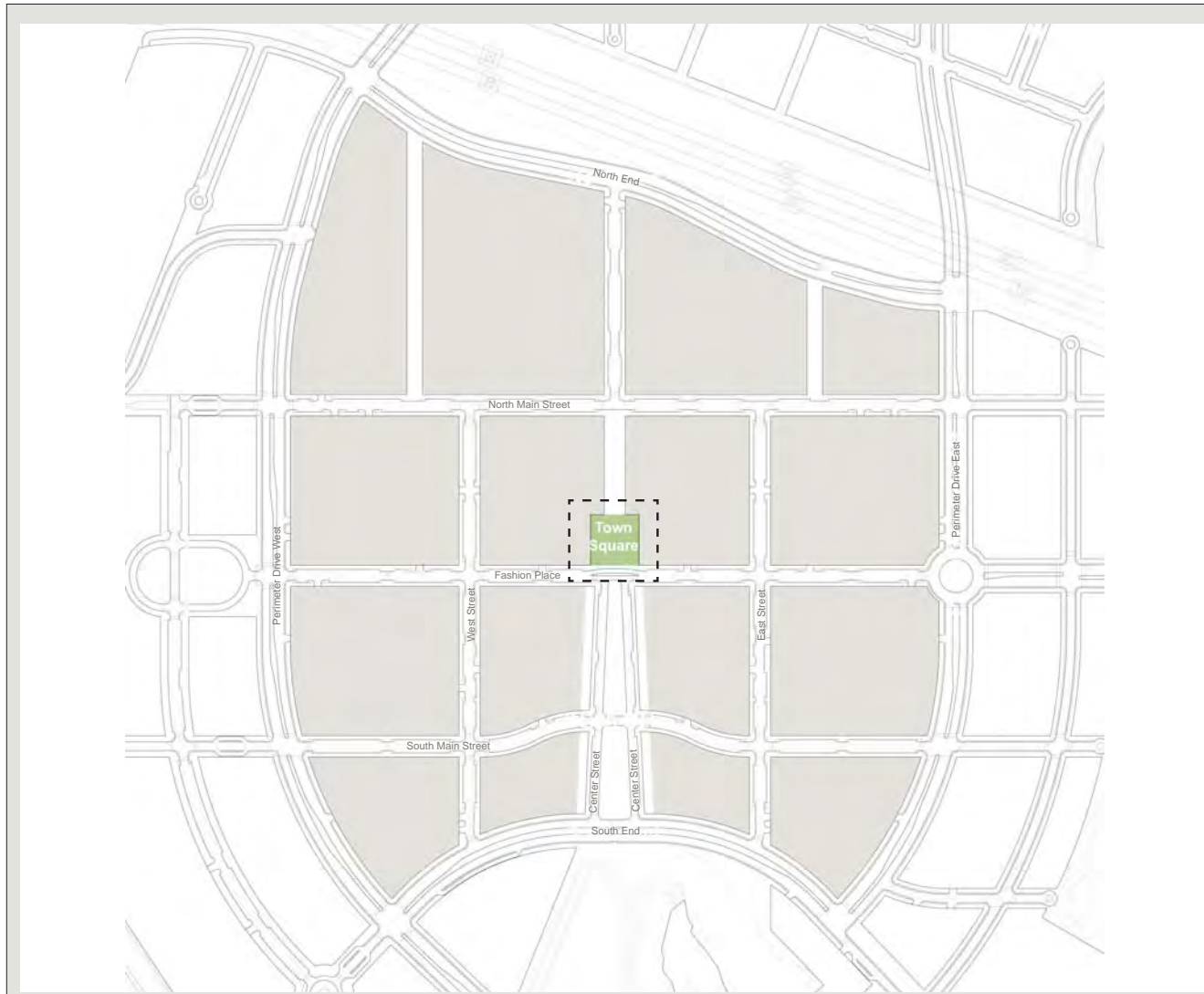
Scale: 1:25



Scale: 1:25



## 2.6.2 Center Street Corridor - Town Square



Overall plan showing location of Town Square.

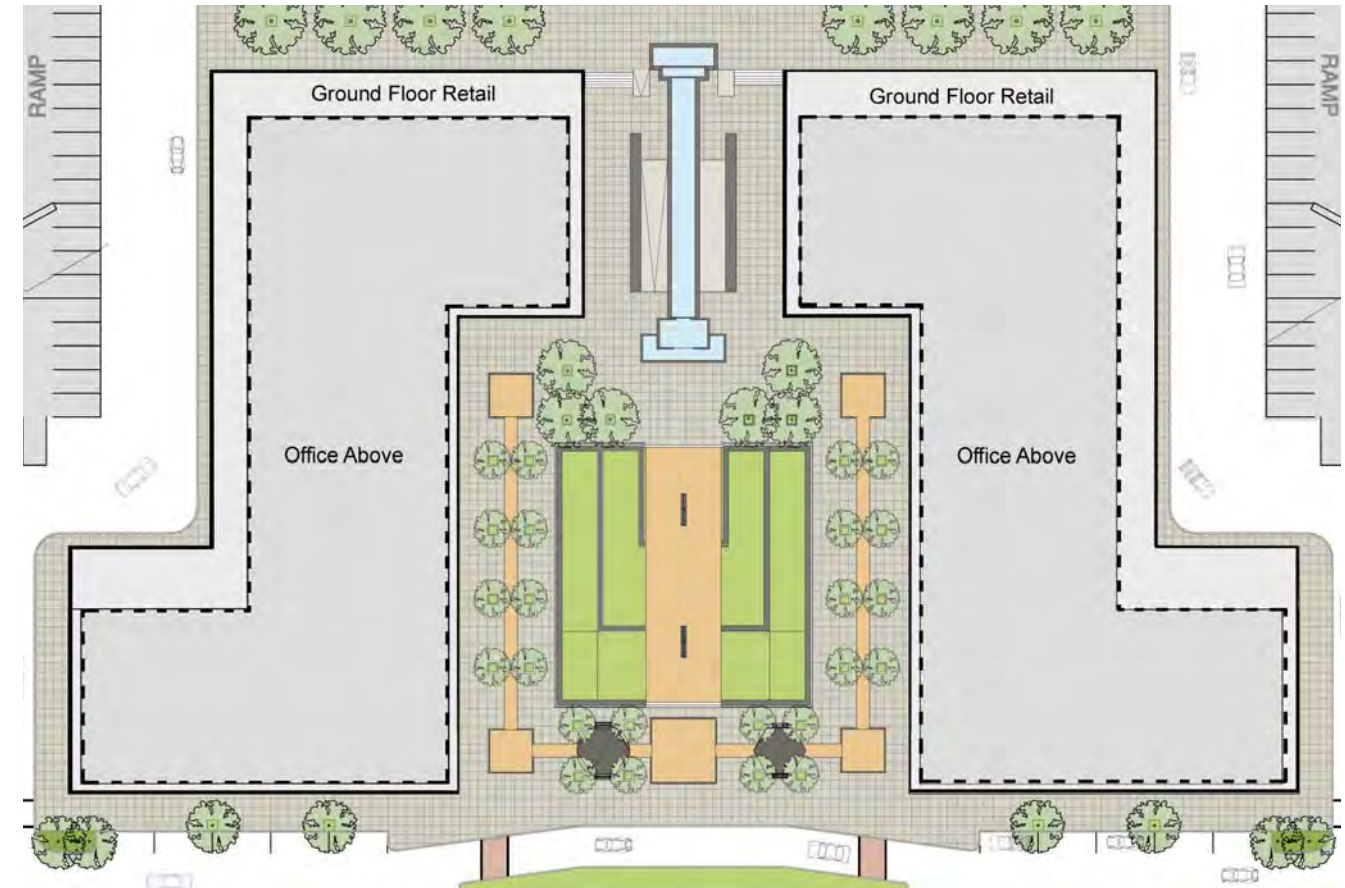
### Town Square:

Town Square is located in the heart of Konterra Town Center East. As part of the only permanent all-pedestrian street in the project, town square is both a circulation pathway as well as a gathering space. Lined with shops, offices, residences and open space the town square is a truly mixed-use environment that is unique in experience and character. As one of the large outdoor open spaces in Konterra Town Center East it is easy to imagine the many uses that this space could be put to, from outdoor dining cafes, to seasonal festivals, art expositions and music concerts. While catering to the shopper, worker and resident the town square provides a needed oasis for relaxation and recreation.

Water will be a major element incorporated into the design of Town Square. A terraced fountain will run the length of the pedestrian passage culminating in the town square which will be a mixture of hard and softscape features, outdoor seating and kiosks. The size of the pedestrian passage allows for multiple experiences to take place in the same general area providing a unique venue and a true urban lifestyle.



Upper level view of Town Square





# 2.6.2 Center Street Corridor - Town Square



**Office over Retail Building Typology - Representative Town Square Plan/Elevation**  
*\* Please see material sample pages in chapter 2.6.4 for more information regarding possible material selection*

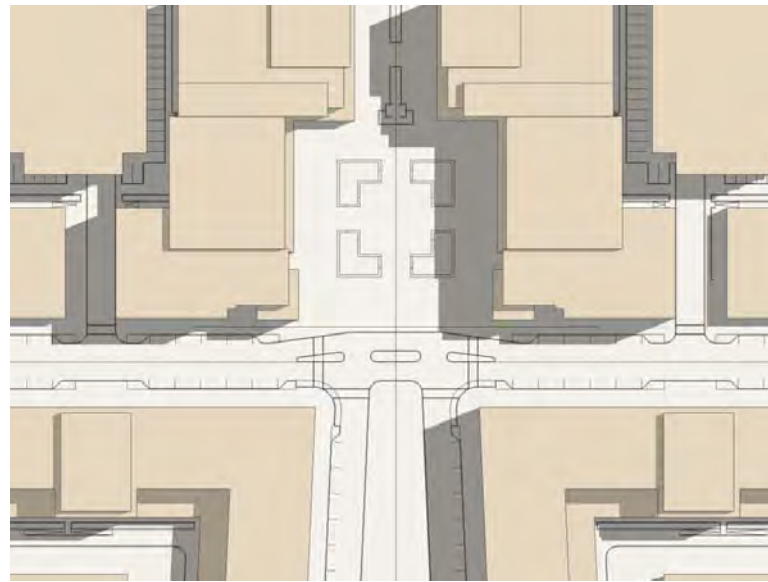
Scale: 1:25



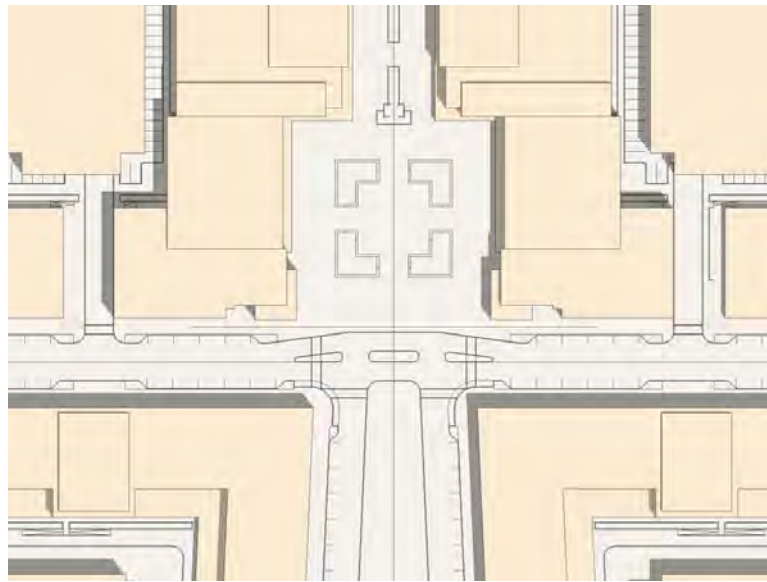
Scale: 1:25



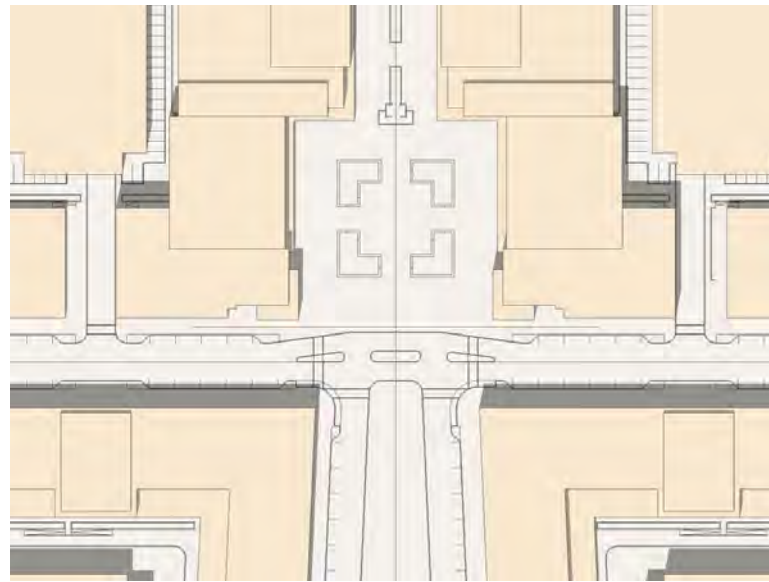
# 2.6.2 Sun Study - Center Street Corridor



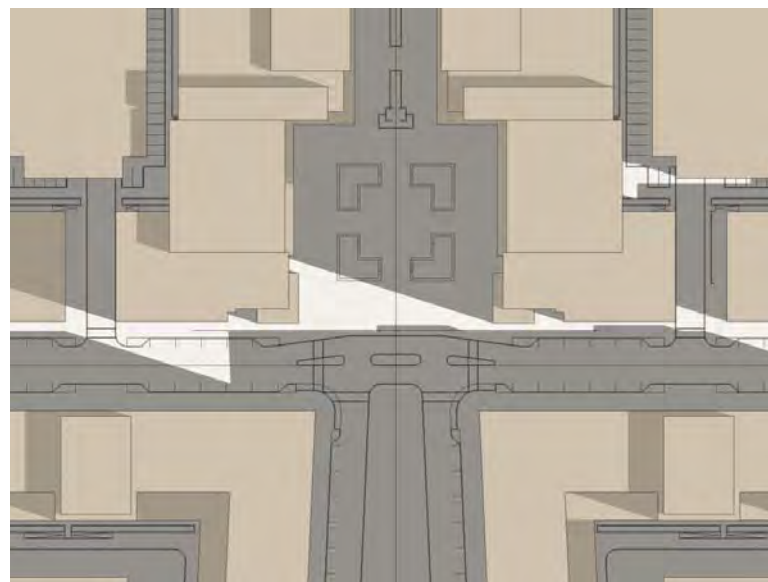
June 21\_09.00 AM



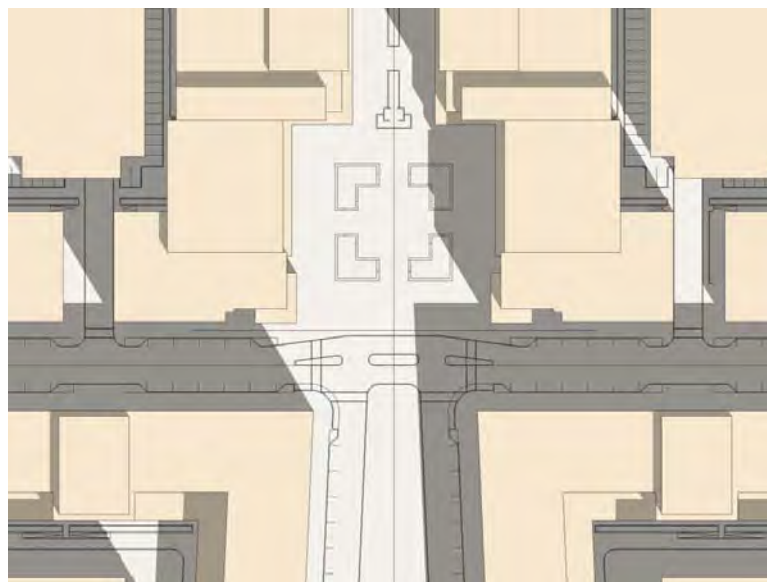
June 21\_12.00 PM



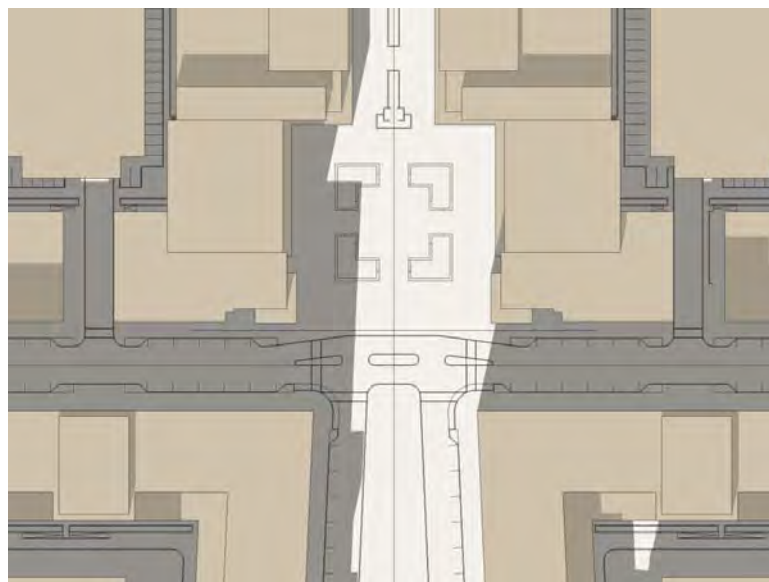
June 21\_03.00 PM



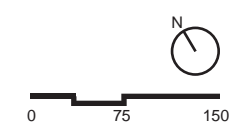
December 21\_09.00 AM



December 21\_12.00 PM

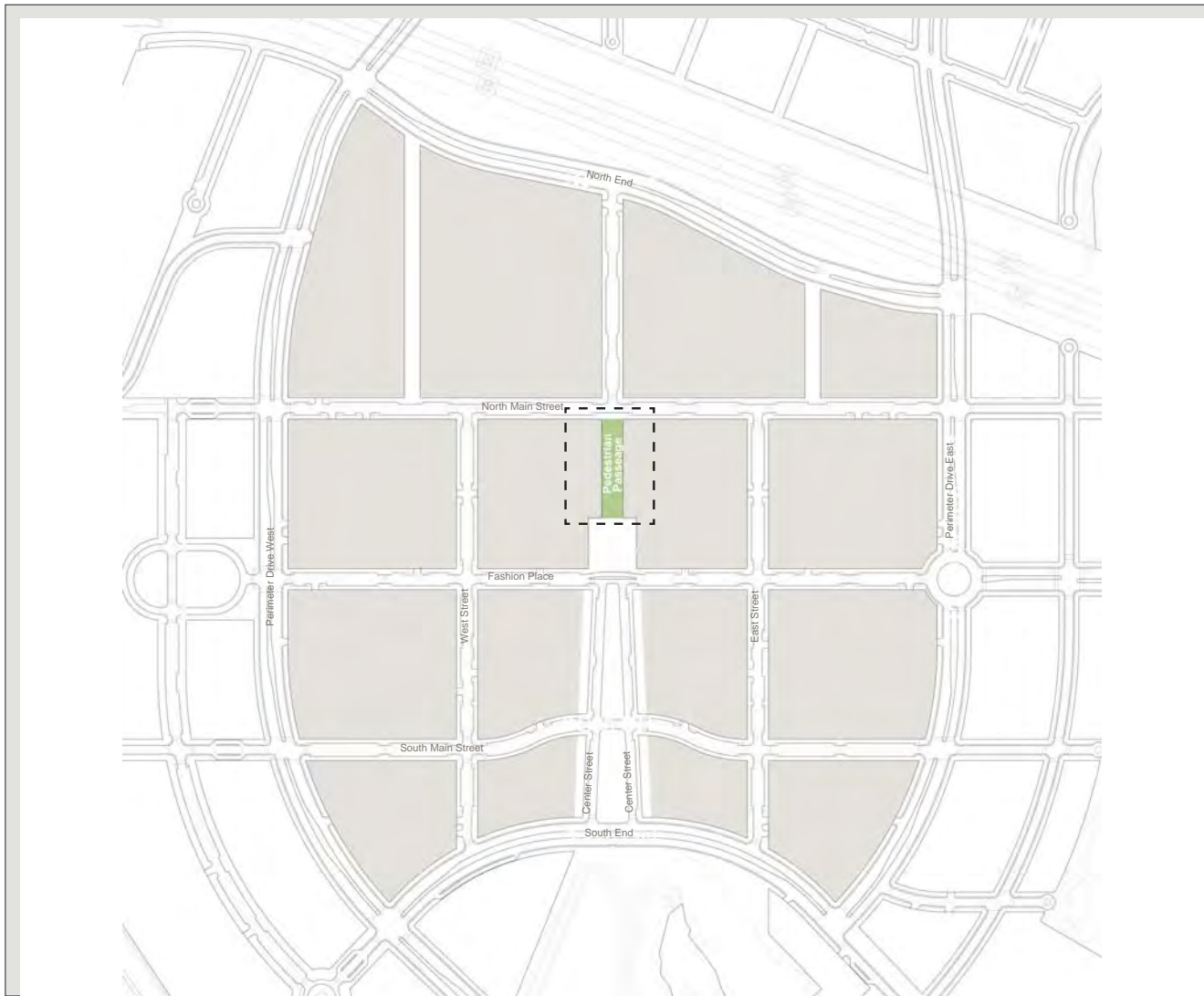


December 21\_03.00 PM





## 2.6.3 Center Street Corridor - Pedestrian Passage



Overall Plan showing Pedestrian Passage location.

### Pedestrian Passage:

The pedestrian passage is located between North Main and Fashion Place and creates the only permanent all pedestrian zone within Konterra Town Center East. The pedestrian passage contains "Town Square" the physical and metaphorical center of the project. Locating the pedestrian passage at the central terminus of Center Street allows the Town Square to act as a destination point from the southern linear park approach as well as a gateway vista for those approaching from the north. As the only all pedestrian area within Konterra Town Center East the passage provides a unique experience to patrons and residents offering a central gathering space and plaza that can be used in several ways.

### Intent: Pedestrian Haven

As an ideal place to locate restaurants and cafes the pedestrian passage will be surrounded by activity while at the same time buffered from vehicular traffic. Kiosks, water features, low walls, street furniture, landscaping and accent paving are all elements that will help to shape and activate the pedestrian passage. The role of the passage is two-fold: destination and circulation.

As a destination within the site the pedestrian passage provides a larger area for gathering and relaxing. Designed as a large plaza at one end, the pedestrian passage offers relief from the urban street wall and provides an opportunity for enhanced landscaping, public art and park amenities. These elements will help to define the passage as both a vista focal point and destination within the site.

The pedestrian passage also functions as a major pedestrian circulation corridor. Located at the center of the project the pedestrian passage gives precedence to the pedestrian over the car allowing passage only to those on foot. This vehicle free zone creates opportunities for activities and gathering that cater to the pedestrian.

### Recommendations:

- Sidewalk including landscape strip should be no less than 15'-0" from the face of curb.
- Trees will be spaced at minimum every 30' - 45' on center where possible
- Lighting will be spaced and sized to accommodate both pedestrians and vehicles.
- Trees will be provided along the open space of the Pedestrian Passage to provide shade and enhance the desired view corridor.
- Building face to building face will vary in this area to accommodate the Town Square Open Space
- Bicycle racks, street furniture and shopping kiosks will be provided at convenient locations



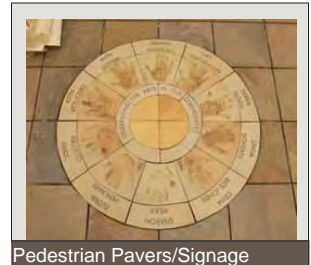
Tree Grate



Brick



Awnings



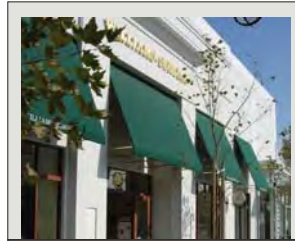
Pedestrian Pavers/Signage



Street Furniture



Street trees and planters



Signage/Awnings



Water features



Signage/Awnings



Stone





## 2.6.3 Center Street Corridor - Pedestrian Passage



**Retail Building Typology - Representative Pedestrian Passage Plan/Elevation**

Scale: 1:25

*\* Please see material sample pages in chapter 2.6.4 for more information regarding possible material selection*

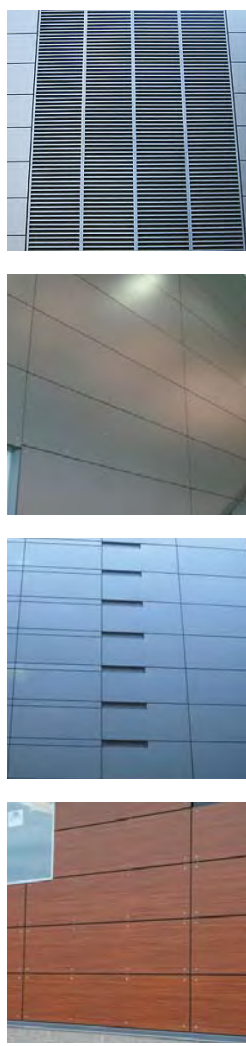


Scale: 1:25



# 2.6.4 Ground Floor Cladding Materials

## Composite Materials



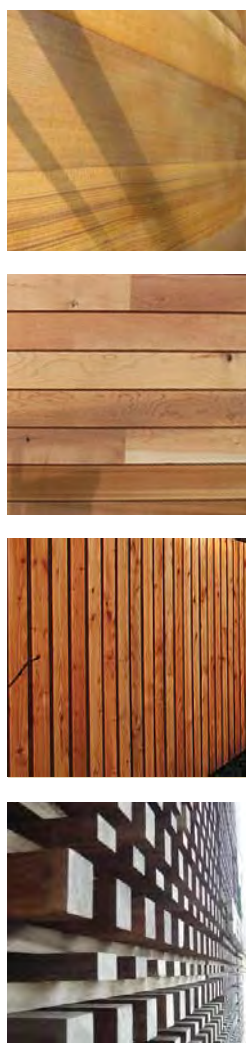
## Composite Material Applications



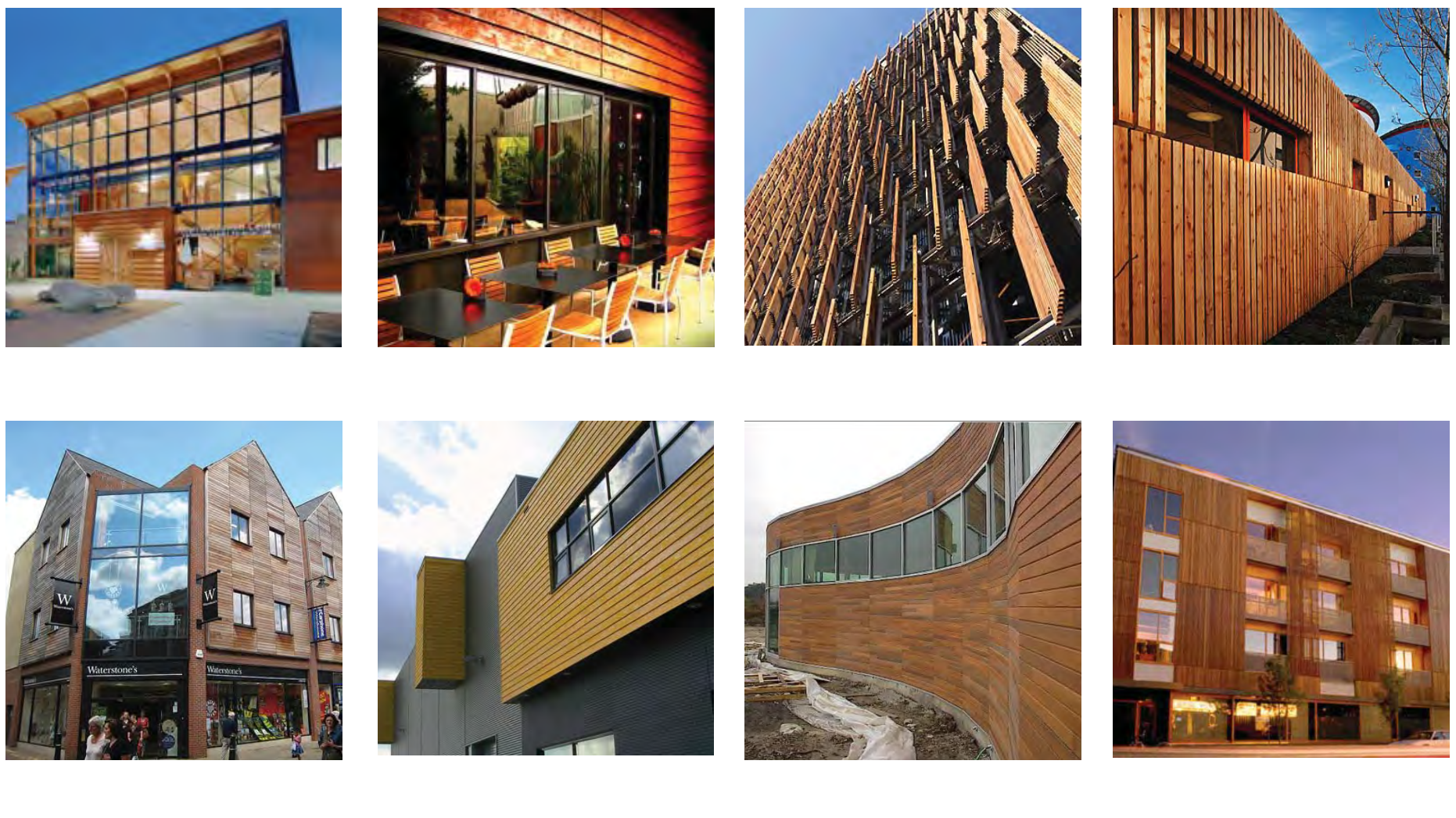


# 2.6.4 Ground Floor Cladding Materials

## Wood Materials



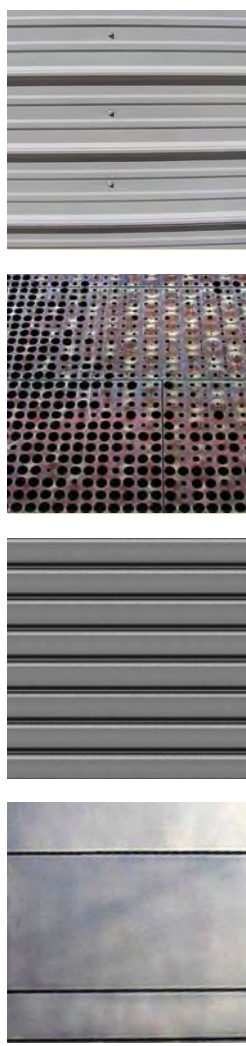
## Wood Material Applications



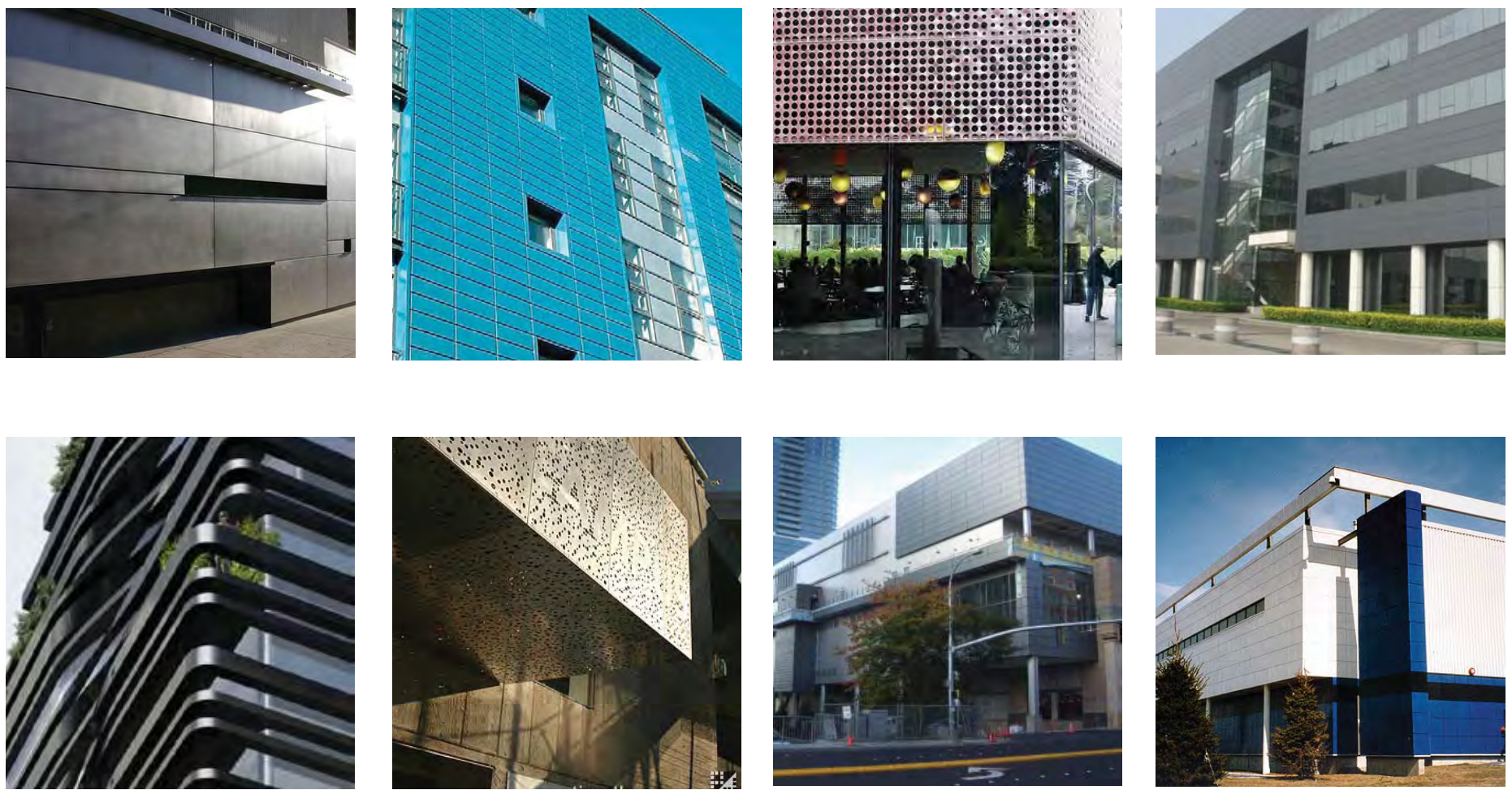


# 2.6.4 Ground Floor Cladding Materials

Metal Materials



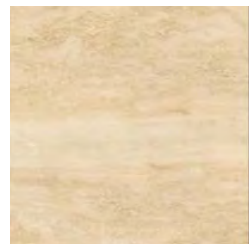
Metal Material Applications



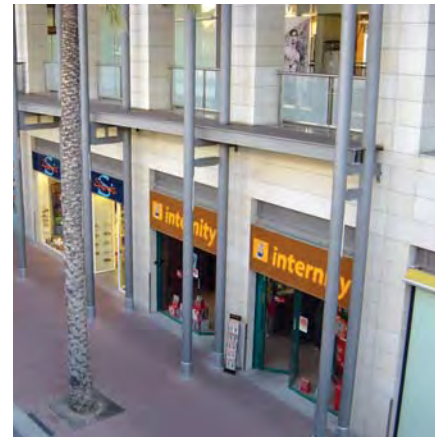


## 2.6.4 Ground Floor Cladding Materials

### Stone Materials



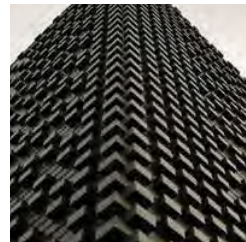
### Stone Material Applications



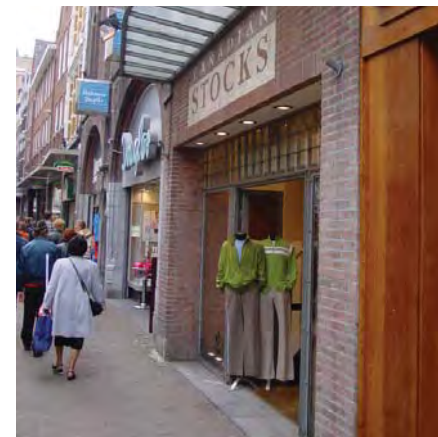
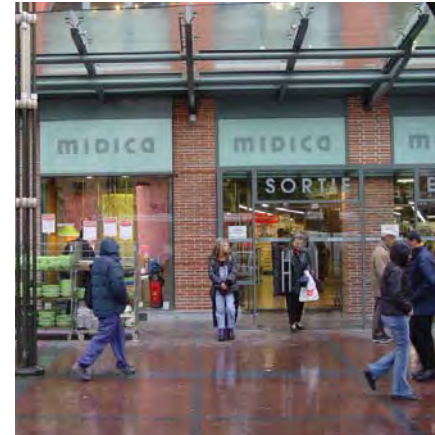
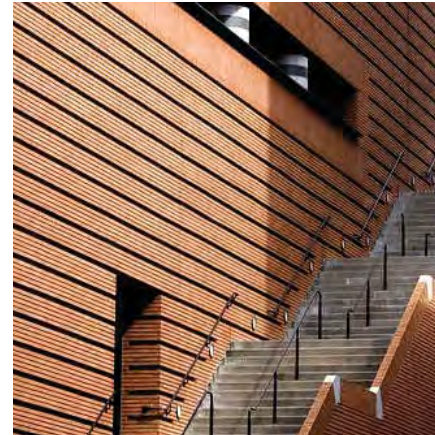


## 2.6.4 Ground Floor Cladding Materials

### Brick Materials



### Brick Material Applications



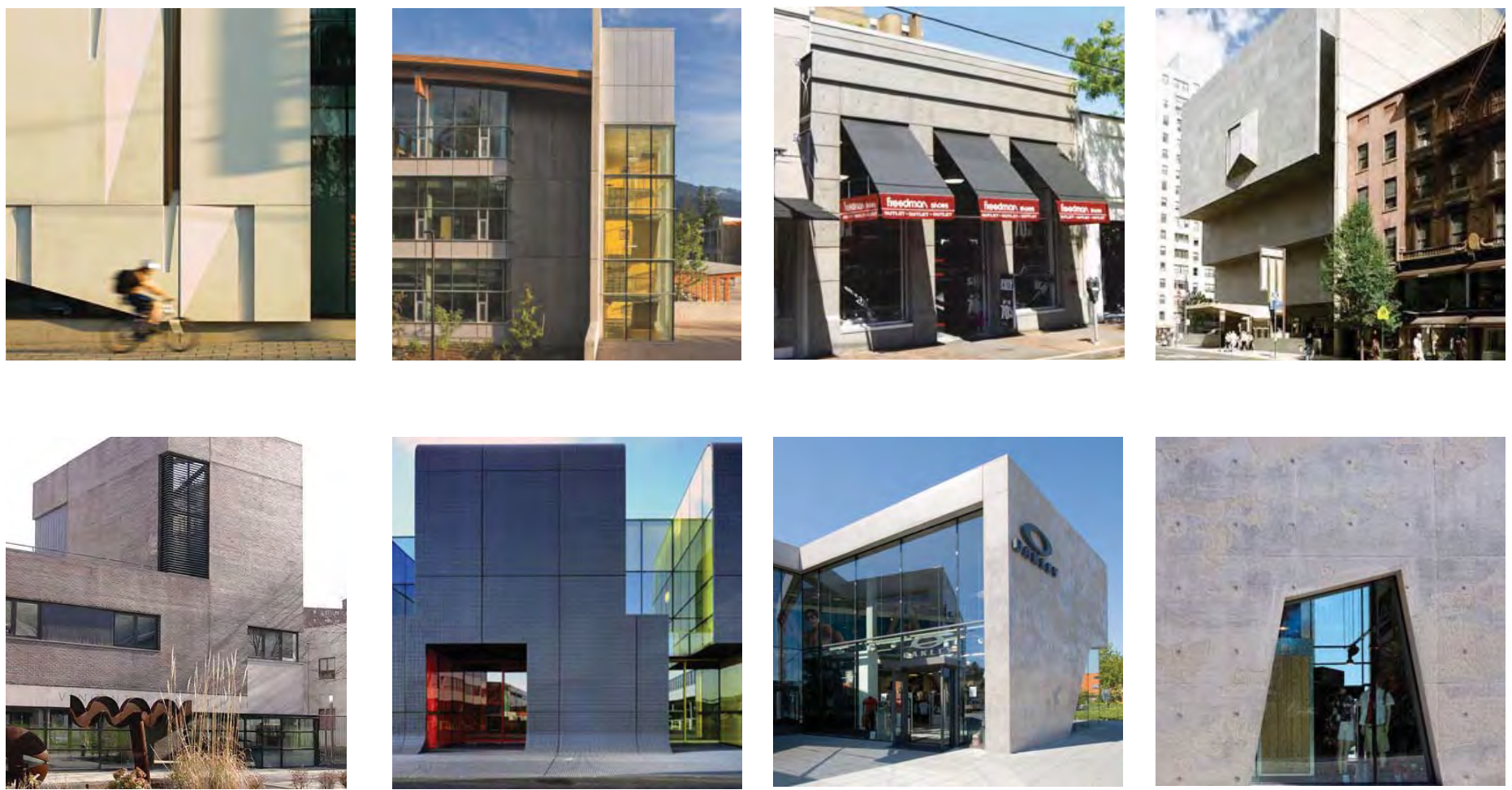


# 2.6.4 Ground Floor Cladding Materials

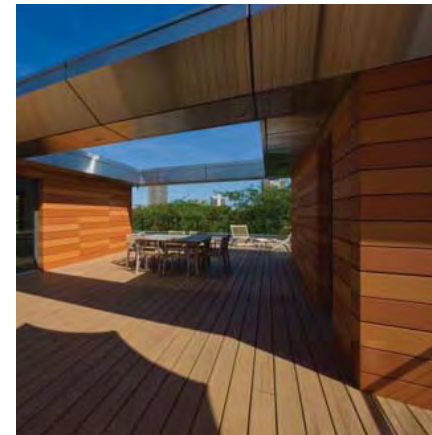
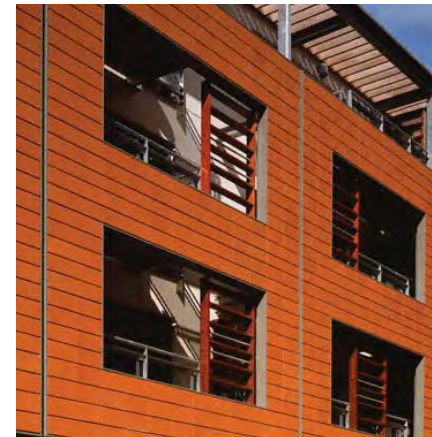
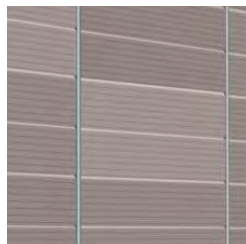
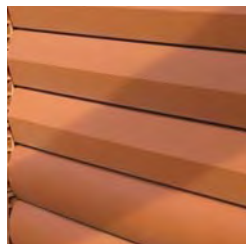
## Concrete Materials



## Concrete Material Applications

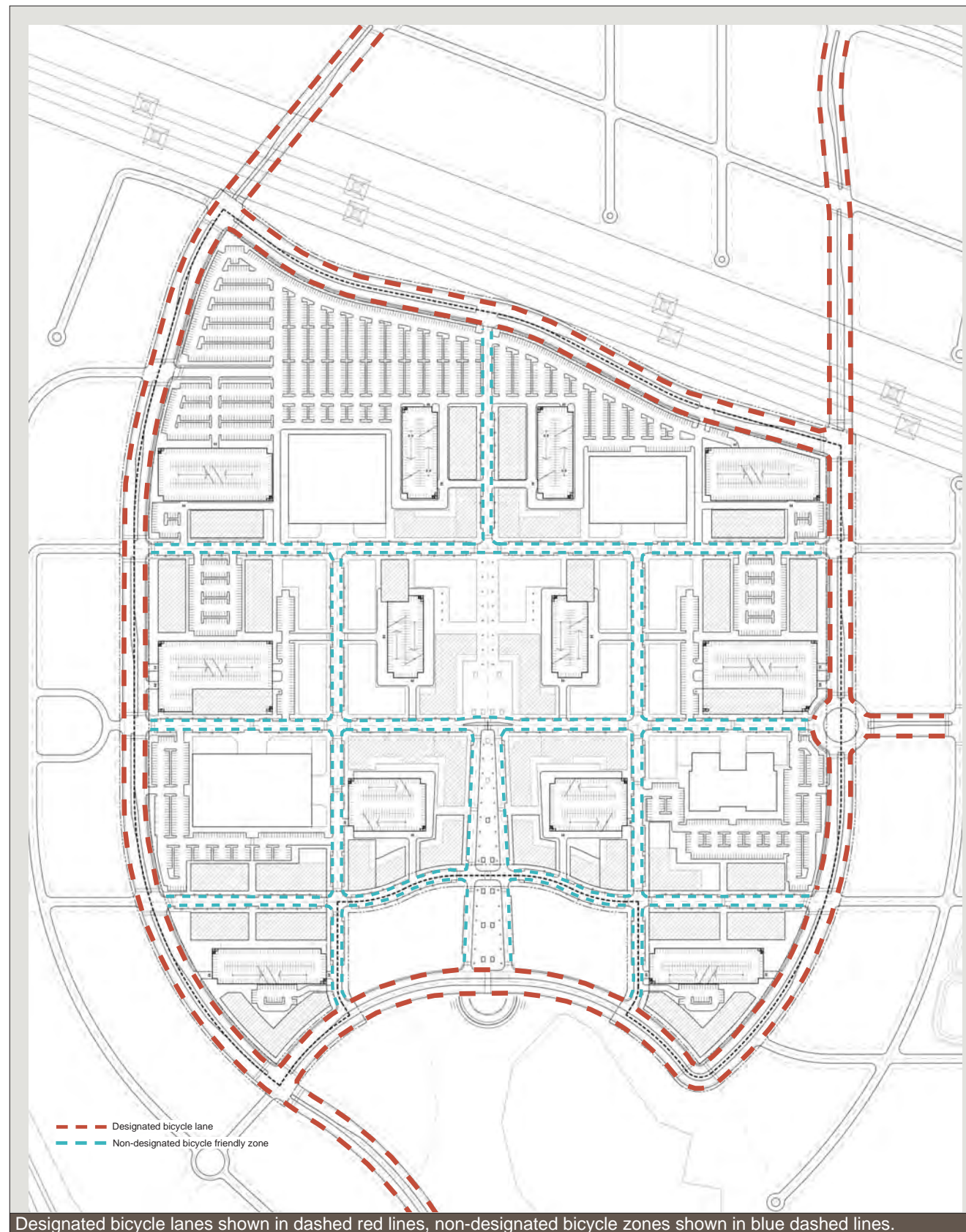








## 2.7 Streets with Bike Lanes



### Intent: Multi-modal Environment

Konterra Town Center East is intended to be a multi-modal development that encourages pedestrian and bicycle traffic to co-exist harmoniously with vehicular traffic. Poised adjacent to an existing bike network in Prince George's County, Konterra Town Center East will benefit from connecting to this recreational system and the greater regional community. By offering amenities and provisions to riders Konterra Town Center East can become a destination point along the trails bringing in potential customers and providing a unique environment for bikers in the area to enjoy.

### Perimeter Road: Bike Lanes

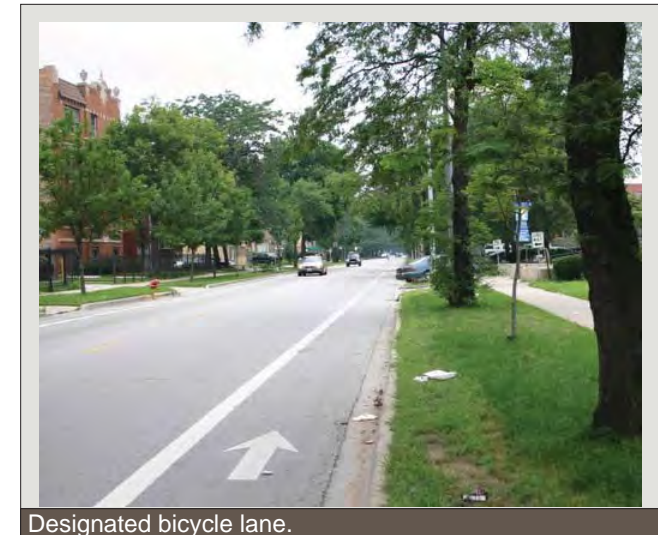
Bike lanes are planned to be striped on both sides of the major collector streets surrounding Konterra Town Center East including: Perimeter Drive East, Perimeter Drive West, North End and South End. By placing the bike lanes along these streets, circulation to the site will be direct and efficient, and bicyclists will not have to contend with on-street parking and the dangers that come from opening car doors into bike lanes while traveling at higher speeds.

### Downtown Core: Bike Lanes

The streets within the downtown are 15 feet wide from the center line of the road to the edge of the on-street parking. This width is sufficient enough for vehicular traffic which requires a minimum 11 foot wide travel lane. The additional 4 feet of street width can be utilized by bicyclists, but will not be striped as a designated bike lane. Patrons and residents to Konterra Town Center East are encouraged to arrive by bicycle, and then once inside the development, to explore by foot. As traffic speeds within the core will be low, it is likely that pedestrian movement will be the most efficient and preferred means of navigating the site. Bicycles will not be permitted to be ridden on sidewalks to protect pedestrians from conflicts.

### Amenities for Bicyclists:

In order to encourage bicycle transportation to Konterra Town Center East, certain amenities will be required such as bicycle racks and lockers so that visitors feel they are provided for and welcome. Bike racks will be placed along the street facing facades of buildings near entries so that they are convenient and visible to users but do not obstruct circulation or access. General areas for lockers and showers should be provided for those who choose bicycle transportation as a means of commuting to and from work, shopping and living.





## 2.8 Roundabouts



### Roundabouts

Konterra Town Center East contains one roundabout at the intersection of Perimeter Drive East and Fashion Place. This intersection is intended to be the major access point to the downtown from the east; and the majority of people who use the new Inter-County Connector to access the project will likely do so through this entrance. It is important therefore to create a gateway at this intersection so that visitors clearly know when they have arrived and where they are intended to enter the site. It is also an opportunity to give the first glimpse of what the project within has to offer in terms of character and design and a chance to draw people into the site.

The advantage of placing a roundabout at the intersection of Perimeter Drive East and Fashion Place is two-fold. Firstly it allows an opportunity to keep traffic moving rather than implementing a system of traffic lights that would force traffic to stop and start causing cars to stack. As Perimeter Drive East is a collector road it is important that traffic move as fluidly as possible. The roundabout acts as both a traffic calming device and facilitator to continually move traffic. While traffic will have to slow down as it moves through the roundabout it will be flowing constantly. This continuous movement will expedite the travel of those continuing along Perimeter Drive East while at the same time providing easy and efficient movement to those accessing the downtown to the west or the future residential to the east.

The second advantage to the roundabout is that it creates an opportunity to establish a gateway for the project. By placing a roundabout at the east entrance to the project it creates a central focal point visible from within the core as well as along Perimeter Drive East. The roundabout becomes a landmark and way-finding device helping visitors to navigate the site.





## 2.9 Pedestrian Pathways

### Intent: Safe Pedestrian Connections

Konterra Town Center East is designed to be a pedestrian oriented development that encourages residents and visitors to navigate the site on foot rather than depending on the car. To facilitate and encourage this behavior a network of pedestrian circulation will be integrated into the overall design of Konterra Town Center East. Included in this network will be:

#### Sidewalks

Pedestrian pathways that are adjacent to Main Streets and Perimeter Roads will be referred to as sidewalks. Typically these will be no less than 10' in width and will be integrated into the perimeter of blocks along with pedestrian rooms, street furniture, planting strips and landscape areas. (see yellow lines)

#### Walkways

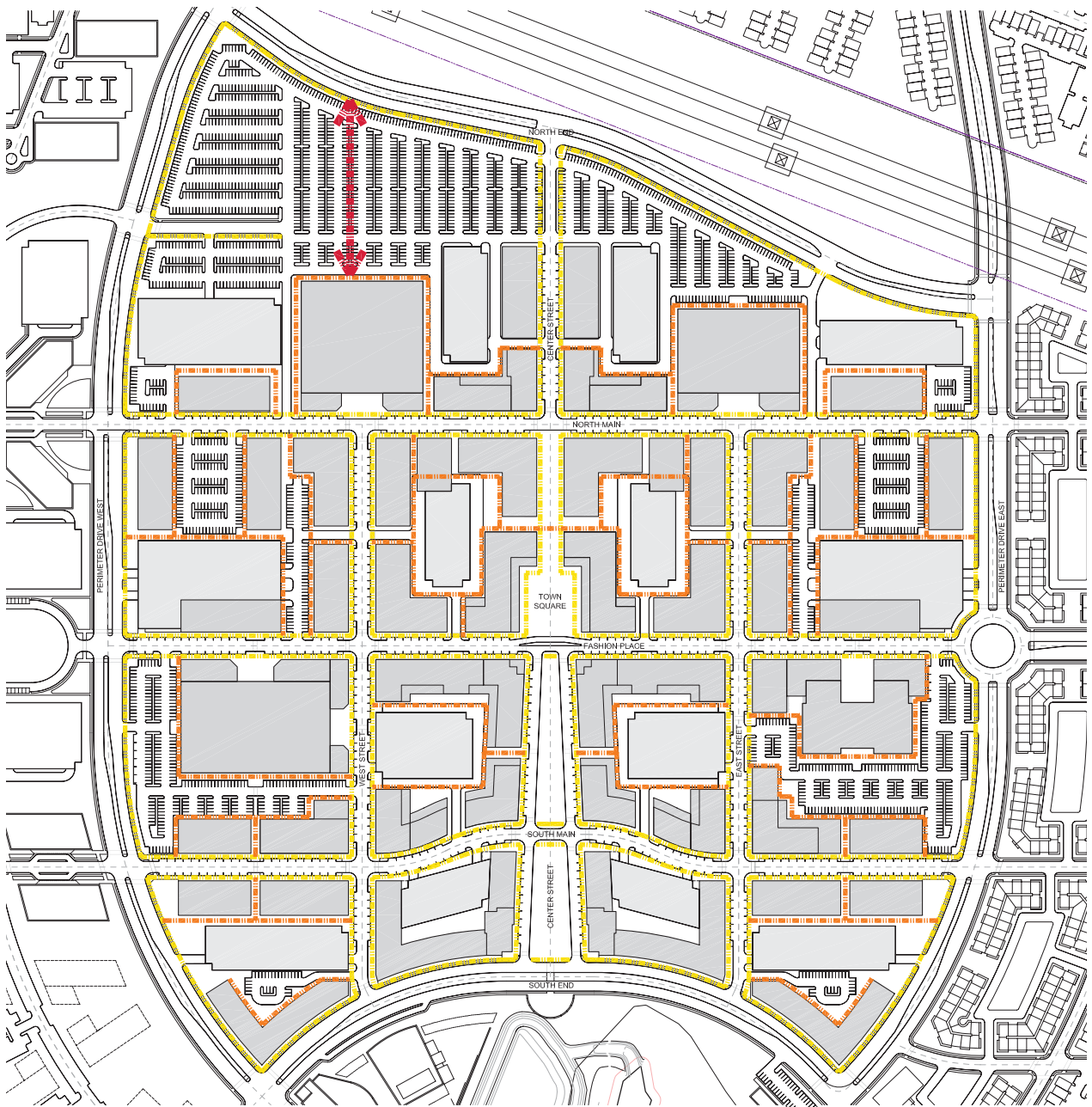
Pedestrian paths referred to as walkways will typically be found within the interior of the blocks. Walkways create circulation routes from sidewalks around the perimeter of the blocks to functions located in the interior of the blocks such as parking lots and parking structures. (see orange lines)

#### Pedestrian Pathways with Allees

Pedestrian pathways that provide protective landscaping as well as circulation paths will be referred to as Pathways with Allees. These Allees will be placed in permanent surface parking lots for which the farthest point exceeds four hundred (400) feet. (see red arrows)



Example of Sidewalk



Final Phase

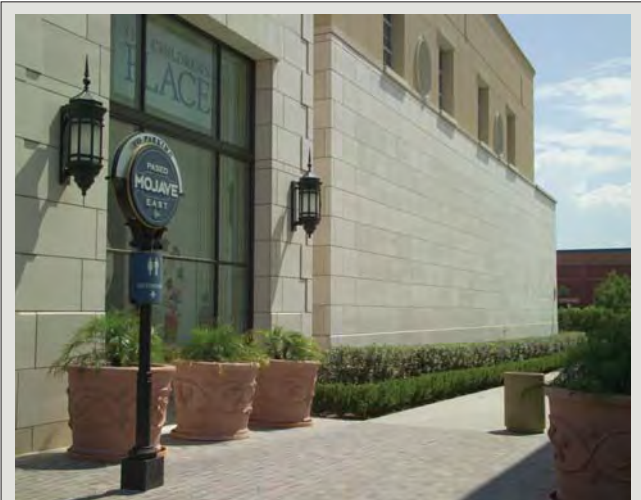
- Sidewalk = yellow Part of R.O.W.
- Walkways = orange
- Pedestrian Pathways w/Allee = red



Example of Sidewalk



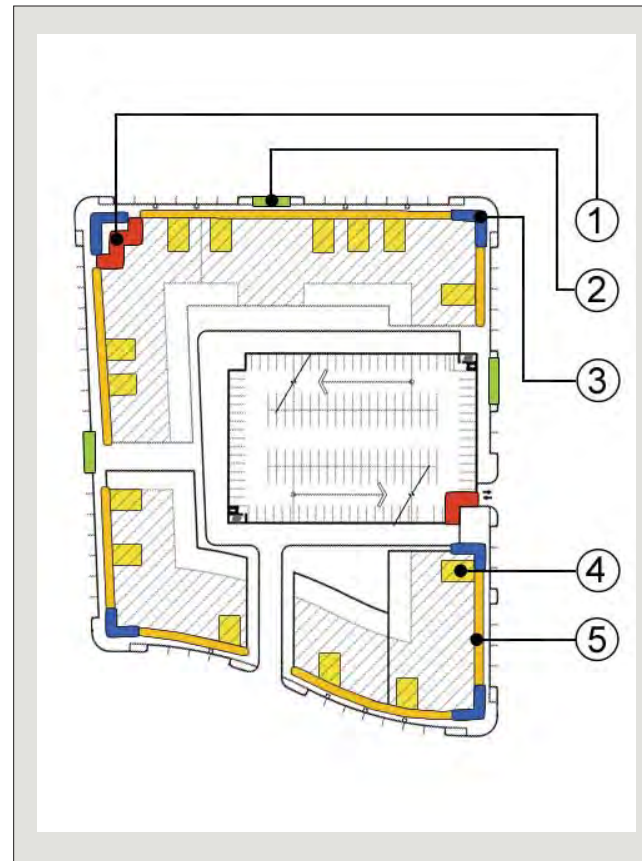
Example of Paking Lot Allee



Example of Walkway



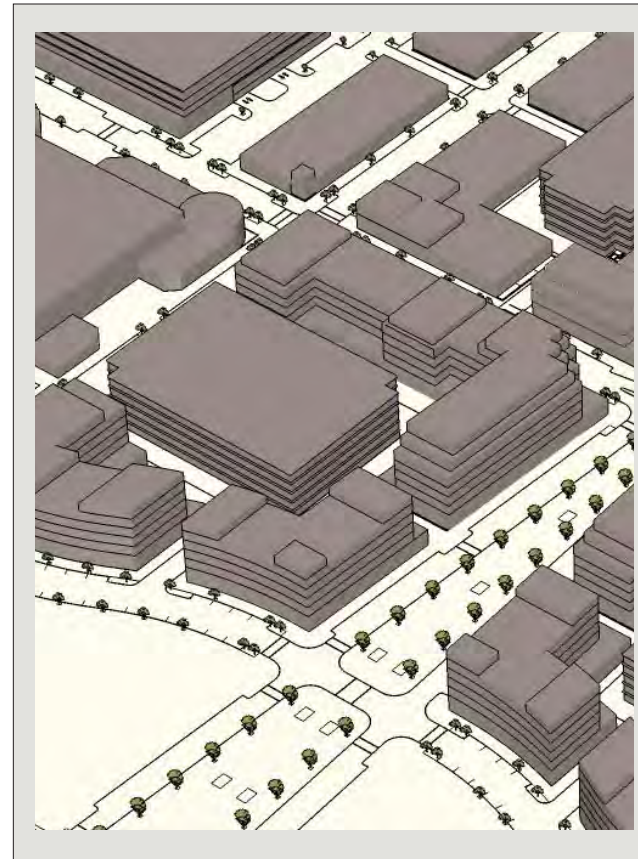
## 3.1 Urban Design Principles



### Streetscape Components

The streetscape, and the relationship it creates between the sidewalks and the buildings, is the first critical urban design issue that must be addressed to insure the success of Konterra Town Center East. The components that define and establish the streetscape are defined below:

- 1. Street Wall Variation:** Building articulation in the form of street wall variation allows the ground floor facade of buildings to vary creating opportunities to engage the pedestrian both visually and physically at a comfortable human scale. (See section 4.0)
- 2. Outdoor Rooms:** These areas exist within the pedestrian sidewalk and landscape zones and create active and passive pedestrian rooms where walkers can sit and rest, people watch and gather. These spaces activate the pedestrian realm and encourage people to pause and enjoy their surroundings.
- 3. Accented Corners:** Because of their exposure and visibility, buildings located at Main Street intersections should be given special treatment within the project. Corners should be articulated to create interesting site lines and memorable moments within the development that can also serve as way finding devices. This can be done in several ways including: heightening, rounding or chamfering the corner, or accenting it with a larger more detailed canopy. (See section 3.10)
- 4. Building Entrances:** These should be oriented toward the street/pedestrian environment and are encouraged to be recessed and architecturally articulated to make them dynamic and functional.
- 5. Storefronts:** Should be transparent in nature, visually appealing and engaging at the street level. (See section 3.8)

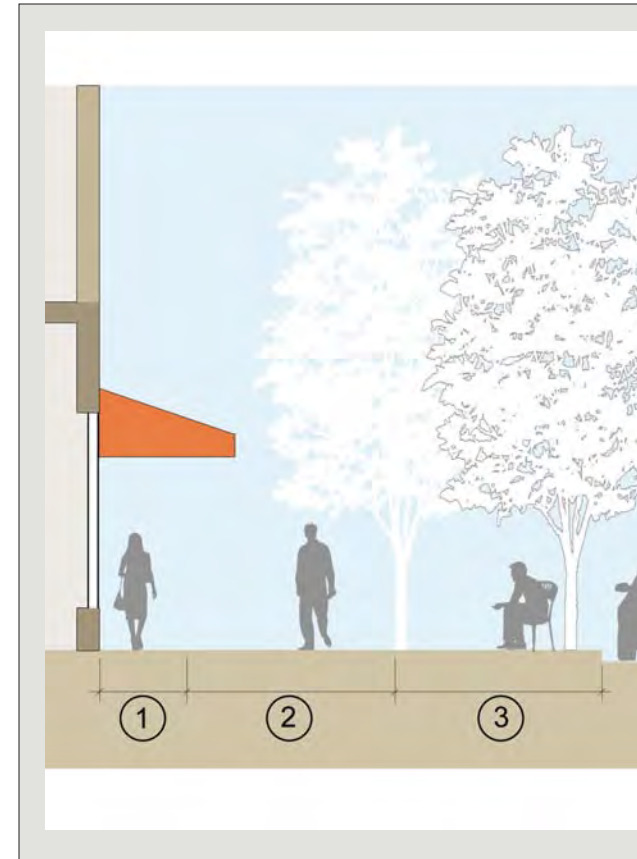


### Building Placement

The setbacks and building heights that determine the street wall are defined by use and are intended to create a consistent, but not monotonous, pedestrian environment of urban scale.

- 1. Ground Floor Activation:** Engaging the pedestrian at the ground level is integral to the success of the project as a pedestrian oriented development. By maintaining a human scale and ensuring that buildings enhance the urban character of the pedestrian realm it is possible to create an active and coherent pedestrian experience.
- 2. Street Wall Definition + Continuity:** This requirement is defined per building use/typology and varies from the lowest minimum of 22'-0" high for single story retail buildings, to a 48'-0" minimum for stand alone office buildings. There are no height maximums within the downtown core. These height requirements mandate uses be of urban proportion and encourage density. (See section 3.3 and 4.0)
- 3. Visual Engagement of the Street:** Opportunities for features such as balconies create a relationship between activities at the street level and activities and uses above creating a cross section of street liveliness and interaction.
- 4. Building Setbacks:** Building setbacks are required to be at minimum 15'-0" from the face of curb, to allow for wide sidewalks, and a maximum of 30'-0" from the face of curb to allow opportunities for plazas and pocket parks. (See section 4.0)

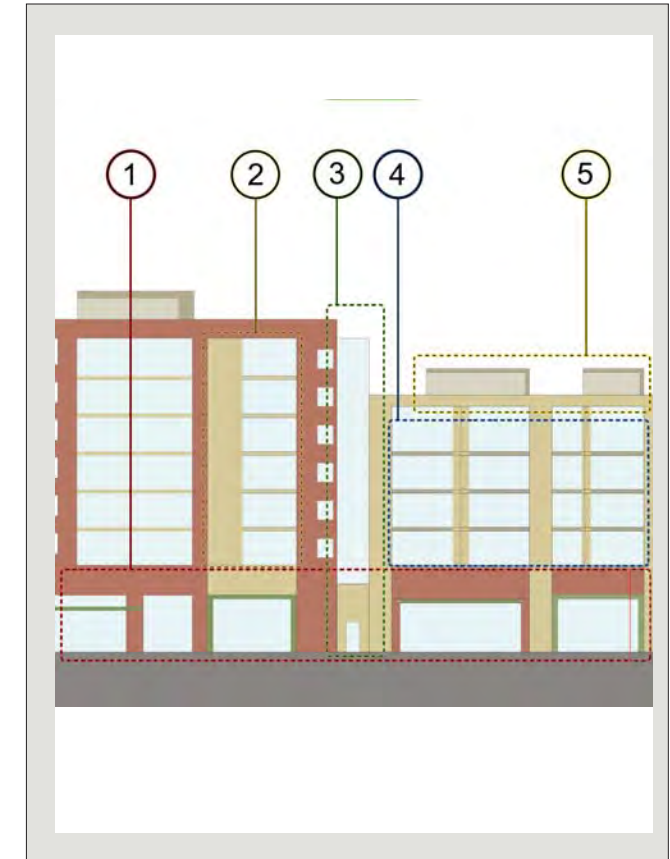
**Note:** The first permit plans for sites located within the adjoining blocks that front on the Main Streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of trees, street furniture and lighting fixtures for that street on which the project site is fronting.



### Pedestrian Realm

The sidewalks of Konterra Town Center East serve multiple functions within the larger streetscape framework. Crucial to the success of a pedestrian oriented environment, the sidewalks must provide and facilitate three major needs. First they must connect the streetscape with the buildings that line them, allowing space for people to transition from inside to outside, and to engage the building facade i.e. shopfronts, in a way that protects pedestrians from the elements and is conducive to intermittent movement. Secondly sidewalks must act as linkages providing space for pedestrian movement to take place in an efficient manner. And lastly, as part of the pedestrian realm sidewalks should also offer places of rest and relaxation via pedestrian rooms, street furniture etc.

- 1. Storefront/Entry Transition Zone -** This area will be composed of transparent and inviting storefronts, as well as building entry ways that are vibrant and lively incorporating building features, like canopies and awnings which play a dual role to protect pedestrians from the elements and add visual interest to the streetscape.
- 2. Circulation Zone -** This zone is defined by its ability to facilitate pedestrian movement. Consideration should be given to provide weather protection and incorporate durable and aesthetically pleasing paving materials to guide pedestrians along designated pathways.
- 3. Outdoor Room/Landscape Zone -** This area is designated as a respite where pedestrians can stop, mingle, relax, and sit in an environment that is pleasant, comfortable and landscaped, containing urban elements such as benches, lighting fixtures and bike racks which provides a buffer between street vehicles and pedestrian circulation.



### Facade Articulation

- 1. Pedestrian/Transparency Zone -** This area will be required to have the greatest ratio of transparent glass to surface area. This zone is intended to engage both the pedestrian on the street and the shopper in the store. The high level of transparency fosters a connection between the indoor areas and the outdoor pedestrian realm while encouraging an open and light atmosphere.
- 2. Upper Floor Articulation Zone:** Distinction of uses through material and scale is encouraged as a way of creating an overall understanding of a building's composition. Differences in transparency and fenestration requirements that are based on use, will create varying facade treatments, making it possible to decipher uses at a glance and create a more diverse environment.
- 3. Entryway Zone -** These are areas that exist at the ground level, such as residential lobbies, but whose major function is to pull people inside at grade level and upwards into an above grade use. These areas should be clearly defined and articulated to be visible and to express the nature of the use to which they connect.
- 4. Upper Floor Visual Impact Zone -** Although the upper floors of a building are not physically adjacent to the pedestrian realm in the same way as the ground floor level, it is possible to engage and connect with the pedestrian zone through visual accents and sight connections, like those created by balconies, which engage the street below and help to shape the overall environment and experience.
- 5. Rooftop Zone -** This zone impacts the built environment as it is seen from above and used by its tenants. Things like rooftop gardens create another layer of architectural expression that acknowledges a need to connect to the outside and an interest in what is happening below.



## 3.2 Solar Orientation

### Intent

To respect solar orientation and use design strategies to maximize the positive uses and minimize the negative impacts of sun on the site.

To design both comfortable indoor and outdoor spaces that respond to changing solar conditions throughout the year.

### Required

#### 3.2.1 Year-round Solar Orientation

Buildings shall be designed to be responsive to their environment with respect to solar orientation and weather conditions implementing strategies that create better seasonal conditions both indoors and outdoors year round. Buildings should minimize winter winds, and take advantage of air circulation in hotter months. Building fenestration should react to its orientation and use. Buildings should incorporate weather/shading devices for overhead protection as well as possible indoor connections to enable comfortable year-round use by visitors and residents.

#### 3.2.2 Reflectivity

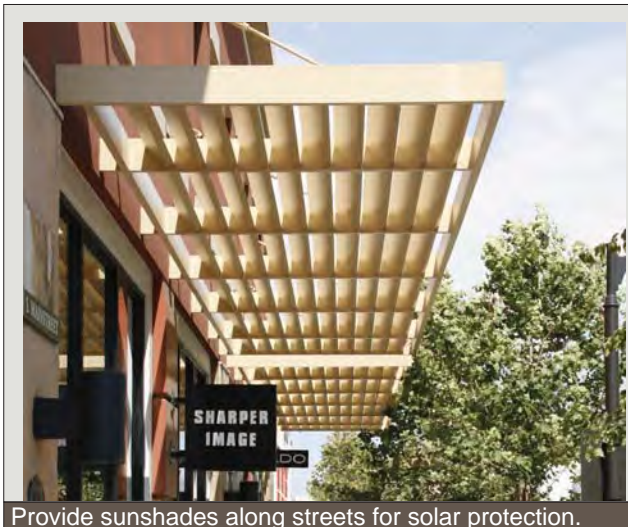
Buildings should not incorporate highly reflective materials such as mirrored surfaces because they create pockets of intense heat and can produce dangerous, visually blinding conditions both for pedestrians and drivers.

#### 3.2.3 Building Shade/ Weather Protection

Buildings located on the south sides of the streets will provide needed shade during the hottest parts of the day throughout the summer months to adjacent sidewalks, buildings and streets. All buildings regardless of orientation should provide some form of overhead protection from inclement weather such as rain and snow.

#### 3.2.4 Winter Shade

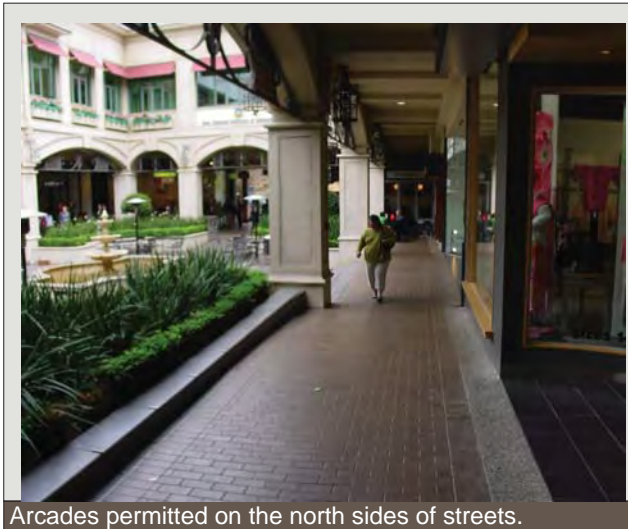
Each building should be designed to minimize the blocking of natural light from neighboring or adjacent buildings where possible. Landscape selection should also play a role in this effort.



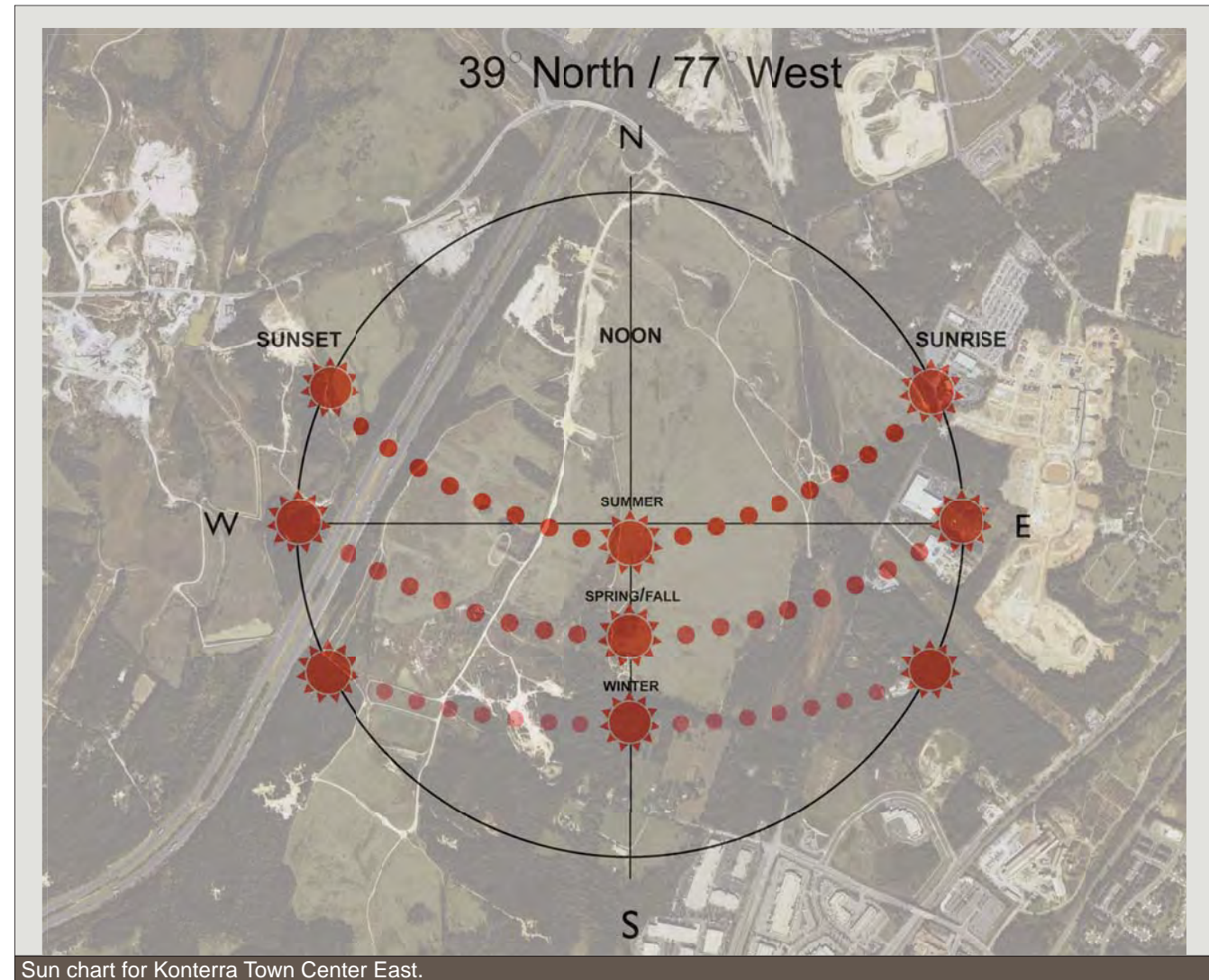
Provide sunshades along streets for solar protection.



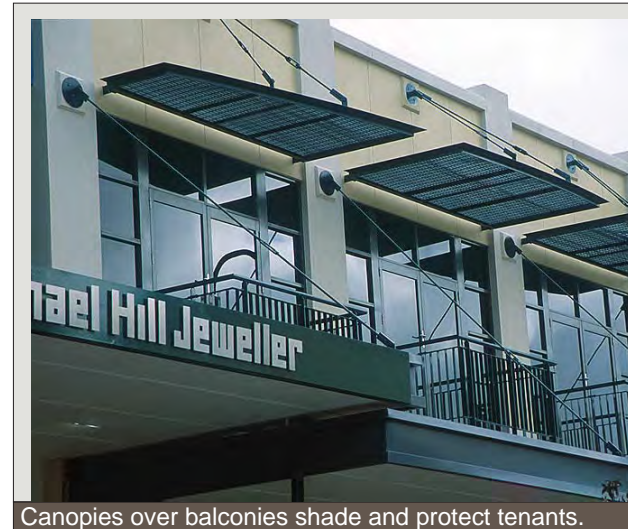
Awnings facing onto the sidewalk for weather protection.



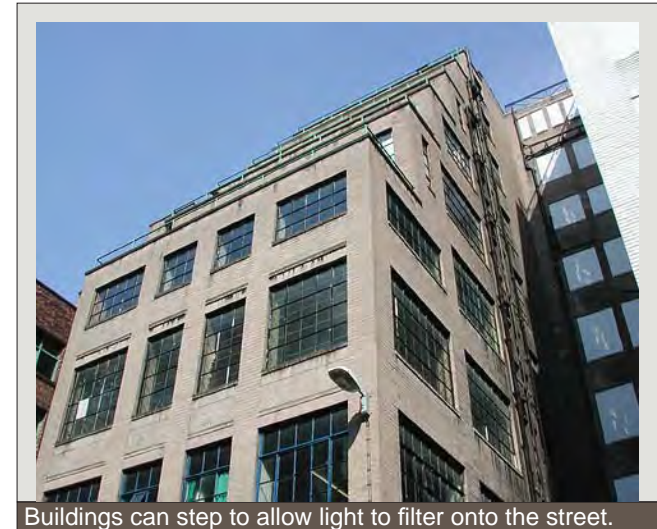
Arcades permitted on the north sides of streets.



Sun chart for Konterra Town Center East.



Canopies over balconies shade and protect tenants.



Buildings can step to allow light to filter onto the street.



## 3.3 Minimum Building Heights

### Intent

- To provide ground floor building spaces that promote an urban character, are sized appropriately for their use, and comfortable in human scale.
- To create an active pedestrian realm that is visually attractive, engaging and indicative of a mixed-use environment.

### Required

#### 3.3.1 Building Heights, by Typology

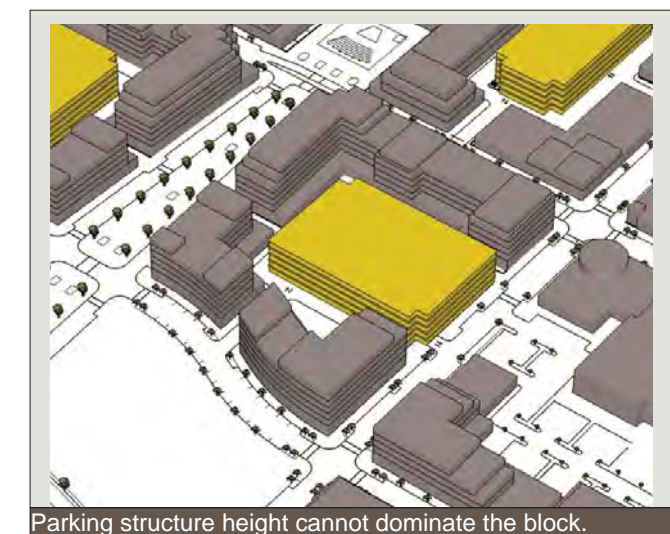
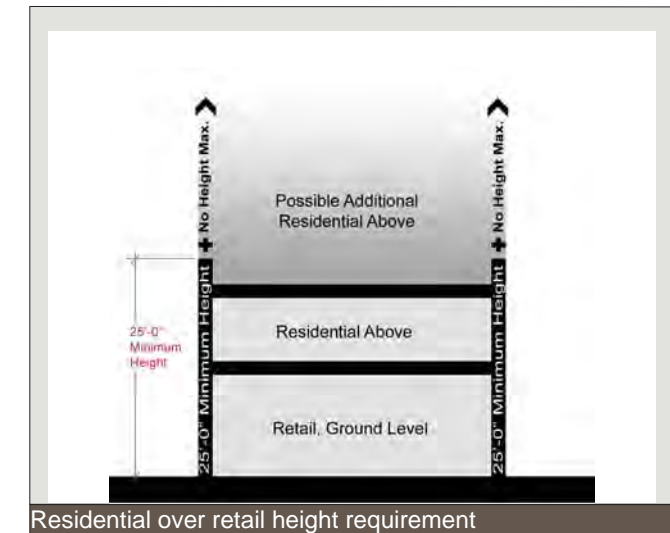
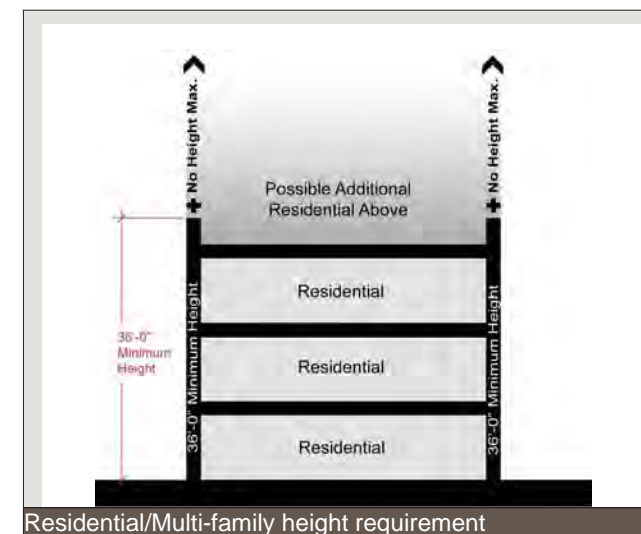
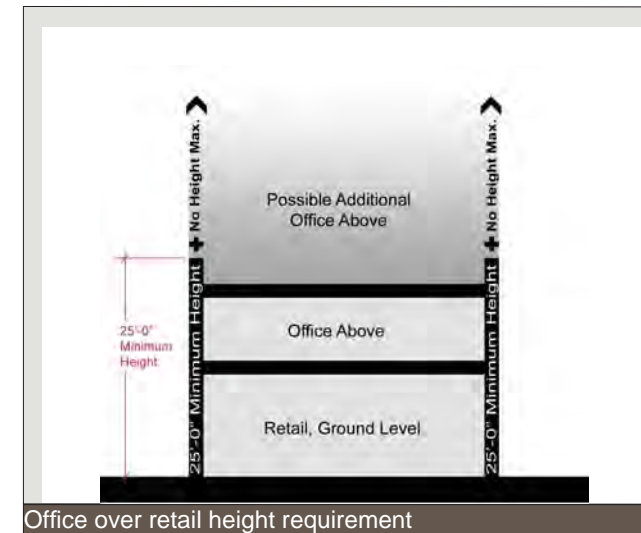
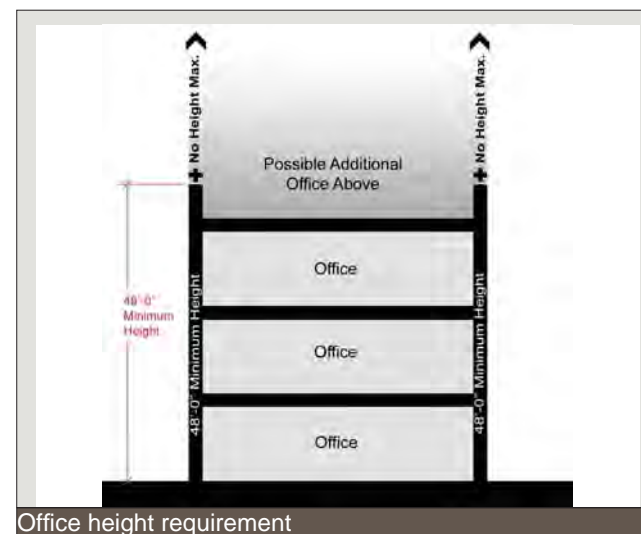
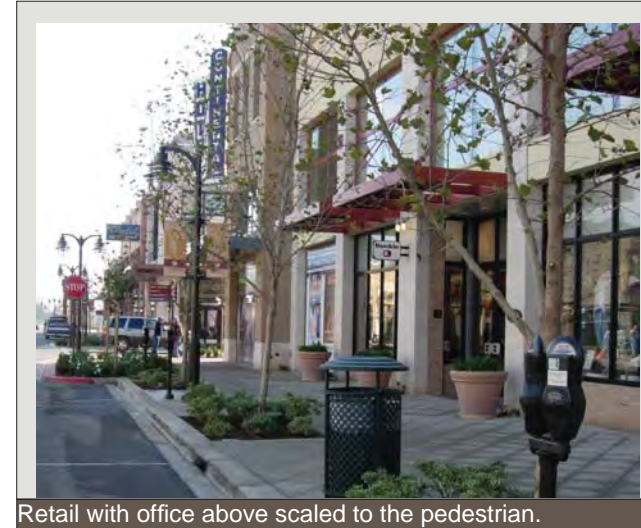
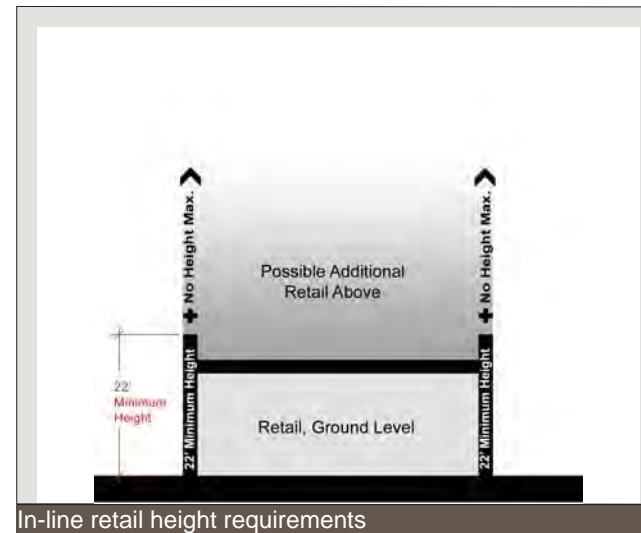
Ground floor heights vary by building use and are stated in terms of minimums in the bulk restrictions. Heights are to be measured from the ground to the top of the parapet wall. They are as follows:

- **In-line Retail:** Minimum 22'-0"
- **Office over Retail:** Minimum 25'-0"
- **Residential over Retail:** Minimum 25'-0"
- **Office:** Minimum 48'-0"
- **Residential/Multi-family:** Minimum 36'-0"
- **Multi-family/Structured Parking:** Minimum 48'-0"

### Recommended

#### 3.3.2 Scale and Proportion

While ground floor building heights may vary in size, street level building facades should incorporate elements that reinforce human scale and proportion, emphasizing the relationship between the street, the building, and the pedestrians interacting with them.





## 3.4 Weather Protection + Shading + Solar Access



Building design allows for light to reach the street in winter.



Awnings to control solar gain.



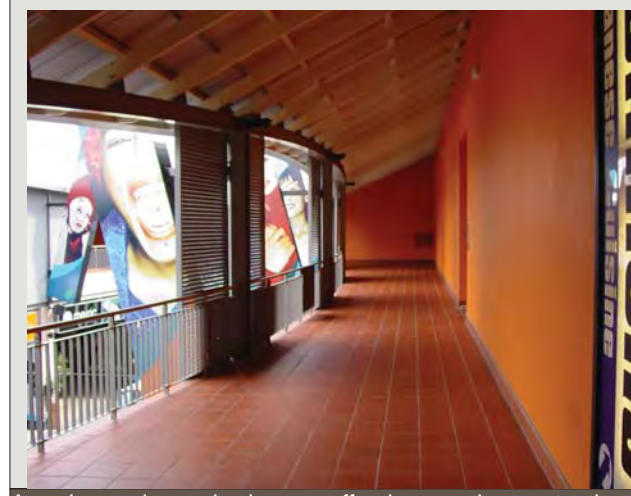
Building height and bulk can effect adjacent uses.



Building projection acting as entrance canopy.



Shade devices should be used on south facing facades.



Arcades and sun shades are effective weather protection.

### Intent

To incorporate architectural elements and design strategies that take into account solar orientation and other weather factors to create comfortable and controlled indoor and outdoor environments within Konterra Town Center East.

### Required

#### 3.4.1 Providing Controlled Environments

Buildings will be scaled and proportioned to take advantage of solar orientation. Buildings located on the south side of the street should be lower in height to allow natural light to filter to the street level year round. Building positioning as well as shading devices should be incorporated to protect against strong summer sun as well as precipitation throughout the year.

#### 3.4.2 Pedestrian Weather Protection

Buildings will provide protection in the form of overhangs, canopies, awnings, etc. to shield pedestrians from sun, and precipitation throughout the year.

### Recommended

#### 3.4.3 Building Orientation

Buildings should be designed to allow sunlight to reach the street level during colder months. Buildings should be positioned and scaled not to block natural light from reaching other adjacent or neighboring buildings.

#### 3.4.4 Solar Strategies

To minimize heat gain, buildings should be oriented to provide shade where desirable during warm months and to access solar heat during winter months taking advantage of passive solar heating and cooling. The streetscape framework will also help to control the pedestrian environment with deciduous street trees, that provide shade in the summer months, and allow more natural light to reach the street during the winter months.

#### 3.4.5 Facade Treatment

Solar orientation should be a major factor in the facade treatment of buildings. Facades with greater solar exposure should implement strategies which protect against heat gain, while facades with less solar exposure should incorporate greater amounts of glazing that maximize natural light opportunities.

#### 3.4.6 Building Entry Protection

Building entries should be protected from weather conditions such as sun, snow and rain by incorporating overhead projections like awnings or canopies and recessing entryways into the facade plane.



Canopies and recessed entryways protect pedestrians.



Canopies and awnings should be used to protect entries.

#### 3.4.7 Maximize Facade/Building Articulation

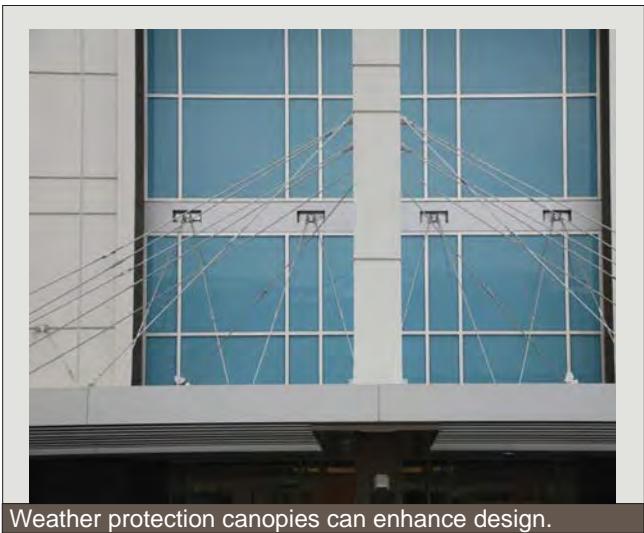
The benefits of increasing the amount of facade articulation is two fold: (1) it provides opportunities to both activate facade planes adding visual interest to the streetscape and (2) provides protection from weather and helps to control thermal temperature both within buildings and outside at the street level through the use of awnings, canopies and building projections like balconies.

#### 3.4.8 Weather Protection Zones

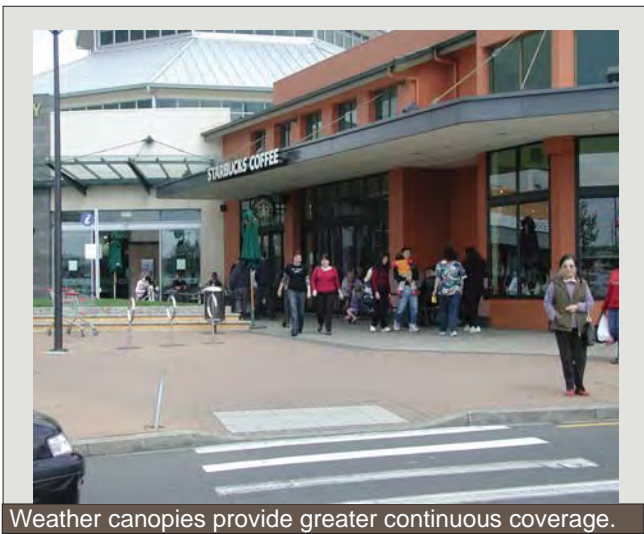
Where possible larger weather protection canopies/overhangs will be placed at block corners and in mid-block locations to provide larger areas for pedestrians to congregate while waiting for weather to pass. These areas will be no less than 150 SF and the projection will be no less than 6 feet from the facade wall.



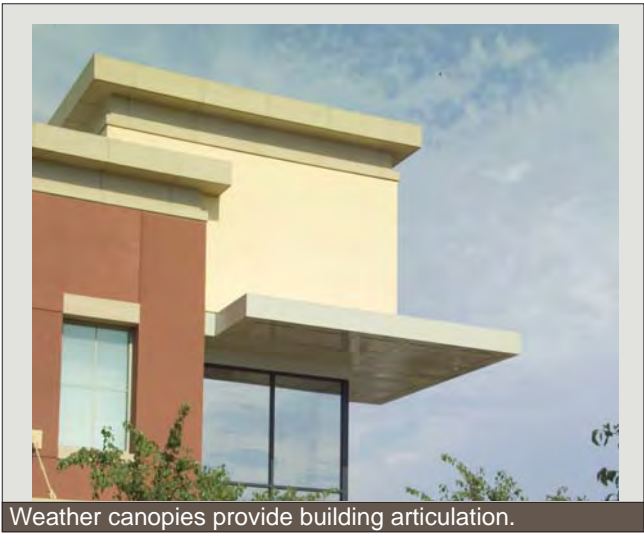
# 3.4 Weather Protection + Shading + Solar Access



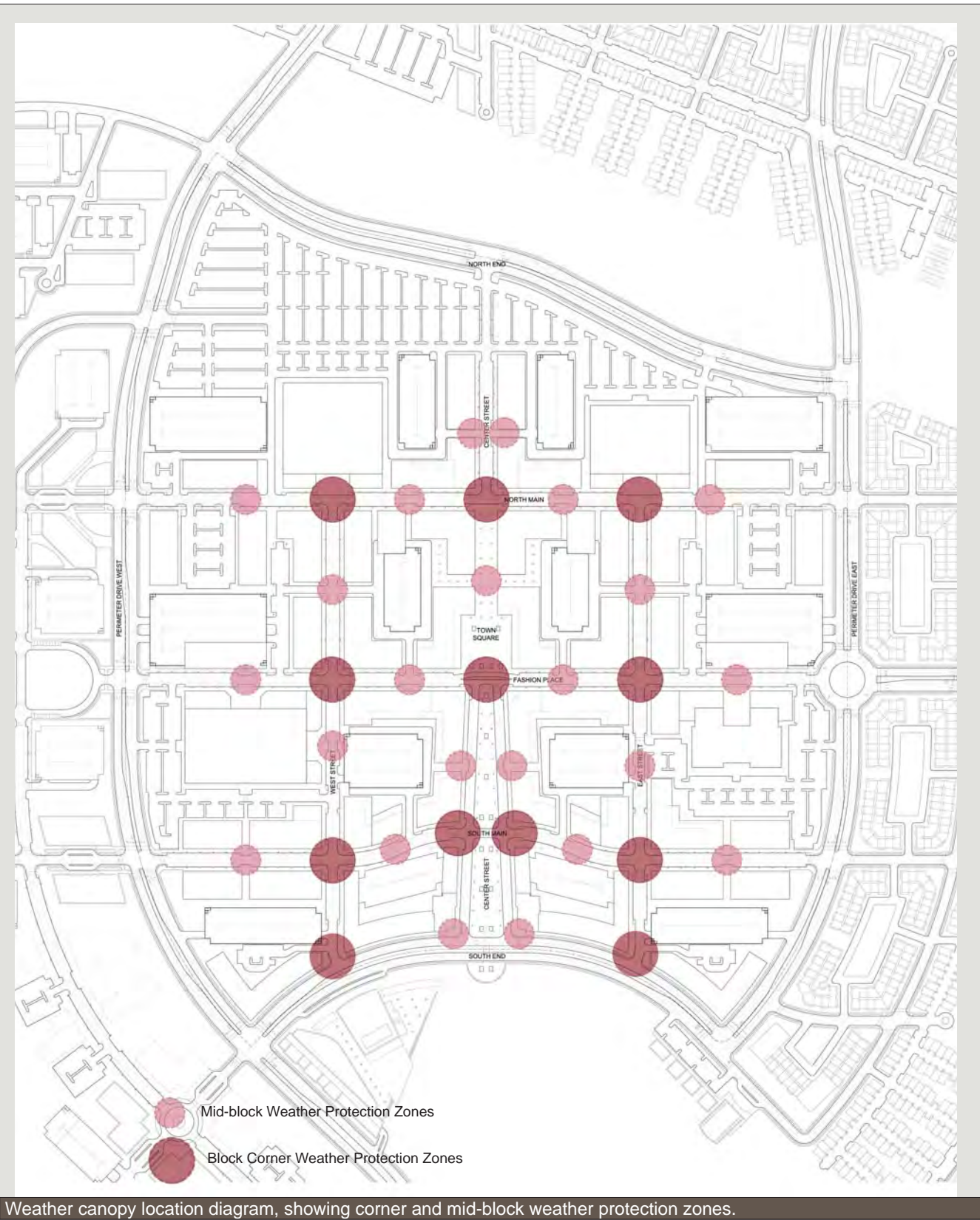
Weather protection canopies can enhance design.



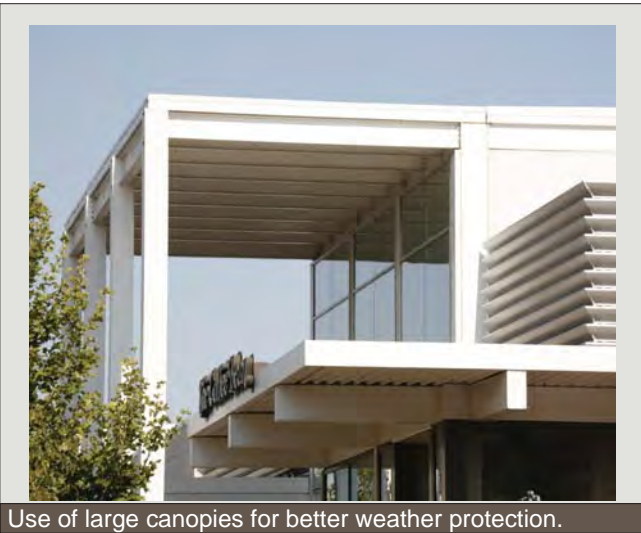
Weather canopies provide greater continuous coverage.



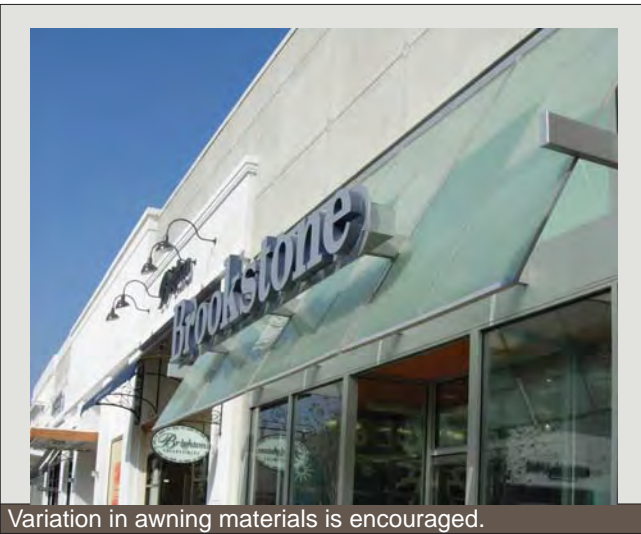
Weather canopies provide building articulation.



Weather canopy location diagram, showing corner and mid-block weather protection zones.



Use of large canopies for better weather protection.



Variation in awning materials is encouraged.



Weather protection is encouraged at all entrances.



## 3.5 Exterior Building Materials and Finishes



### Intent

To create an environment that is rich and diverse in expression through the use of material, articulation and layering which supports and creates a durable and sustainable neighborhood. To promote and characterize Konterra Town Center East as a highly developed and contemporary place composed of high quality exterior materials and construction with vernacular influence.

### Required

#### 3.5.1 Material Selection

Materials appropriate to the desired overall character of the development will be used, and there shall be an emphasis on traditional regional materials such as brick, stone and masonry.

#### 3.5.2 Material Quality

The materials used within the project must be of high quality, durable and sustainable materials. Particular care must be given to areas, such as the ground floors of buildings, where human interaction with the building materials is greatest and has the most direct impact on the pedestrian.

#### 3.5.3 Material Appearance

The materials used in the project will communicate a high level of quality and will enhance the overall design and character of the development, reinforcing the town center's urban nature and appearance.

### Recommended

#### 3.5.4 Material Proportion and Synthesis

Materials should be intentionally and thoughtfully chosen and combined with regard to color, texture and scale so that they enhance, not detract from the overall concept of the development.

### Acceptable

#### 3.5.5 Technological Advancement Over Time

Allow for the implementation of advancement in materials and building techniques over time to be incorporated into the development in the future, especially those which enhance environmental sustainability without jeopardizing the overall character and visual appearance of the development.

### Unacceptable Materials

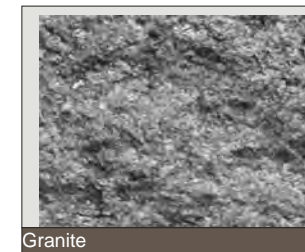
- Untreated wood siding
- Concrete masonry units
- E.I.F.S. at street level
- Stucco - limited use at street level, acceptable above



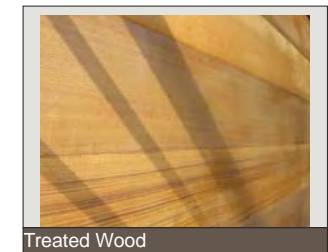
Brick



Limestone



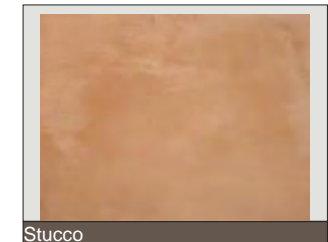
Granite



Treated Wood



Variation in Exterior Materials



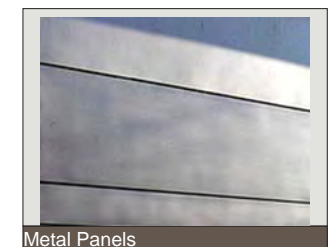
Stucco



Stacked Stone



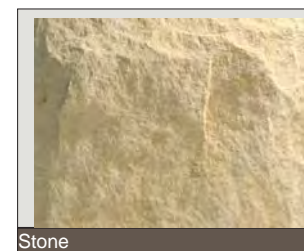
Variation in exterior materials, stucco allowed in small quantities at ground floor level, i.e. pilasters.



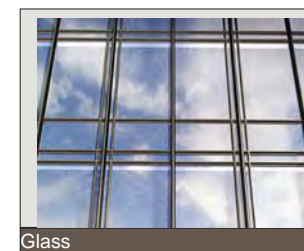
Metal Panels



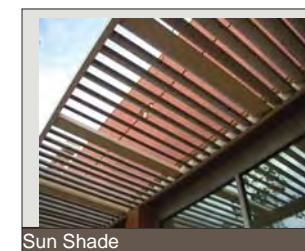
Steel/Glass



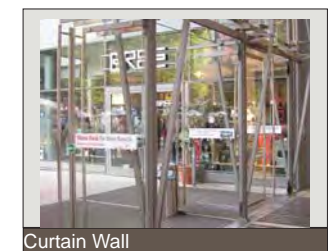
Stone



Glass



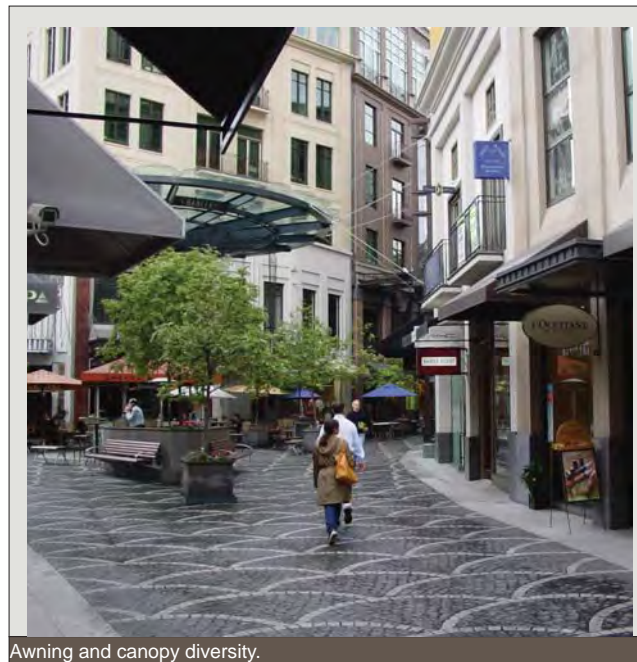
Sun Shade



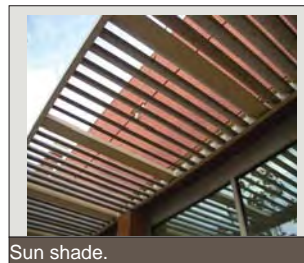
Curtain Wall



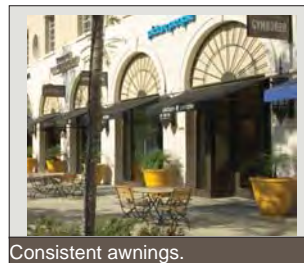
## 3.6 Awnings, Canopies & Building Projections



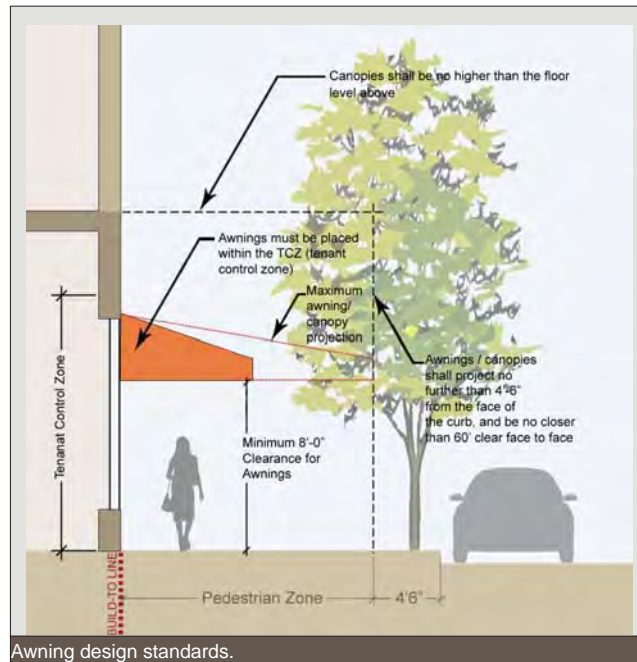
Awning and canopy diversity.



Sun shade.



Consistent awnings.



Awning design standards.

### Intent

- To provide visual appeal and functionality to the pedestrian environment through a system of shade and weather protection devices that enhance the use and experience of Konterra Town Center East.

### Required

#### 3.6.1 Building/Projection Compatibility

The design, material, visual quality and implementation of awnings and canopies shall be congruous, enhancing rather than detracting from the building's form, as they are an extension of the building to which they are attached.

#### 3.6.2 Awning, Canopy & Projection Locations

The location of awnings, canopies and projections is critical, so that they do not obstruct the signage, and views of the businesses/residences which they protect and enhance. Awnings must be placed within the tenant control zone, while canopies may be placed outside the tenant control zone. The placement of canopies and awnings shall provide the best protection from the elements at critical times of day as well as seasonal times of year to provide the greatest functionality of the outdoor development. Care must be given to the placement of awnings and canopies as they relate to code requirements for access and egress.

#### 3.6.3 Projection & Height for Canopies/Awnings

All awnings, canopies and projections must be at minimum 8 feet clear so as not to obstruct circulation. Awning and canopy projections will be a maximum of 4'6" from the face of curb, and a 60'-0" minimum clearance must be maintained between facing projections.

#### 3.6.4 Building Projections: Balconies etc.

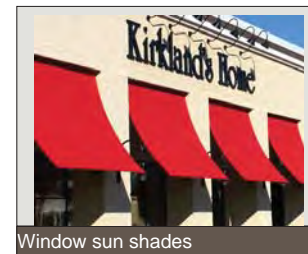
All building projections including, but not limited to, balconies, terraces, exterior walls etc. must adhere to the following standards regarding height and projection: (1) Projections may extend no further than 5'-0" past the build to line. (2) Projections must be at minimum 8'-0" above the ground plane so as not to obstruct the circulation and flow of pedestrians.

#### 3.6.5 Awning/Canopy Materials & Types

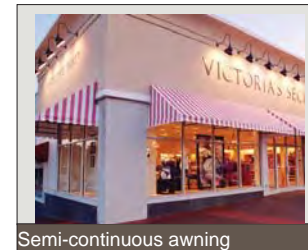
Where awnings and canopies are attached to facades that face public streets they should be permanent fixed structures. Adjustable awnings are permitted on patios, pedestrian passageways, and non-public street facing facades of buildings. All awnings should be made of durable, high quality materials.

#### 3.6.6 Signage on Awnings

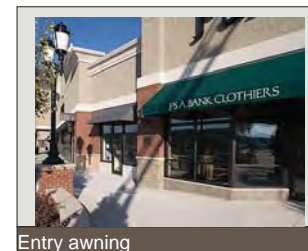
Please refer to section 6.0 on signage/graphics for these requirements.



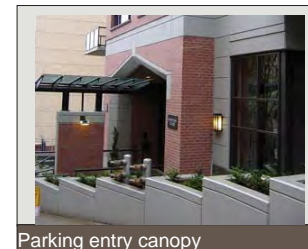
Window sun shades



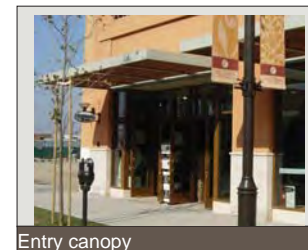
Semi-continuous awning



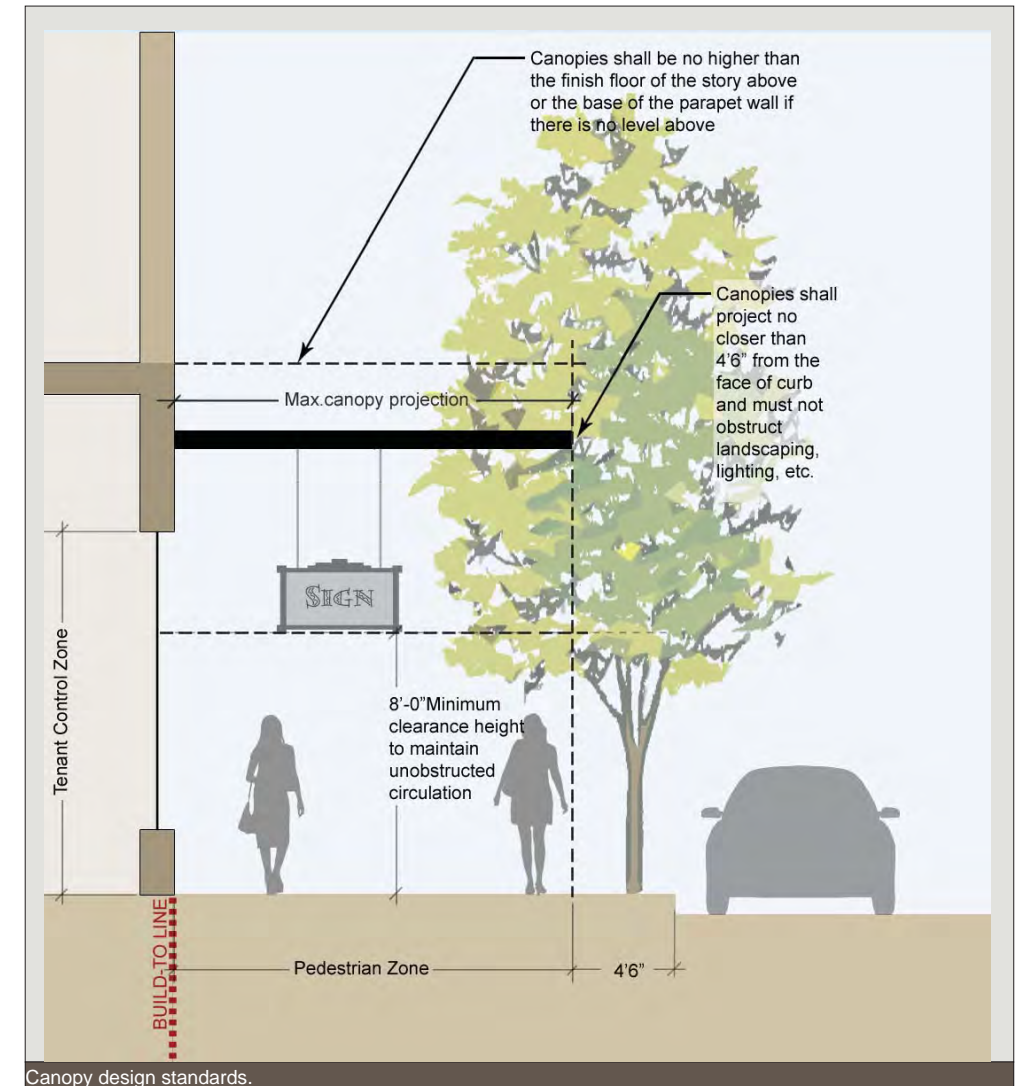
Entry awning



Parking entry canopy



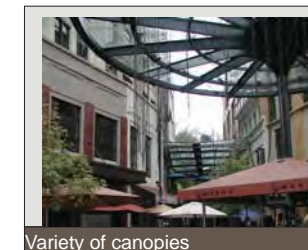
Entry canopy



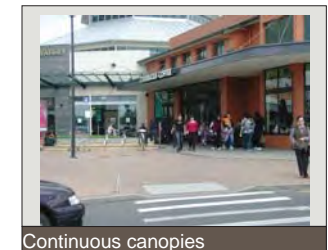
Canopy design standards.



Corner accent canopy



Variety of canopies



Continuous canopies

### Recommended

#### 3.6.7 Awning Color

Awnings are preferred to be solid in color. If patterned or polychromatic they should enhance rather than distract from the overall character of the streetscape.

#### 3.6.8 Awning and Canopy Alignment

In order to maintain a visual harmony and congruity, awnings should be placed in alignment with those on adjacent facades to reflect a consistent street line.



## 3.6 Awnings, Canopies & Building Projections

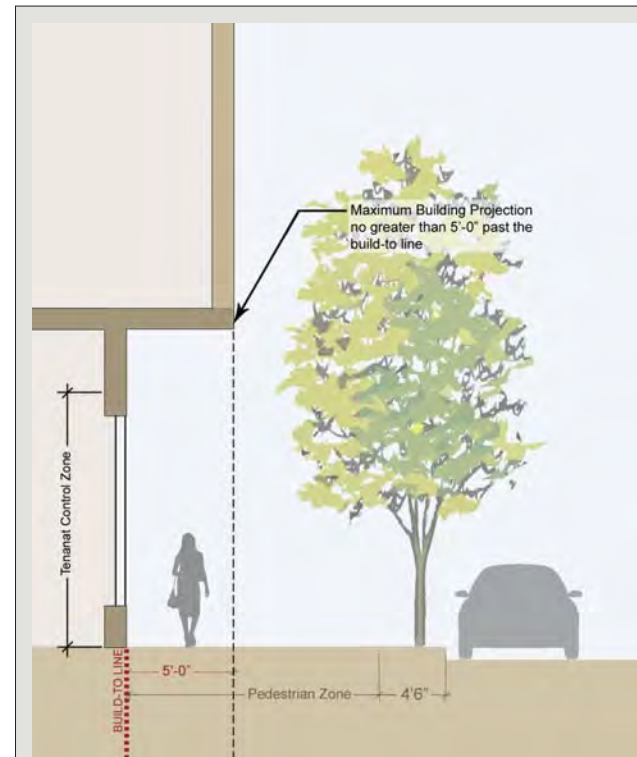
### 3.6.9 Awning/Canopy Variety

Variety in the design and material of awnings and canopies is encouraged throughout the development to contribute to and create the vibrant urban fabric within Konterra Town Center East. This is to be achieved within the limits set forth and should be compatible with the overall building and streetscape concept for the development.

### Unacceptable

#### 3.6.10 Placement of Awning/Canopy Supports

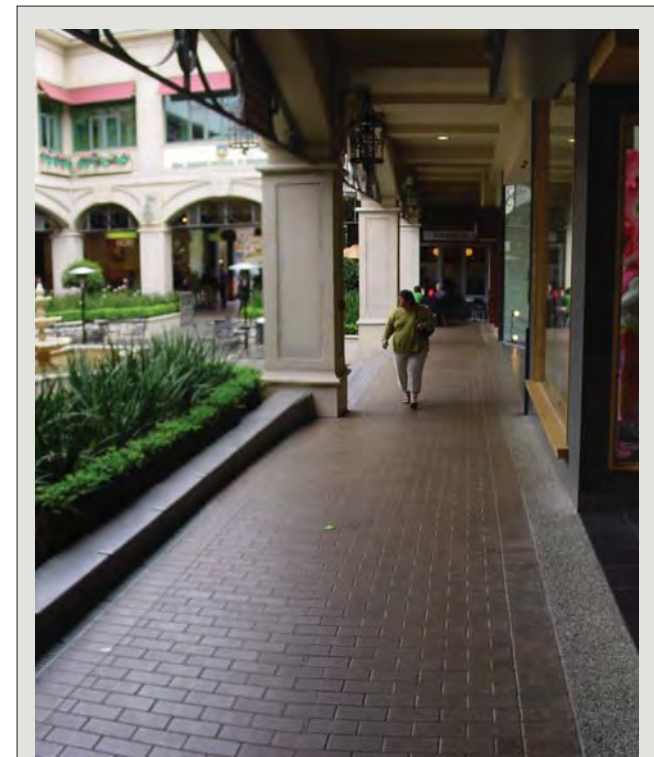
Awnings and canopies should be supported by the facade wall to which they are attached. Supports such as columns which would require placement in the pedestrian or landscaping zone are not permitted.



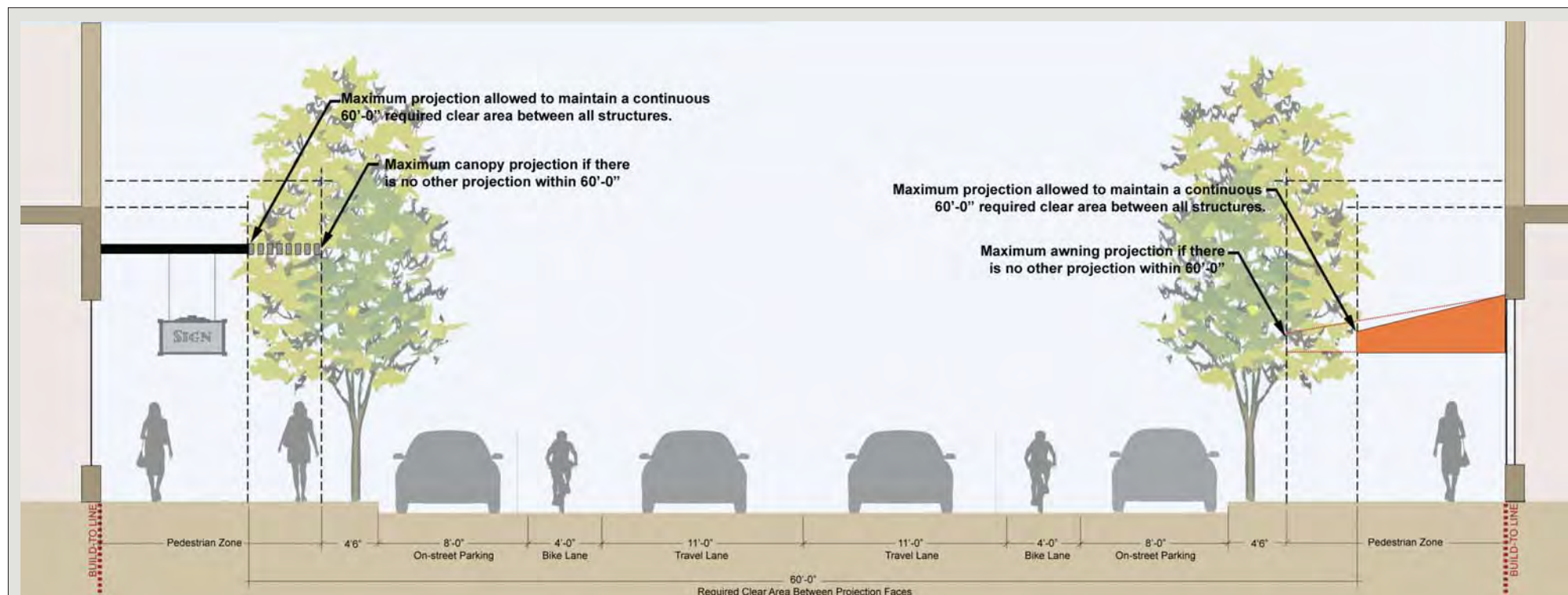
Building Projection Standards.



Terrace walkway building projection.



Building projection acts as covered walkway at grade.



Standard regarding 60'-0" required clear area between buildings and their projections.



Second story canopies.



## 3.7 Building Fenestration

### Intent

- To encourage transparency and therefore interaction between active uses
- Increase visibility of activity throughout the day and at night, to activate and further engage pedestrians both indoors and outdoors
- To reinforce human scale and create a relationship between built the environment and its users
- To distinguish between active and passive facades. Requiring fronts visible to the public realm to be more highly fenestrated and allowing facades that are hidden or rarely visible to be less fenestrated.

### Required

#### For Street Level Facades that Face the Public Realm:

##### 3.7.1 Street-Level Fenestration

To encourage a dialogue between indoor and outdoor uses and activities, transparent glass storefronts are required on street-level facades. This transparency will give the illusion of a larger public realm extending from the streetscape into the buildings themselves, and incorporate the use of natural light.

##### 3.7.2 Minimum Street-Level Fenestration

The minimum amount of street level fenestration varies by building use. For retail buildings (including office over retail and residential over retail) no less than 75% of the street-level facade can be transparent glazing. All other uses not including retail shall be no less than 40% transparent glazing. The area between the finished floor and 12'-0" above the finished floor is the zone which must adhere to the above minimum fenestration requirements. (See section 4.0)

##### 3.7.3 Glass Selection and Reflectivity

The transparency and reflectivity of glass will allow visibility between pedestrians on the sidewalk and the ground floor use inside. Glare should be kept to a minimum to insure comfort and safety. Low E, thermal insulated glazing should be used. No mirrored glass will be permitted.

### Recommended

##### 3.7.4 Respond to Solar Orientation

The design of upper level facades should reflect/react to their siting in regard to climate and solar exposure to create the most responsible and sustainable building design possible.

### Acceptable

##### 3.7.5 Opaque/Ornamental Glass

Ornamental, opaque or stained glass may be used minimally as specialty windows to accent or enhance a facade or screen structure/utilities from view.



Transparency allowing views to active uses.



Higher percentage of glazing on ground floor.



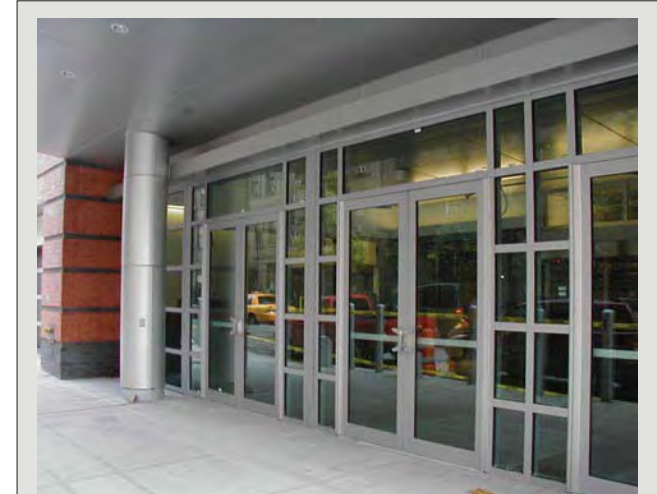
Transparency for street-level windows.



Glass is too tinted and reflective.



Transparency allowing views to active uses.



Transparency allowing views to active uses.



Transparency allowing views to active uses.



Transparency in upper level windows.



Transparency of Ground floor retail.



## 3.8 Storefront Design

### Intent

- To develop vibrant, unique, visually engaging and well designed storefronts that draw people in and act to enhance the character of Konterra Town Center East.
- To activate the pedestrian realm by providing a high level of retail activity throughout the downtown core.

### Required

#### 3.8.1 Storefront Design

Will enhance the streetscape and pedestrian realm by giving individualized identity to storefronts and building entrances and outwardly expressing the uses within. Storefront designs should be engaging and foster a visual relationship between the streetscape and the building uses inside.

#### 3.8.2 Storefront Entries

Storefront entryways should be attractive and engaging along the street front to interest shoppers and promote pedestrian movement. Entrances should be close in proximity to each other to encourage walkability and to offer variety in design. Thresholds should be compliant with ADA requirements to accommodate easy access by all persons.

#### 3.8.3 Storefront Security Device Placement

All security devices such as folding doors and overhead rolling grilles should be incorporated entirely into the storefront design so they are not visible from the public realm during open store hours.

#### 3.8.4 Entryway Transition

In order to provide a necessary transition zone between inside and outside, and to accommodate door swings and pedestrian traffic, building entries that front public streets will be recessed into the storefront.

#### 3.8.5 Storefront Positioning

Storefronts and entrances to other uses will orient toward public street to engage pedestrians and activate the public environment. Where buildings do not face a public streets (for example if they instead face a park or plaza) entrances and storefronts will front toward the major pedestrian realm.

### Recommended

#### 3.8.6 Storefront Proportion

Storefronts should be scaled and detailed for comfort and use. Large facades of buildings should be broken down into smaller more pedestrian friendly increments, and differentiated to avoid monotony. Surfaces should contain architectural and urban elements, signage, material/color changes, planar shifts and other devices to activate and stimulate pedestrian and vehicular interest.

#### 3.8.7 Storefront Streetwall

Storefronts should typically work together to create a consistent streetwall. Parameters for facade plane variation and exceptions for plazas etc. should be incorporated to

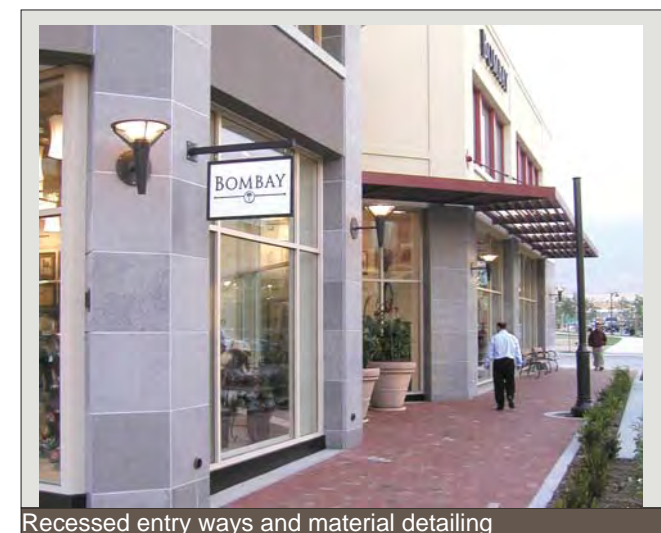
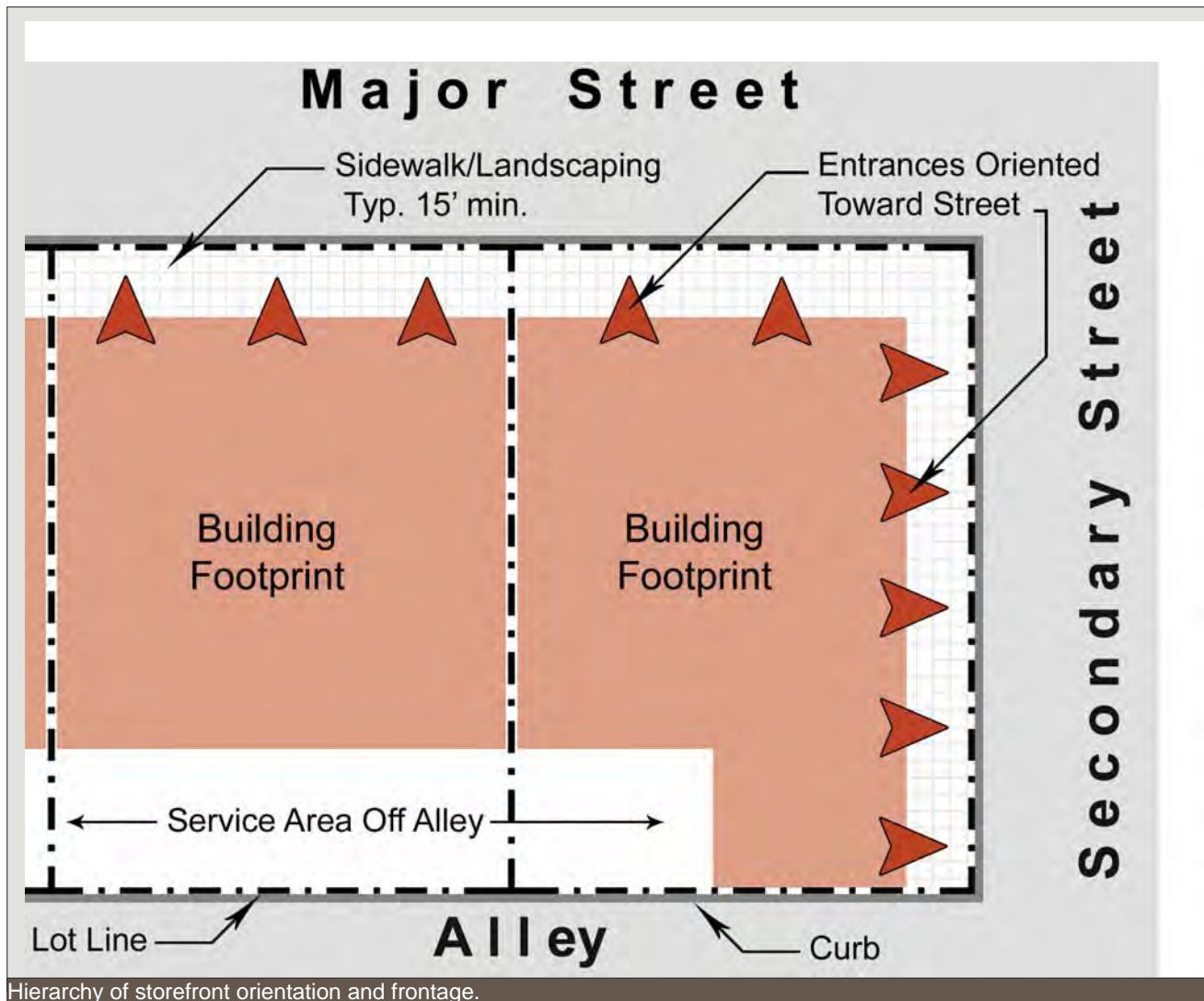
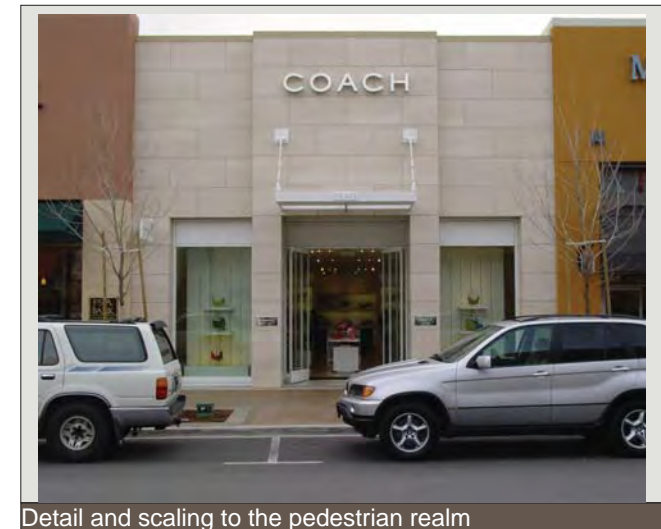
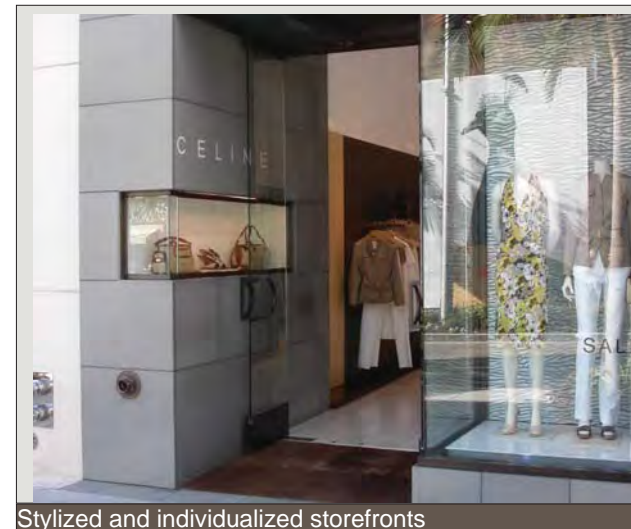
prevent monotony. Storefronts should not extend into the sidewalk zone except for allowed overhead awnings, signage, canopies and building projections (like balconies) that do not obstruct pedestrian movement.

#### 3.8.8 Storefront Facades

Facades should be diverse in architectural expression offering variety in detailing, color, vertical and horizontal variations in facade plane, textures and materials.

#### 3.8.9 Storefront Diversity

Individualized storefront designs and expressions should be encouraged over a repetitive series of storefronts. Multiple small storefronts are preferable to longer more sparsely spaced storefronts. The overall goal is to create a lively, visually diverse and active pedestrian oriented environment that conveys the uses of the tenants within in an individualized and stylized way.





## 3.9 Articulation

### Corner Enhancement



Dramatic corner entry canopy.



Rounded recessed and heightened corner condition.



Double height, extruded and chamfered corner condition.

### Change in Plane



Balconies projected or recessed activate the facade.



Creates a plaza & minimizes the mass of larger building.



Example of projected and recessed building plane.

### Intent

For buildings to create a comfortable and diverse streetscape and pedestrian realm, that is human in scale and varied in material use, color, texture, meter, form and proportion. Facades must be articulated per their category, i.e. public facing front facade, publicly visible side facade or hidden back facade. (See section 3.13)

### Required

#### 3.9.1 Articulation through Variation in Plane

Street walls at the ground level should incorporate a variation in wall plane every 30 linear feet or less of building frontage. Variations should not be less than 4 inches in depth or projection. Parapet walls should change in height no less than every 60 linear feet. Change in height will be no less than 2 feet.

#### 3.9.2 Building Scale and Variation

Buildings with upper stories of other uses should be differentiated by incorporating design elements that reflect their use, i.e. balconies for residential units. Change in material and color are also encouraged. Uses should be distinguishable by design and differentiated in elevation. The scale of the building shall remain conscious of the desired pedestrian oriented development and offer a comfortable human scale at ground level.

### Recommended

#### 3.9.3 Facade Variation

Facade variation should reflect the structural grid of the building and the use within. Certain areas such as primary entryways and building corners should be accentuated to indicate prominent points of passage and engagement as well as to provide individualized design moments.

#### 3.9.4 Projections

Projections including balconies, terraces and building walls are encouraged to be incorporated where appropriate, to further activate publicly fronting facade walls and create visual interest. Projections may protrude no further than 5 feet past the build-to line and may not occur at ground level.

#### 3.9.5 Structural Articulation

Should be expressed, detailed and articulated to enhance the fabric of the building facade, creating depth with shadow though changes in plane and material variation.

#### 3.9.6 Accentuate Corners

Buildings on the corners of Main Streets should be given priority as areas to enhance, articulate and emphasize. Corners will serve as landmark building opportunities within the project. (See section 3.10)

#### 3.9.7 Light Filtration

Buildings on the south side of streets are encouraged to be lower in height and/or stepped to allow sunlight to filter to street level throughout the day within the project.

### Material, Color, Texture



Horizontal variation in material, texture and color.



Vertical material and use differentiation.



Variation of material in architectural elements.



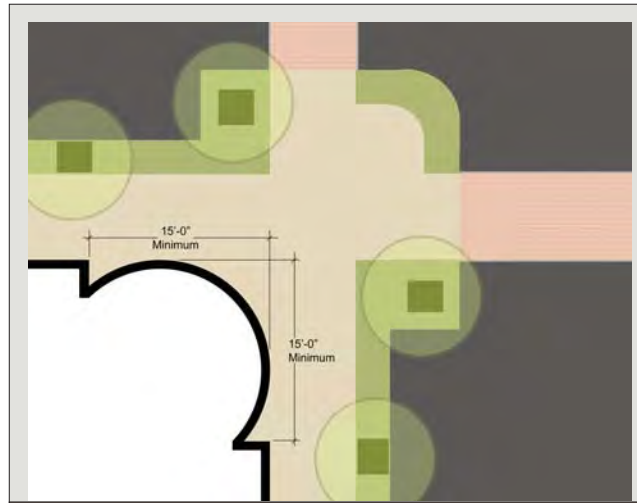
## 3.10 Corner Conditions



Example of extruded corner condition.



Example of recessed corner condition.



Example of rounded corner condition.

### Intent

To create visually accented street corners that differentiate blocks, add visual identity as well as activity to street views, and aid in way finding.

To enhance the urban feel of single story development by creating visually heightened buildings that add variation to the street wall.

### Required

#### 3.10.1 Single Story Corner Height

Where corner buildings are one-story (i.e. single story retail) and face onto 'Main' streets, the corner of the building will be required to be no less than one and a half times the height of the rest of the building. This change in height must occur for a minimum distance of 15 linear feet in each street facing direction measured from the outermost point of the building 'corner'.

#### 3.10.2 Multi-story Corner

Where multi-story buildings occupy the block corner facing onto 'Main' streets, they are required to address the intersection that they front in one of the following ways:

- **Height:** increase building height by no less than half a typical floor height for 15 linear feet of distance in each street fronting direction.
- **Roof Shape:** The roof shape at the corner must be articulated differently from the rest of the building to add variety to the building's silhouette.
- **Rounded/Chamfered Corners:** Changing the shape of the corner of the building, i.e. rounding or chamfering the wall planes to alter the perception the corner. This must be done for no less than 15 linear feet of distance in each street fronting direction.
- **Recessed/Extruded Corners:** Recessing or extruding at minimum the ground floor level of a corner building for a difference of no less than 6 feet from its adjacent facade plane for a minimum distance of 15 linear feet in each street fronting direction.

### Recommended

#### 3.10.3 Entry ways

Entry ways should be located in or near the corner of the building and be incorporated into the accented design.

#### 3.10.4 Materials/Colors

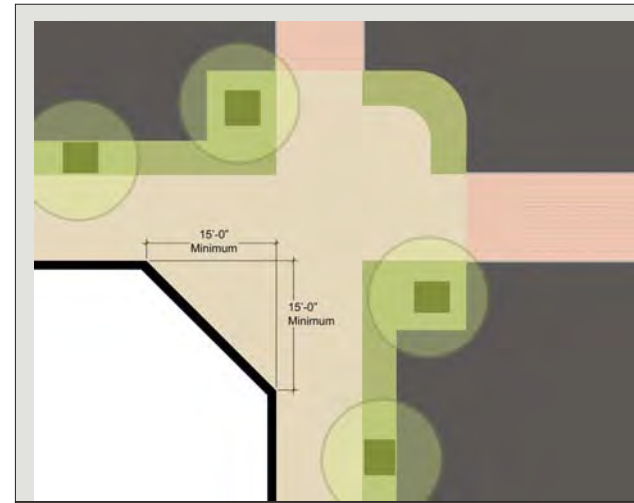
Further differentiation at the corners through material and/or color changes is also encouraged.



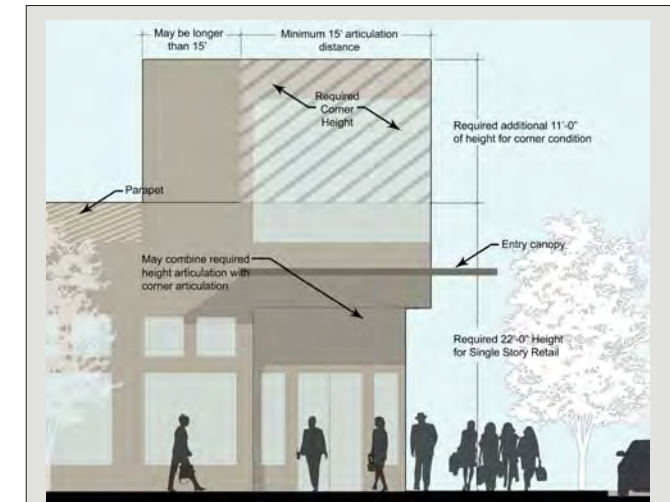
Example of recessed corner condition.



Example of extruded corner condition.



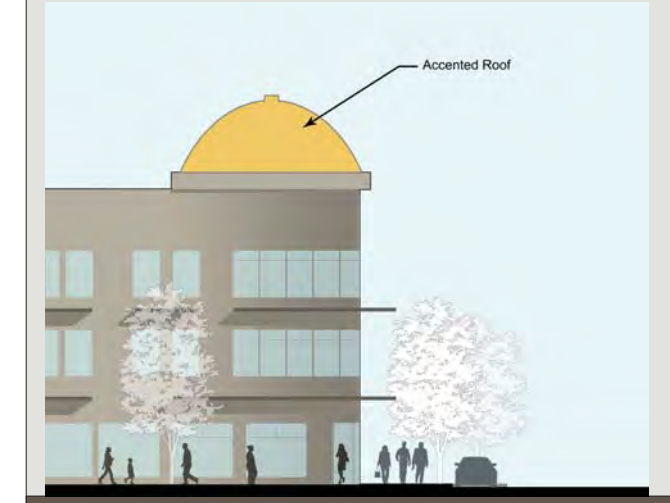
Example of chamfered corner condition.



Additional height required for single story corner buildings.



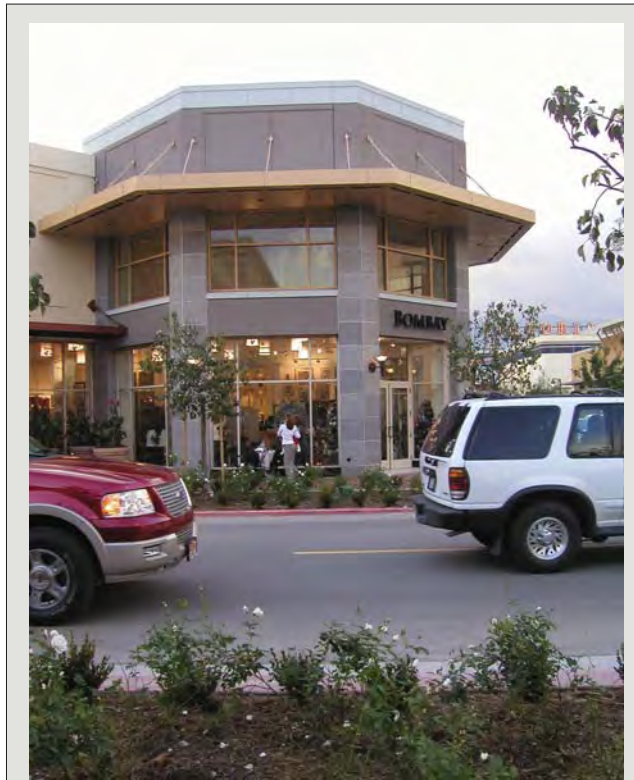
Corner may be square in plan if it is altered in elevation.



Multi-story buildings may accent corners with roof only.



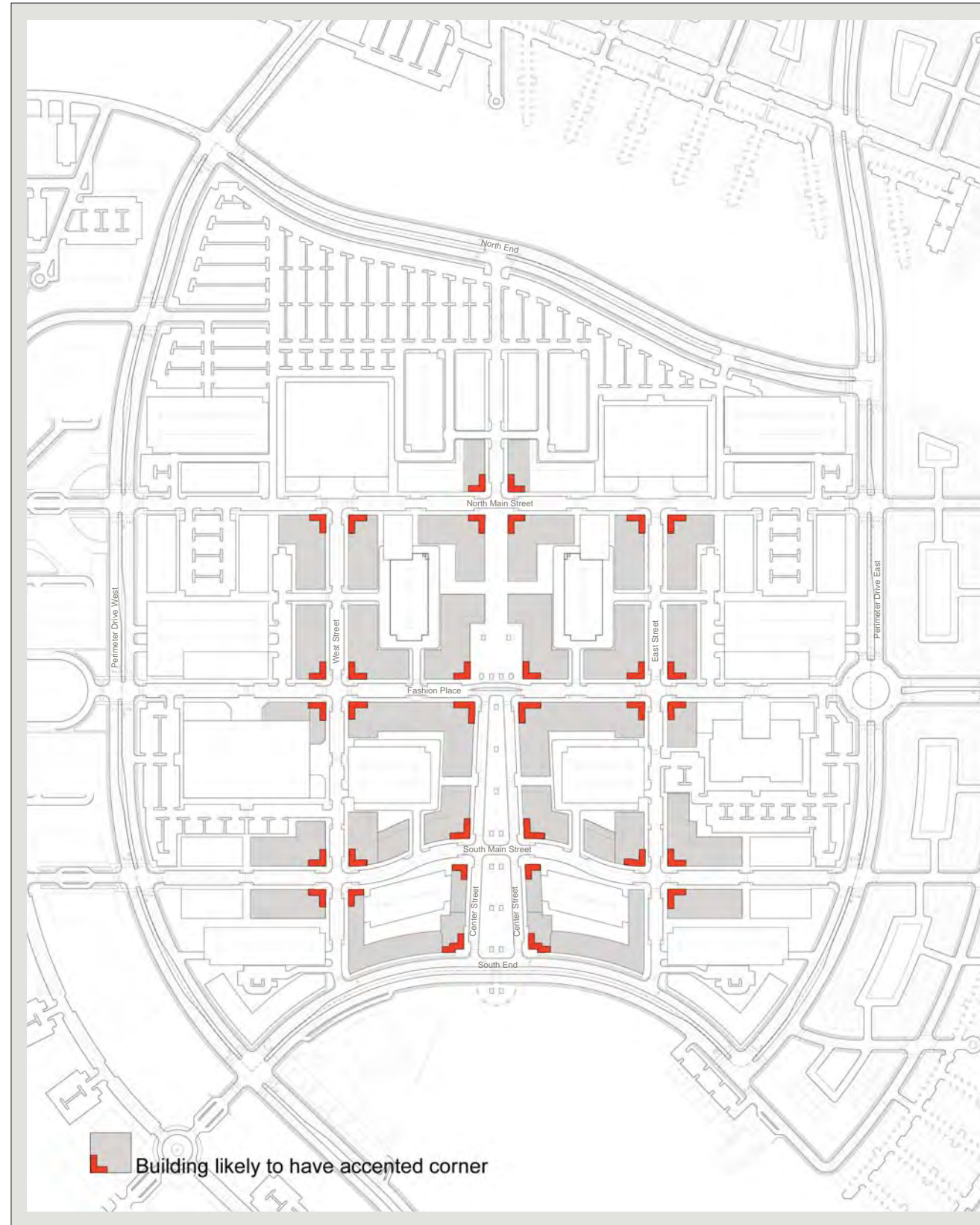
### 3.10 Corner Conditions



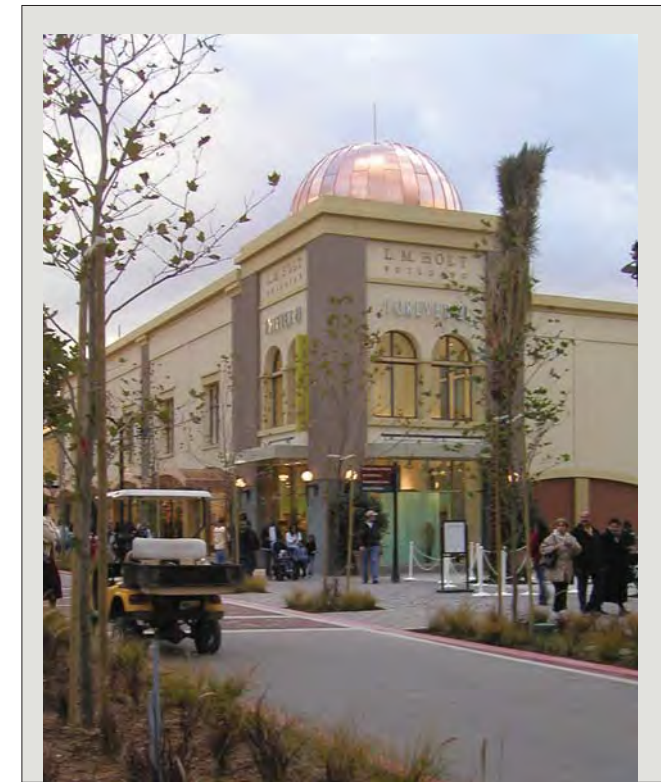
Example of extruded corner condition.



Example of recessed corner condition with large canopy.



Example of chamfered corner condition.



Example of accented roof corner condition.



## 3.11 Building - Parking Relationships

### Surface Parking:

Surface parking lots will be landscaped and paved to break up large expanses of asphalt making them more pedestrian friendly and easier to navigate. Trees will be planted in parking islands providing shade and reducing heat island effect to control temperature and possible micro-climate conditions. Special paving will also act as a safety feature indicating areas where pedestrians are likely to be.



Surface parking lot with landscaping.

### Parking Courts:

Parking courts are smaller surface parking areas that are contained within the middle of the block or between buildings in a courtyard type condition. Courts will be landscaped to provide shade and to aesthetically enhance these areas that also serve as pedestrian connections from building to building. These lots will typically serve the buildings adjacent to them i.e. as employee parking.



Parking court between buildings.

### On-street Parking:

Street parking will occur throughout Konterra Town Center East and will primarily serve short term parking users. Street parking will be an important component of the streetscape character and will act as a buffer between traffic moving on the street and pedestrians on the sidewalk. Parallel parking also acts as a traffic calming device, allowing cars time to pull in to, and out of, spaces as a further safety measure.



On-street parallel parking.

### Intent

- To provide safe and comfortable conditions between parking (both lots and structures) and the buildings that they serve.
- To provide the necessary parking stalls in an efficient manner to facilitate trips from parking to the desired destination of the user.
- To reduce the visual impact of parking within the development by concealing the structures within the core of the block and limiting their height relative to nearby buildings.

### Required

#### 3.11.1 Noise from Parking

Structures that attach directly to buildings will be designed in accordance with the building code requirements for separation to minimize noise impact on adjacent uses.

#### 3.11.2 Parking Structures Visible to the Public

Where parking structures are adjacent to or visible from public view, attention must be given to the design of the exposed parking structure facade to ensure that the quality, and appearance of the parking facade wall matches the character, quality, and material palette of the public realm to which it is a part.

#### 3.11.3 Parking Structure Ventilation

The ventilation system for parking structures will not release exhaust directly into public open spaces or pedestrian areas. These public spaces will also be insulated from the noise associated with such systems.

#### 3.11.4 Parking Structure Lighting

Parking structure lighting will be designed to be sufficient and comfortable, to promote a safe and positive experience for those using the structures. (See section 7.0)

#### 3.11.5 Parking Access From Pedestrian Areas

Access to parking will be convenient, well lit, safe and functional within the project. Pathways leading to parking areas will be easy to find and navigate as well as be comfortable.

### Recommended

#### 3.11.6 Shading Devices and Amenity Decks

Parking structures, attached or adjacent to residential uses, will be considered for adapting the top most level to include amenity decks for recreational use.

#### 3.11.7 Rooftop Design

Incorporating outdoor usable space within parking structures is encouraged to provide public and/or residential recreational space, as well as to provide green roofs and attractive roofscapes that may be visible from taller nearby buildings. Care should be given to make these decks as aesthetically appealing and functional as possible.



Stand alone parking structure.

### Stand-Alone Parking Structures:

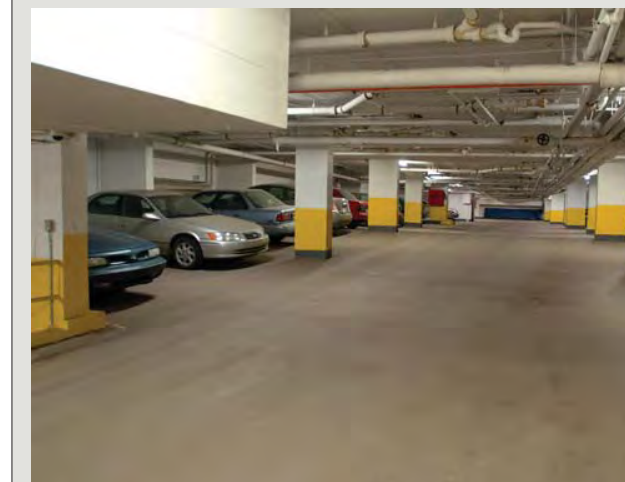
The majority of parking structures in Konterra Town Center East will be stand alone structures, located in the center of the block, and surrounded by buildings of other uses. These structures should not dominate the block in height and should be detailed and articulated to streetscape standards on the facades that are visible from the public realm.



Attached parking structure.

### Attached-Parking Structures:

In some instances parking structures will be directly attached to the building they serve. In these cases care will be given to properly control noise and vibration from the parking structure. Structures will be located to the rear of buildings and entry ways will be clearly marked on the street to aid vehicle and pedestrian way-finding.



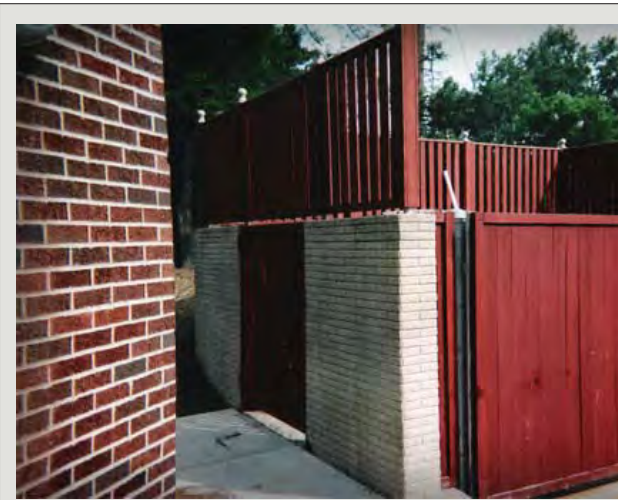
Possible below grade parking structure.

### Below Grade Parking:

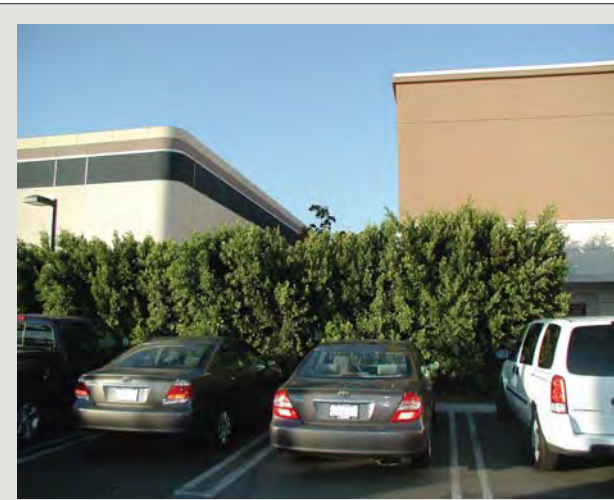
There are opportunities to incorporate underground parking into Konterra Town Center East, taking advantage of the natural terrain of the site. In such cases entry ways will be clearly marked for easy way-finding. Lighting and signage will be integral to the success of the underground parking and should create comfortable and safe pedestrian and vehicular use.



## 3.12 Storage, Loading and Screening



Solid trash enclosure.



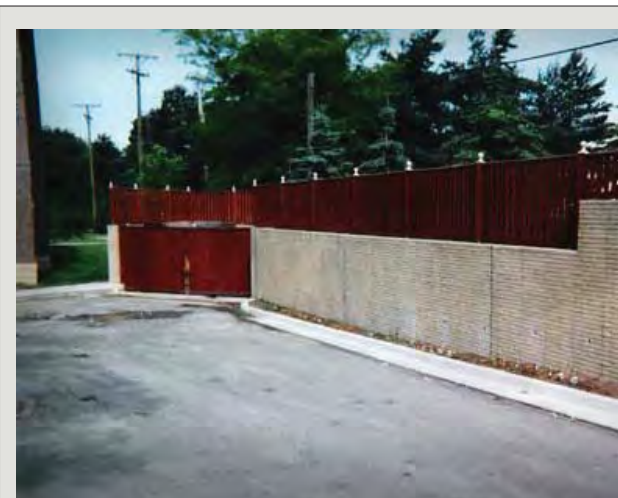
Landscape screen between buildings and parking.



Parapet wall high enough to shield mechanical equipment.



Screen for mechanical equipment.



Brick wall trash enclosure.



Service/loading can happen behind and under buildings.

### Intent

To locate service, trash collection, equipment storage and loading zones in areas where they will have the least visual and noise impact on Konterra Town Center East.

### Required

#### 3.12.1 Visibility from Public Areas

Service areas, trash enclosures etc. will be located to the rear of buildings, and mechanical equipment will be masked by the parapet when located on the roof of a building. Care will be given to ensure that loading docks, trash storage, service alleys and mechanical equipment are not visible from the public/pedestrian areas.

#### 3.12.2 At Grade Screening & Buffering Options

The concealment of loading docks, storage areas and mechanical equipment from public view will be achieved through the use of screening walls, made of acceptable materials that either stand alone or are incorporated into the design of the building it serves. Landscaping may also be used in some cases as long as it is sufficient to conceal the area.

#### 3.12.3 Rooftop Concealment

Mechanical and utility equipment must be concealed from public view when located on the roof of a building. The height of the parapet wall must be equal to, or greater than the height of the equipment it will conceal. The interior wall of the parapet should compliment the roof color of the building.

#### 3.12.4 Ground Mounted Utilities

With the exception of electrical rooms and fire sprinkler risers all other ground mounted utilities will be located outside the building. Ground-mounted utilities should be screened with walls, landscape or incorporated into other screened enclosures such as trash or storage areas.

### Recommended

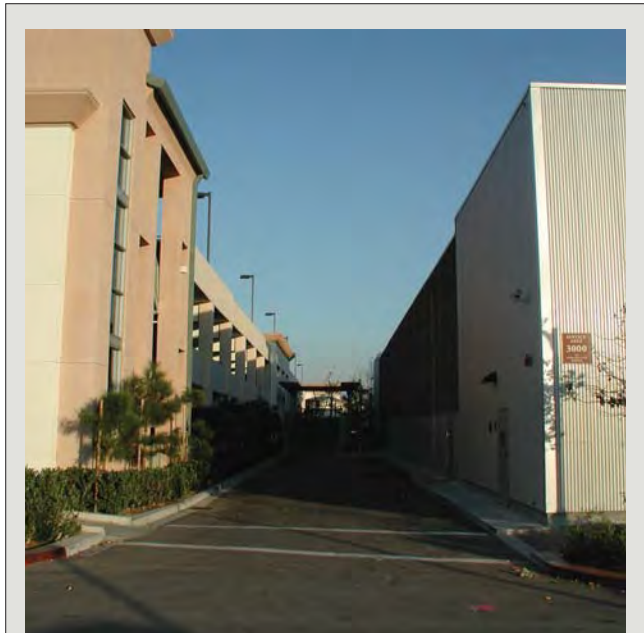
#### 3.12.5 Loading Areas

Loading areas should be sized to adequately serve buildings allowing for ease of ingress, egress and needed maneuvering of vehicles and products. The servicing of buildings should not obstruct or disrupt on-street traffic.

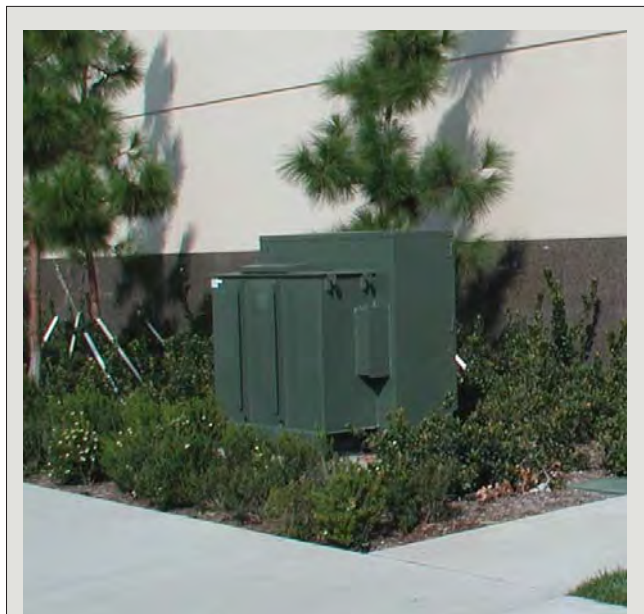
### Acceptable

#### 3.12.6 Roof Utility Screening

Where utilities need to be located on the roof of a building careful and adequate screening must be provided to minimize their visual impact on the surrounding site.



Discreet service alley.



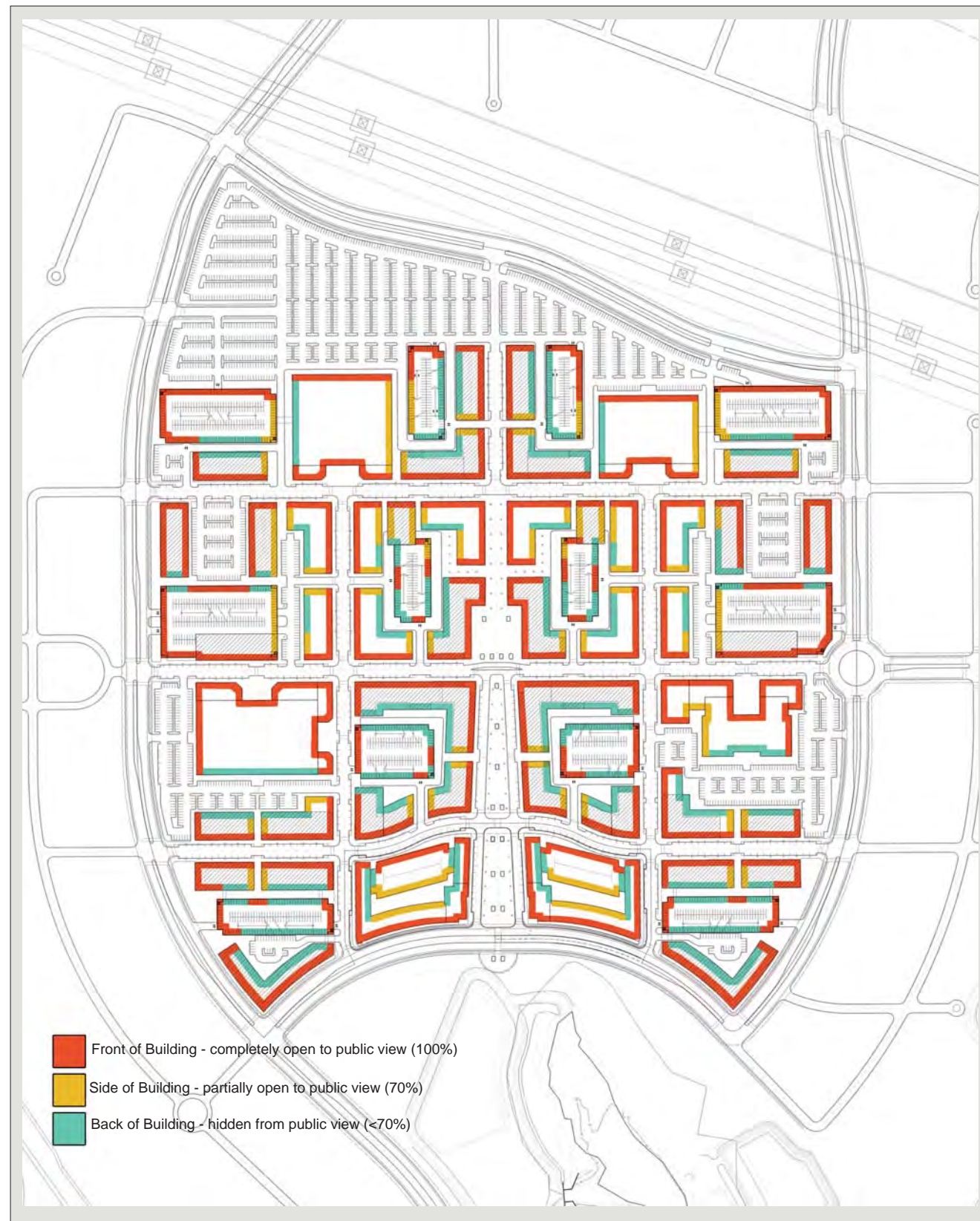
Landscaping that will grow up to hide mechanical equip.

#### 3.12.7 Landscape Screening

Landscaping, or a combination of landscaping with other screening devices, is acceptable if it provides complete visual concealment of the area. Time should be allowed for landscaping to mature to full growth.



## 3.13 Back of House



### Intent

To allow the “backs of buildings”, meaning facades that do not front onto public streets or that are not visible from the public realm, to be held to different standards regarding articulation and fenestration.

As the backs of buildings serve primarily a service function at the ground floor for delivery of goods or access to trash storage it is unnecessary and misleading for these areas to be articulated to the degree of the public facing facades. These areas should be treated as functional and utility driven so as not to invite people from the public realm into areas that are not intended for pedestrian or public use.

### Required

#### 3.13.1 Fenestration

The backs of buildings are not required to uphold the standards that public facing facades must maintain in terms of percentage of opening to solid surface. As many of these areas are utility oriented it is necessary that they meet these needs in terms of functionality, i.e., the size of loading dock bays and service access doors.

#### 3.13.2 Articulation

The back facades of buildings are not required to provide the same degree of articulation as public facing or visible facades. The following categories of articulation may be adjusted so that un-needed cost is not applied to areas that will not benefit the overall perception of ‘place’ that is to be achieved:

- **Street Wall** - The shift in plane every 30 linear feet is not required and may be lengthened or eliminated at the ground level depending on the major function of the back facade.
- **Projections/Balconies** - These elements are not required unless they serve a particular function to the back facade of the building, i.e. an overhead canopy to protect a loading dock from weather during the unloading of goods. Balconies at lower levels should not conflict with truck access and are undesirable if located too closely to trash collection areas or truck routes due to smell and noise concentrations.
- **Building Scale** - The back facade of a building should be scaled for its intended use whether that is a back entrance for patrons or a delivery zone for large trucks.
- **Building Corners** - Corners need not be articulated if they are not intended to be seen or used by pedestrians or patrons in vehicles.

### Recommended

#### 3.13.3 Material Continuation

The back of building should not differ dramatically in ma-



Less articulated side facade.



Minimally articulated back facade.

terial composition from the rest of the building. While the articulation of the facade itself may be less detailed the overall building material should be consistent.

#### 3.13.4 Color Continuation

The color of the back facade should not differ dramatically from the adjacent walls. Colors should be uniform to present a clear and cohesive approach to the design of the building as a whole.

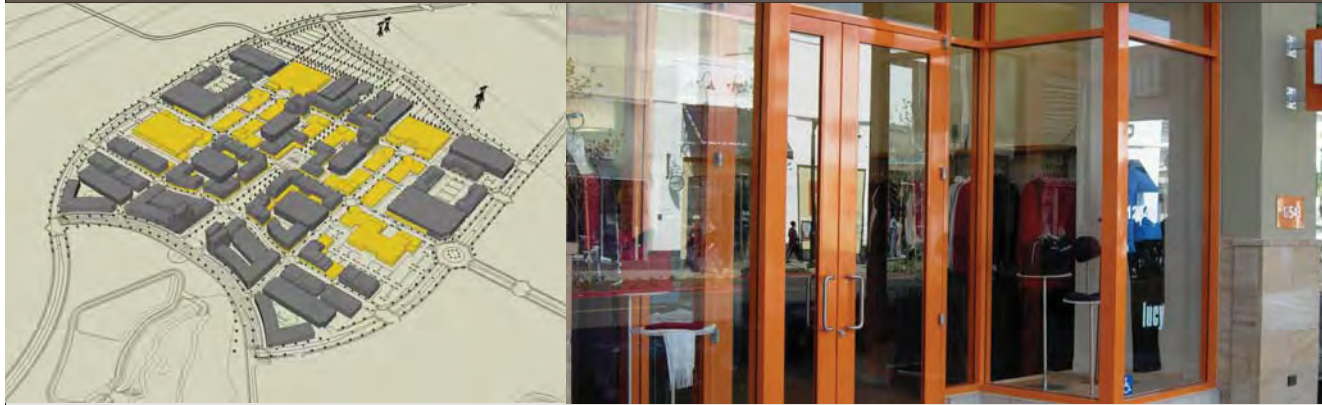
#### 3.13.5 Visibility from Upper Floors

Back facades that are visible from adjacent buildings should incorporate enough articulation (primarily on upper floors) to create visually pleasing views for neighboring buildings.



## 4.0 Building Typologies

### 4.1: Retail



### 4.4: Department Store Retail



### 4.2: Residential over Retail



### 4.5: Stand Alone Office



### 4.3: Office over Retail



### 4.6: Stand Alone Residential



**Note:** The first permit plans for sites located within the adjoining blocks that front on the Main Streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of trees, street furniture and lighting fixtures for that street on which the project site is fronting.



## 4.1 Retail

### Recommendations:

- Orient retail entries to face public streets and sidewalks, or if it does not face a public street then it should face onto the pedestrian realm.
- Locate parking to the rear of the building away from public view.
- Encourage pedestrian connections from parking areas to building entries at public streets.
- Screen trash enclosures, loading and service areas from public view using screens and/or landscaping.
- Buildings will be setback a minimum of 15' from the face of curb to allow for a consistent pedestrian sidewalk.
- Provide outdoor dining terraces with tables, chairs, and other furniture such as display shelves to bring activity to the street.
- Encourage architectural expression of building entry features
- Screen mechanical equipment from view
- Storefront canopies may extended, at maximum, 4'-6" from the curb face (a 60'-0" clear area between buildings and their projections must be maintained at all times for fire safety)
- Ground floor retail storefronts will be no less than 75% transparent to encourage views into active uses.
- Changes in parapet height must occur no less than every 60 linear feet, and be no less than 2 feet in height.
- Horizontal material change is encouraged to distinguish stores from one another along the street edge.
- On street parking should be provided to facilitate short term or one stop trips and act to buffer street traffic from pedestrian sidewalk traffic.
- Awnings and canopies are encouraged to provide shade and protection from the weather.
- Signage shall be restricted to the size and location determined in this document.



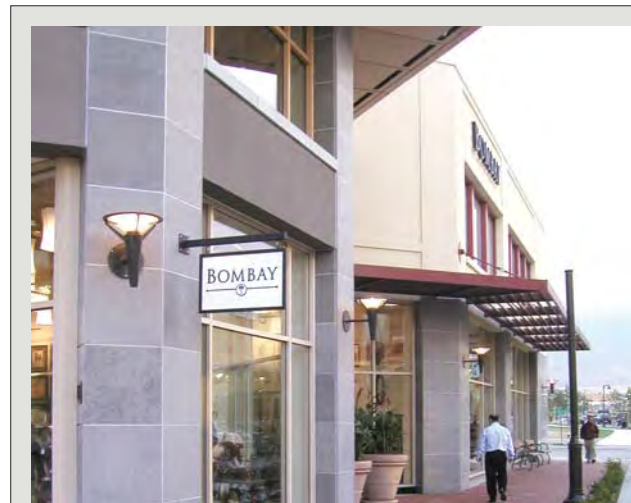
Storefronts are individualized and articulated by material.



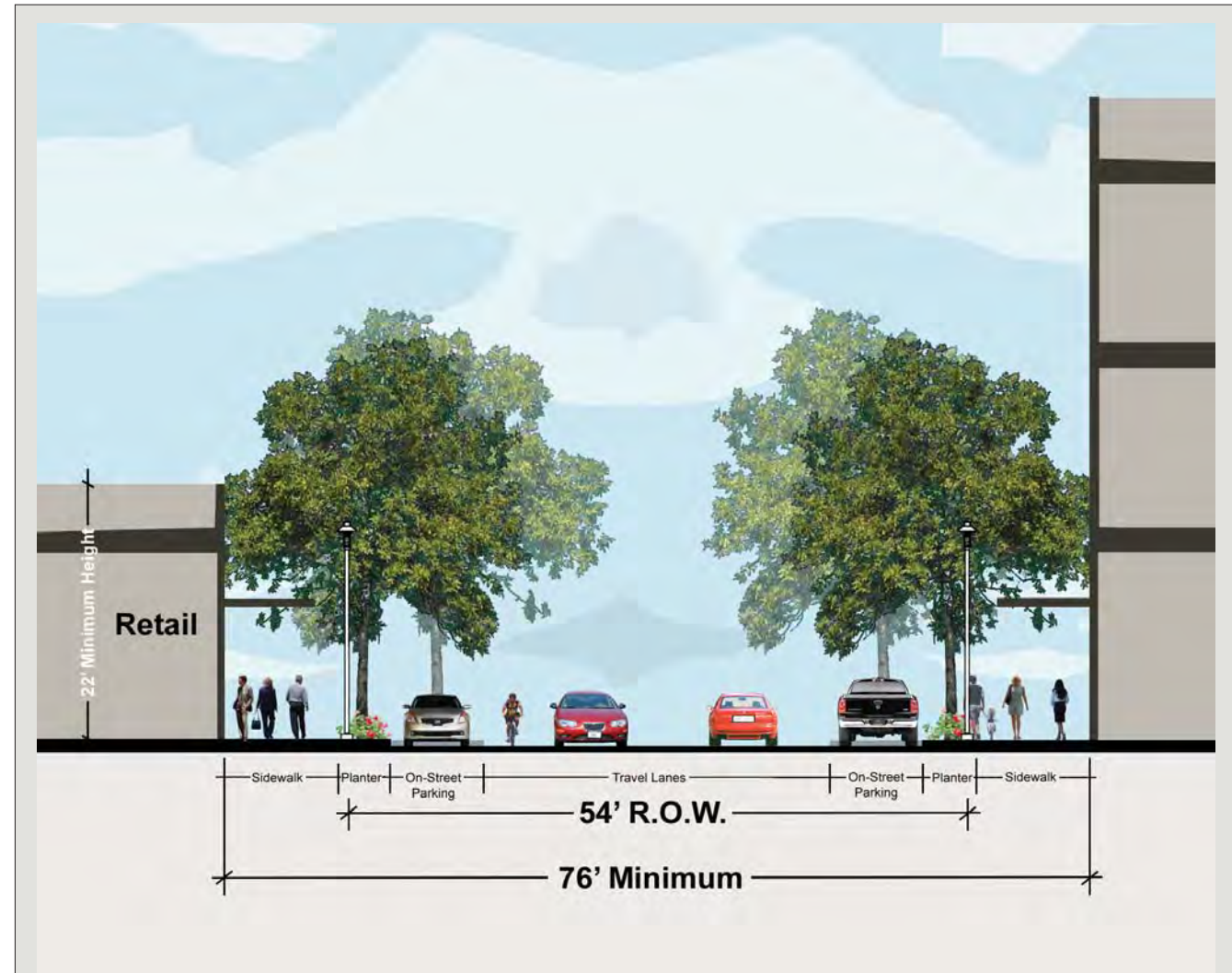
Transparency allowing views to active uses.



Retail only buildings highlighted in yellow.



Double height spaces help to reinforce the urban scale.



Cross section of retail only building opposite office over retail building.

### Retail

Konterra Town Center East will be an active retail destination to those residents who live inside the local community as well as the greater regional area. As such it is important to provide a variety of retail building typologies that can house the necessary tenants to make the development successful. Creating the use breakdown of a mixed-use development is dependent on many factors including: market forces, the capability of infrastructure to absorb additional capacity, and the ability to supply the type of product necessary to attract both tenants that will draw shoppers as well as buildings that will draw tenants. For these reasons it is necessary to incorporate single story retail into Konterra Town Center East.

Although there is a fair amount of retail planned for Konterra Town Center East, the design parameters that govern the end product insure that the intended overall urban character and sense of 'place' is upheld.

The way in which the building relates to the street is integral to the pedestrian experience. As a pedestrian oriented development this relationship is crucial to its success. In order to maintain the desired urban feel a required height minimum of 22 feet gives the impression of a second story to the retail buildings. This height minimum is only

3 feet lower than the minimum requirement for retail with uses above, i.e. residential over retail or office over retail. The additional height helps to create consistency throughout the project, and acts to frame the streetscape to a comfortable human and urban scale.

The incorporation of retail only buildings is also beneficial to the project because it is intrinsically lower in height which allows sunlight to filter down to the street level and also permits greater opportunity for air circulation. These qualities help to enhance the overall pedestrian experience and make for a more comfortable and appealing environment.

Retail will be a major contributor to economic growth and increasing job opportunities in the area. Konterra Town Center East will be both a destination place for shopping as well as a true local community. The retail only building component of the development offers variety in product type which may in some cases appeal to certain types of tenants which prefer not to have other uses stacked above. By providing this typology it opens the development up to a broader scope of retail tenants and allows for the greatest mix of retail product to best serve the community.



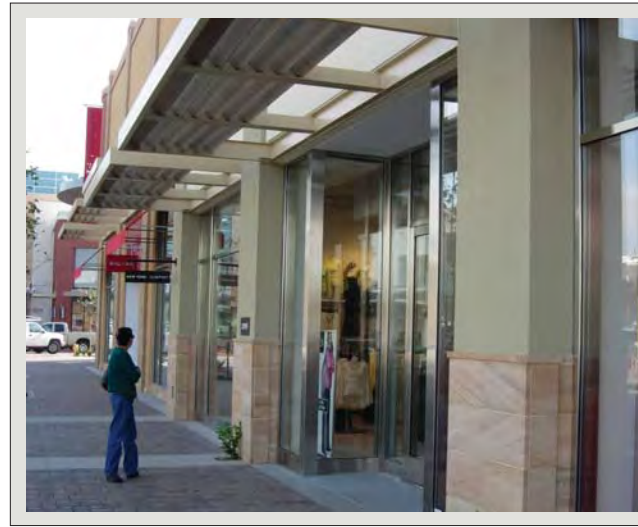
## 4.1 Retail



**Parapet Breaks:** Will occur no less than every 60'-0" of linear distance and change no less than 2 feet in height to add visual interest and relief from monotonous expanses of singularly planar walls.

**Setbacks for Facade Breaks:** Facade setbacks will be no less than 4" of difference in depth/projection from the adjoining wall.

**Awnings and Covered Walkways:** These elements are encouraged to enhance the street experience, further delineate uses as well as add distinction to individual tenants.

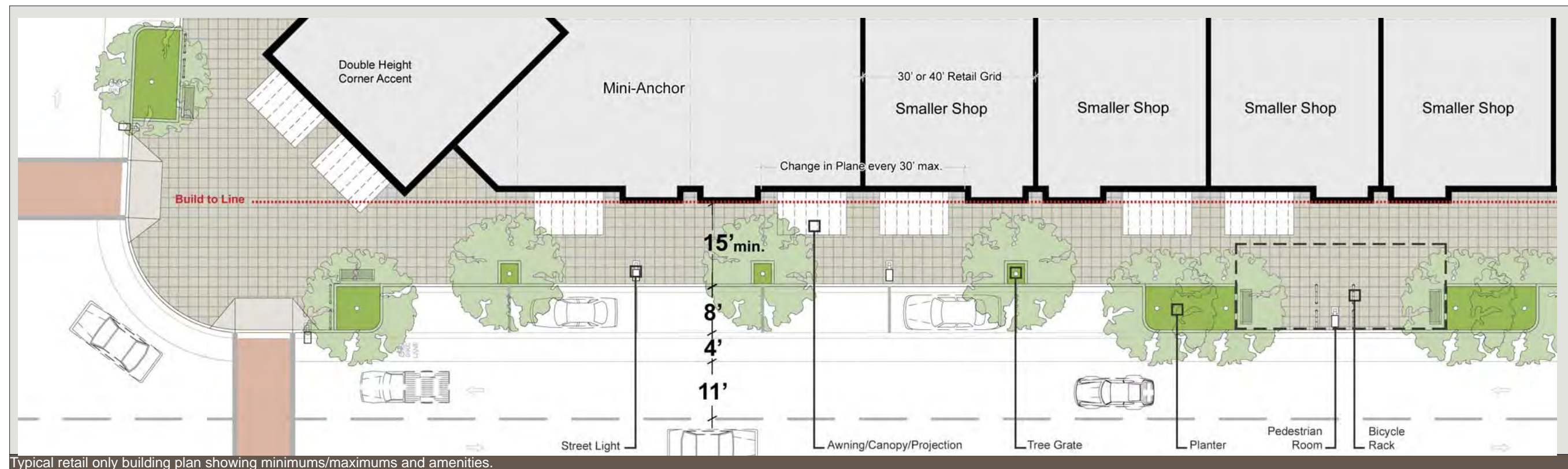


**Retail Setbacks:** Buildings will be setback no less than 15'-0" and no greater than 30'-0" in depth from the face of curb to provide a consistent pedestrian sidewalk and allow opportunities for plazas.

**Retail Facade Breaks:** Facade breaks should occur no less than every 30 linear feet, and be no less than 4 inches in depth, to prevent monotony. Where possible materials/appearance should vary between facade breaks.



**Pedestrian Rooms:** Where possible 'pedestrian rooms' shall occur where sidewalks and setbacks are at their maximums. These spaces should be enhanced with street furniture and landscaping to encourage use.





## 4.1 Retail

### Lot Coverage



#### Requirements:

**60% Minimum Lot Coverage**  
**100% Maximum Lot Coverage**

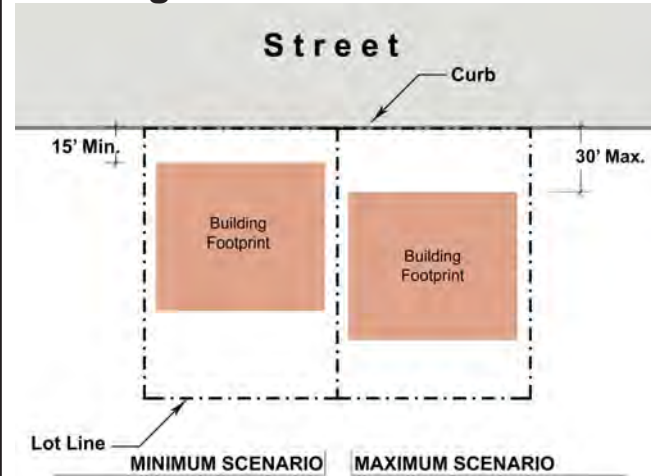
### Building Height Restrictions



#### Requirements:

**22' Minimum Height**  
 (from ground to top of parapet)  
**No Maximum Height Req.**  
 (from ground to top of parapet)

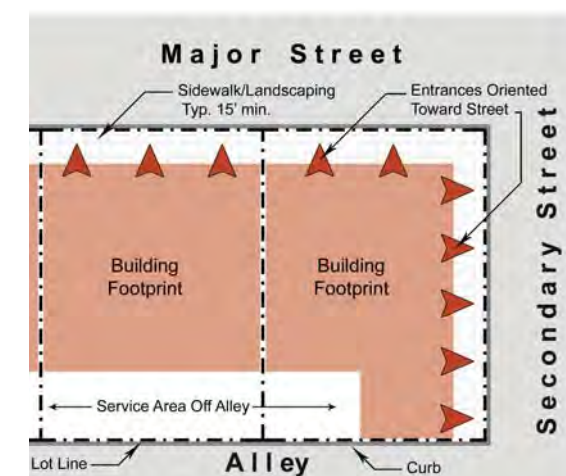
### Building Setback



#### Requirements:

**15' Minimum Setback**  
 (front of building to front of curb)  
**30' Maximum Setback**  
 (front of building to front of curb)

### Orientation / Service



#### Requirements:

**Locate Building Entrances so they Orient Toward the Street or public realm**  
**Locate Service Areas Off Alleys and at the Rear of Buildings**

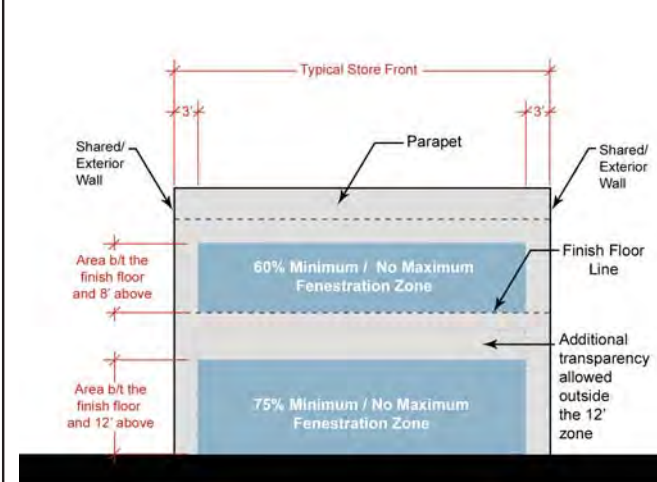
### Vertical Differentiation



#### Intent:

- To express each storefront architecturally using color, material, size and proportion to effectively achieve a coherent and thoughtful composition that reflects the various tenants within a block.

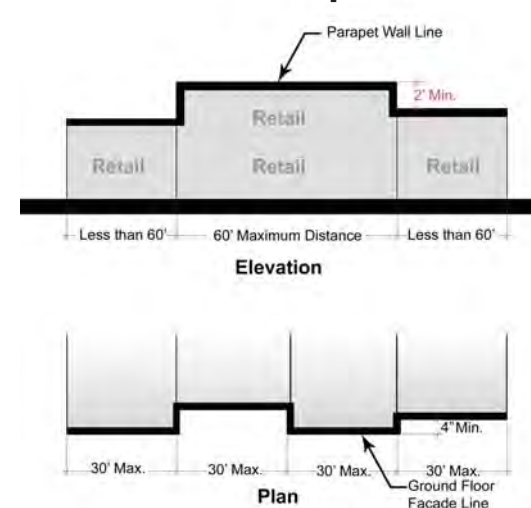
### Fenestration



#### Requirements:

**Ground Floor:** 75% Minimum Transparency within the Fenestration Zone  
**Upper Floor:** 60% Minimum Transparency within the Fenestration Zone  
 See diagram for definition of Fenestration Zone

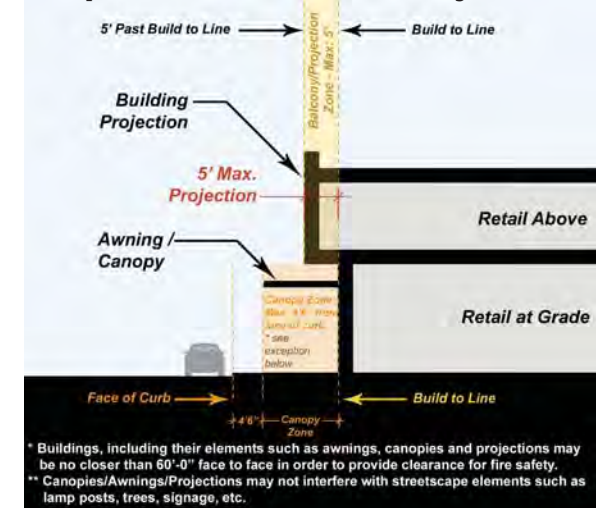
### Facade Break / Parapet



#### Requirements:

**Parapet Wall** should vary every 60 linear feet or less, to create visual interest.  
**Ground floor facade walls** must change plane every 30 linear feet or less, to prevent monotony.  
*Depth change not < 4", Height change not < 2'*

### Canopies/ Balconies/ Projections

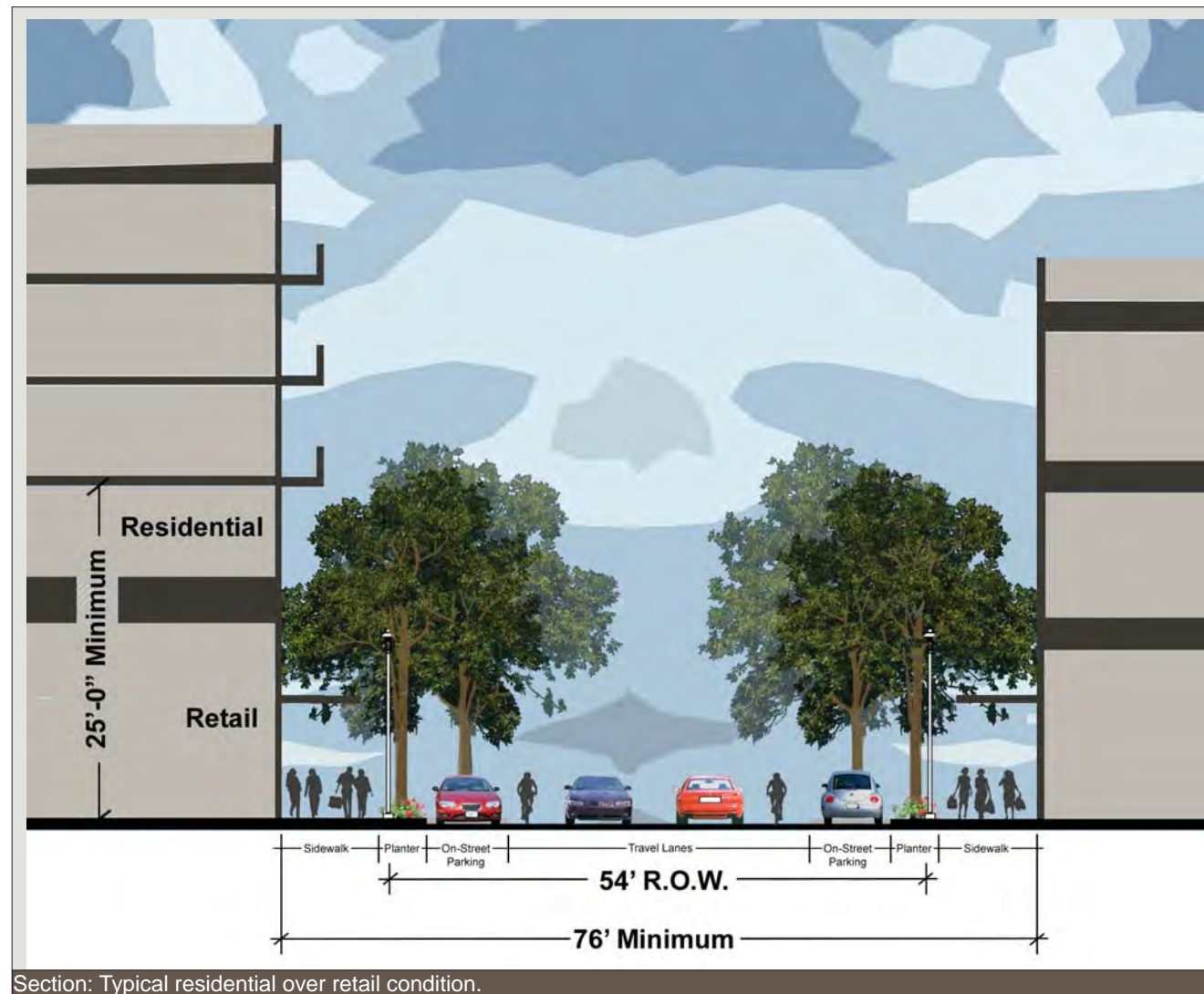


#### Requirements:

**Balconies / Building Projections:** extend no more than 5' past the build to line.  
**Awnings / Canopies:** may project no further than 4'6" from the face of curb, and may not obstruct lighting, landscaping, etc. (note exception above)



## 4.2 Residential Over Retail



### Residential Over Retail:

Konterra Town Center East is a vertical mixed-use project that integrates the essential urban uses, retail, residential and office, by adjacency as well as stacking. The residential over retail typology is one example of stacking that truly gives the development an urban feel. These buildings are located throughout the site and vary in massing and density.

#### Intent: Urban Lifestyle

The incorporation of vertical mixed use residential typologies typically found in urban contexts provides a housing product that is authentic to a city lifestyle. Rather than separating uses causing isolation and pockets of disassociation Konterra Town Center East integrates all uses to foster a 24 hour community that is active and dynamic.

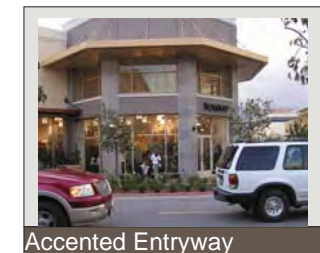
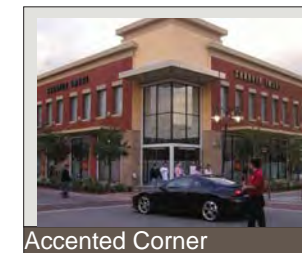
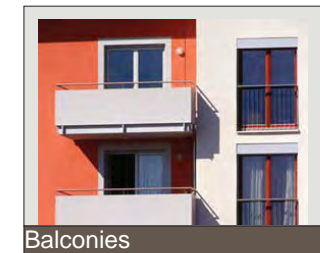
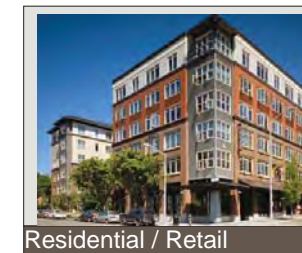
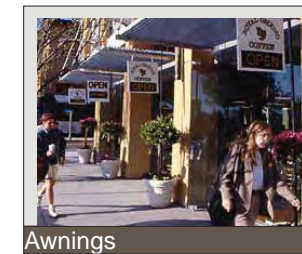
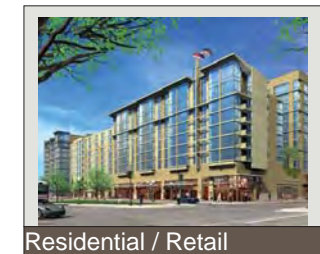
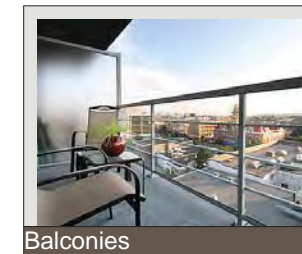
The residential over retail typologies are concentrated toward the center of the downtown area. This allows residents to feel as though they are truly part of the urban atmosphere, not pushed to the outskirts looking into the project. As residents will largely inhabit the downtown in the evenings and on the weekends this contributes to the 24 hour nature of the project infusing the downtown core with activity at night.

The residential over retail typology occurs in Konterra Town Center East primarily along the east/west Main Streets and the Promenade. Convenient parking is essential to the success of both the retail and the residential uses and by integrating these types together an opportunity for shared parking presents itself. While most residential over retail buildings are located near parking structures where they would have allotted spaces for residents and store patrons it is not unlikely that street parking for residents may be even more convenient and would certainly provide essential visitor parking in the evenings.

Incorporating the residential over retail housing product into Konterra Town Center East provides a truly urban lifestyle opportunity that ensures that the downtown core will be diverse in use and experience. This typology also allows for a different architectural product to be expressed in the project contributing to the heterogeneity and diversity of both the physical appearance and experience of the site.

### Recommendations:

- Orient retail and residential entries to face public streets and sidewalks.
- Encourage pedestrian connections from parking areas to building entries at public streets.
- Provide visitor drop-off area and on-street parking at public building entries.
- Locate parking to the rear of the building away from public view.
- Screen trash enclosures from public view with a fence and/or landscaping.
- Provide outdoor dining terraces with tables, chairs, and other furniture to bring activity to the street where appropriate.
- Encourage architectural expression of building entry features.
- Screen mechanical equipment from view.
- Residential balconies will extend no more than 5 feet past the building line.
- Residential facades will be no less than 40% transparent.
- Ground floor retail facades will be no less than 75% transparent.
- Retail awnings/canopies/projections will extend at maximum 4'-6" from the face of curb, 60'-0" clear area between buildings (including all projections i.e. balconies, awnings, canopies etc. must be maintained for fire safety).
- Materials will be used to differentiate between changes in vertical program.
- Street level facades will vary every 30 linear feet or less at a depth of not less than 4".
- Parapet walls will vary every 60 linear feet or less and differ in height by no less than 2'-0".





## 4.2 Residential Over Retail



**Parapet Breaks:** Will occur no less than every 60'-0" of horizontal distance and change in height by no less than 2 feet, to add visual interest and relief from monotonous expanses of singularly planar walls.

**Setbacks & Facade Breaks:** Facade walls at the ground floor will vary no less than every 30'-0" and have a depth change of no less than 4" from the adjoining wall. Buildings will be setback no less than 9'-6", and no more than 30'-0", from the back of curb to allow for sidewalks and plazas.

**Awnings and Covered Walkways:** These elements are encouraged to enhance the street experience and further delineate uses as well as add distinction to individual tenants.



**Residential Over Retail Setbacks:** These are encouraged on the south side of the street to allow for better natural lighting.

**Upper Level Facade Breaks:** Are not required to mimic the ground floor below but must adhere to the 60'-0" parapet break requirement and any change in wall depth must be no less than 4".

**Pedestrian Rooms:** Where possible 'pedestrian rooms' shall occur where sidewalks and setbacks are at their maximums. These spaces should be enhanced with street furniture and landscaping to encourage use.



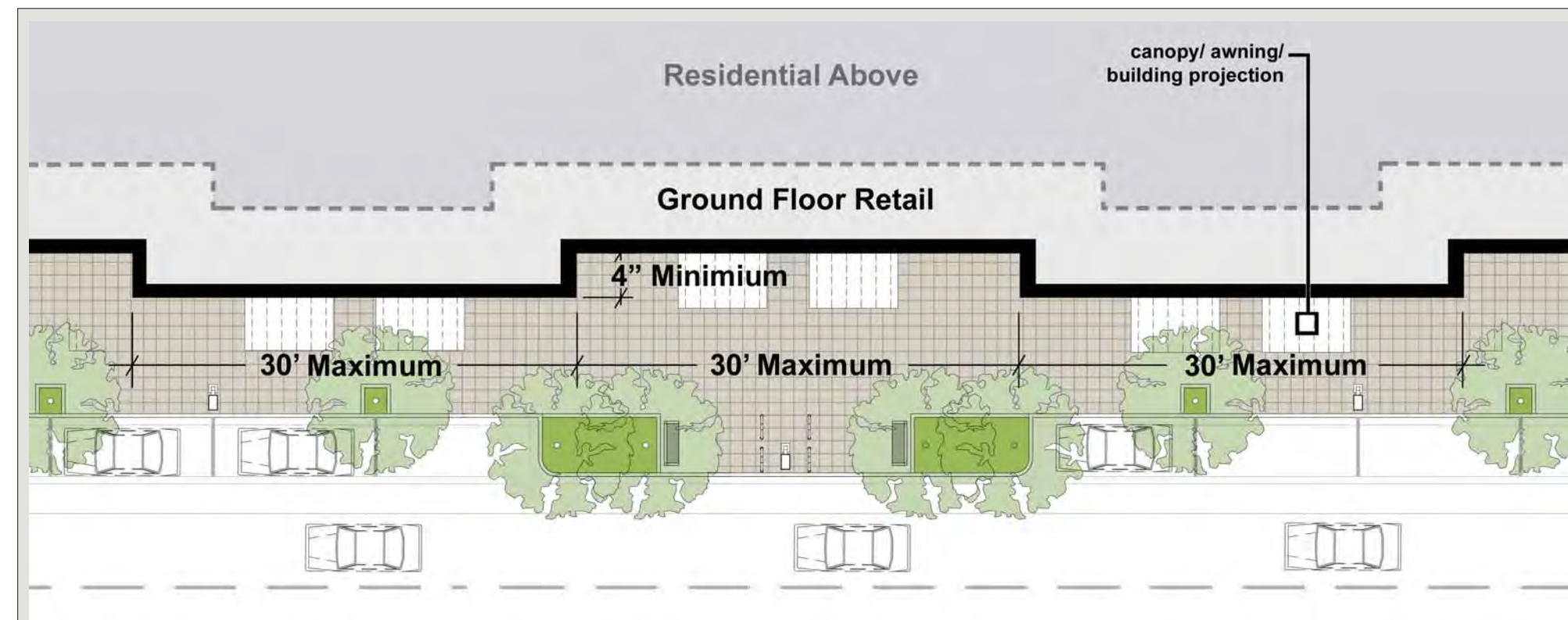
**Materials and Facade Breaks:** Where possible materials/appearance should vary between facade breaks to create a diverse built environment.

**Vertical Material/Color Diversity:** Materials and/or color should be used to differentiate between grade level uses and upper level uses.

**Balconies:** are encouraged for residential typologies and should extend no further than 5'-0" past the build to line.



**Setback Uses:** Where possible encourage the sidewalk space gained from allowed setbacks to be used functionally, i.e. for outdoor seating, dining or weather protection.



Residential over retail setback minimums and maximums.



Residential over retail facade metering.



4.2 Residential Over Retail

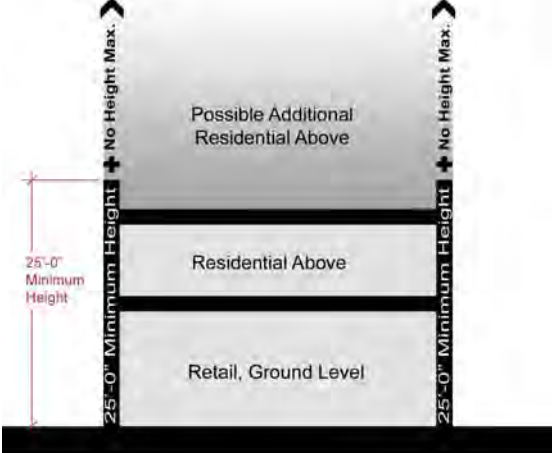
Lot Coverage



**Requirements:**

60% Minimum Lot Coverage  
100% Maximum Lot Coverage

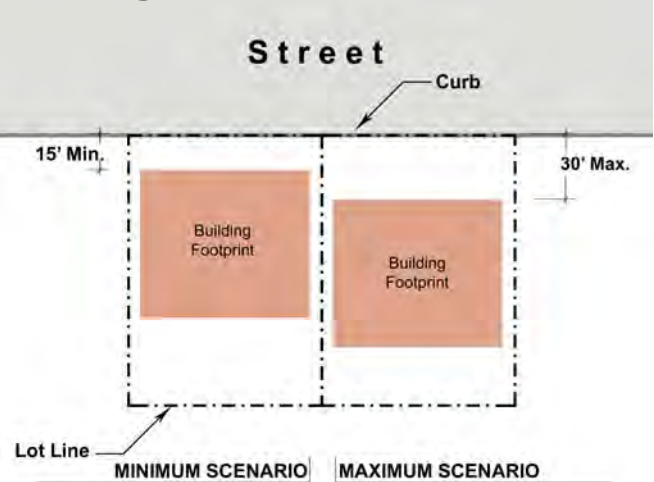
Building Height Restrictions



**Requirements:**

25' Minimum Height  
(from ground to top of parapet)  
No Maximum Height Req.  
(from ground to top of parapet)

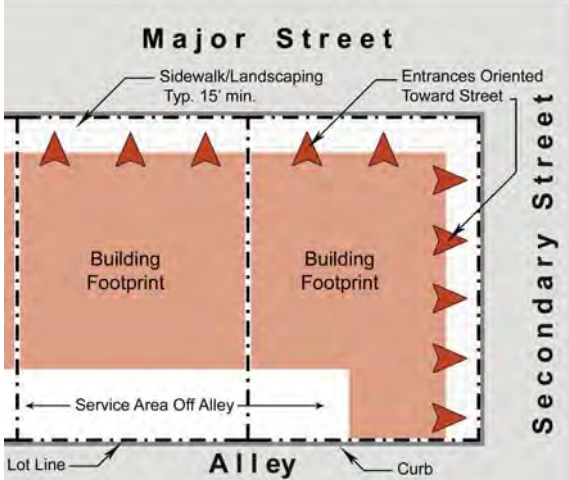
Building Setback



**Requirements:**

15' Minimum Setback  
(front of building to front of curb)  
30' Maximum Setback  
(front of building to front of curb)

Orientation / Service



**Requirements:**

Locate Building Entrances so they Orient  
Toward the Street or public realm  
Locate Service Areas Off Alleys and at the  
Rear of Buildings

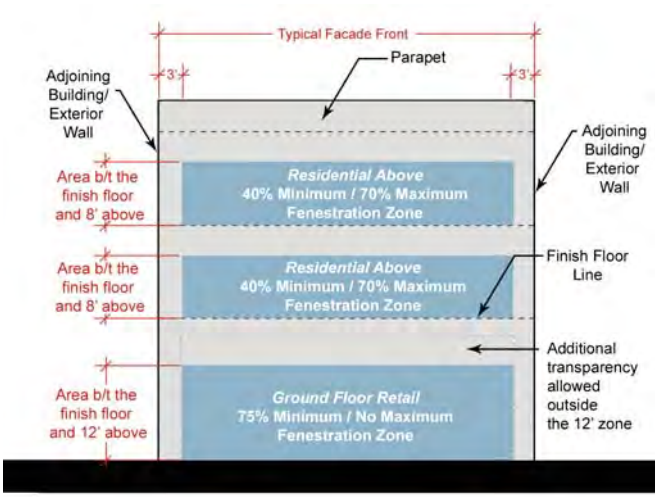
Use Differentiation



**Intent:**

- To express each use architecturally using color, material, size and proportion to effectively achieve a coherent and thoughtful composition that reflects the entirety of the building's uses

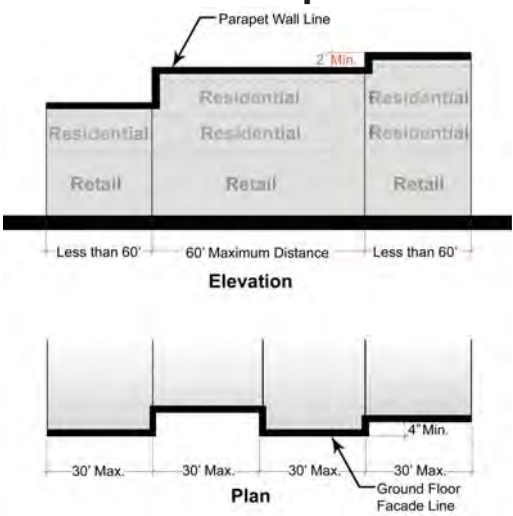
Fenestration



**Requirements:**

**Ground Floor:** 75% Minimum Transparency within the Fenestration Zone  
**Upper Floor:** 40% Minimum / 70% Maximum Transparency within the Fenestration Zone  
See diagram for definition of Fenestration Zone

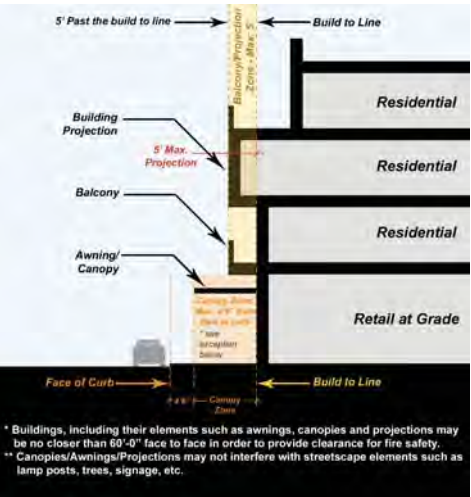
Facade Break / Parapet



**Requirements:**

Parapet Wall should vary every 60 linear feet or less, to create visual interest.  
Ground floor facade walls must change plane every 30 linear feet or less, to prevent monotony.  
*Depth change not < 4", Height change not < 2'*

Canopies/ Balconies/ Projections



**Requirements:**

**Balconies / Building Projections** extend no more than 5' past the build to line.  
**Awnings / Canopies** may project no further than the back of curb, and may not obstruct lighting, landscaping, etc.



## 4.3 Office Over Retail



### Office Over Retail:

Konterra Town Center East is a vertical mixed-use project that integrates the essential urban uses, retail, residential and office, by adjacency as well as stacking. The office over retail typology is one example of stacking that truly gives the development an urban feel. These buildings are located primarily in the central core of the site and vary in massing and density.

#### Intent: Variety of Working Product

The incorporation of vertical mixed use office typologies typically found in urban contexts provides a work product that is authentic to a city lifestyle. Rather than separating uses causing isolation and pockets of disassociation Konterra Town Center East integrates all uses to foster a 24 hour community that is active and dynamic.

The office over retail typologies are concentrated toward the center of the downtown. This allows residents to feel as though they are truly incorporated into the urban atmosphere, not pushed to the outskirts looking into the project. As offices will largely function during typical working hours in the downtown, they will have little impact on the development in the evenings and on the weekends contributing to the 24 hour nature of

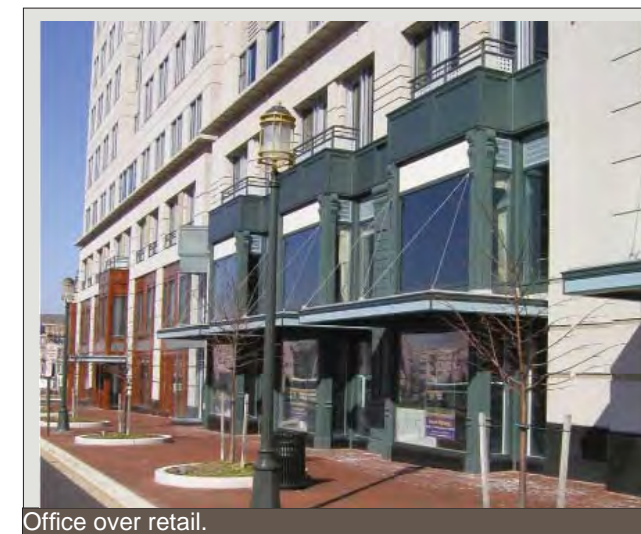
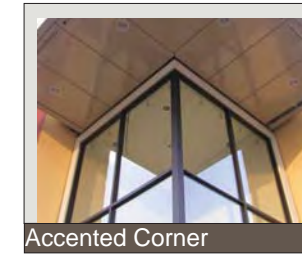
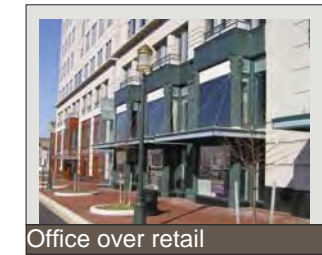
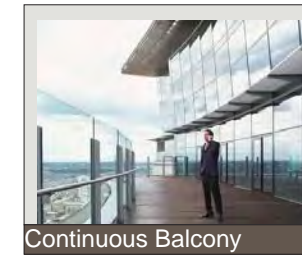
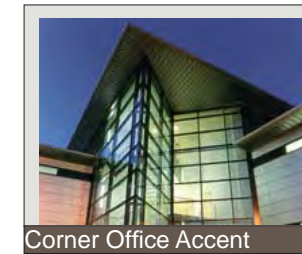
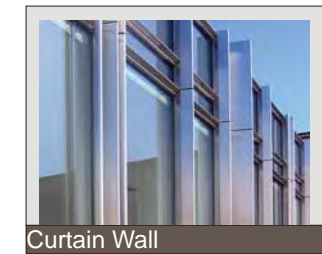
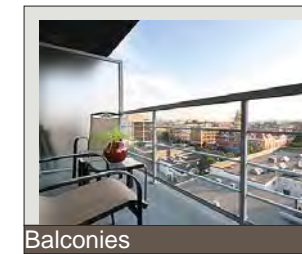
the project infusing the downtown core with daily, morning and afternoon weekday activity.

The office over retail typology occurs in Konterra Town Center East primarily along the east/west Main Streets and the Promenade. Convenient parking is essential to the success of both the retail and the office uses and by integrating these types together an opportunity for shared parking presents itself. While most office over retail buildings are located near parking structures where they would have allotted spaces for employees and store patrons it is not unlikely that street parking for visitors or patrons may be even more convenient.

Incorporating the office over retail typology into Konterra Town Center East provides a truly urban lifestyle opportunity that ensures that the downtown core will be diverse in use and experience. This typology also allows for a different architectural product to be expressed in the project contributing to the heterogeneity and diversity of both the physical appearance and experience of the site.

### Recommendations:

- Orient retail and office entries to face public streets and sidewalks.
- Encourage pedestrian connections from parking areas to building entries at public streets.
- Provide visitor drop-off area and on-street parking at public building entries.
- Locate parking to the rear of the building away from public view.
- Screen trash enclosures from public view with a fence and landscaping.
- Buildings will be setback a minimum of 15'-0" from the face of curb to provide wide sidewalks and areas for landscape.
- Provide outdoor dining terraces with tables, chairs, and other furniture to bring activity to the street.
- Encourage architectural expression of building entry features.
- Screen mechanical equipment from view.
- Balconies will extend no more than 5 feet past the build-to line.
- Office facades will be no less than 40% transparent
- Office windows will range in size but will be no less than 1' in width.
- Retail facades will be no less than 75% transparent.
- Retail awnings will extend no further than 4'6" from the face of curb while maintaining a 60'-0" clear area between buildings (including projections, canopies etc.) for fire safety.
- Materials will be used to differentiate between changes in vertical program.
- Facades will be broken every 30 linear feet or less.





## 4.3 Office Over Retail



**Facade Breaks:** Will occur no more than every 60'-0" of horizontal distance to add visual interest and relief from monotonous expanses of singularly planar walls.

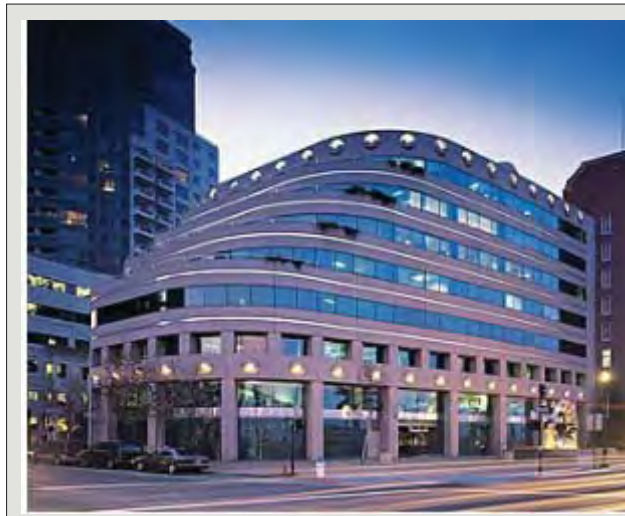
**Setbacks for Facade Breaks:** Facade breaks will be no less than 4" in depth between adjoining walls and must occur every 30' or less.

**Awnings and Covered Walkways:** These elements are encouraged to enhance the street experience and further delineate uses as well as add distinction to individual tenants.



**Office Over Retail Setbacks:** Will be no greater than 10'-0" in depth from the facade wall on the ground floor. These are encouraged on the south side of the street to allow for better natural lighting.

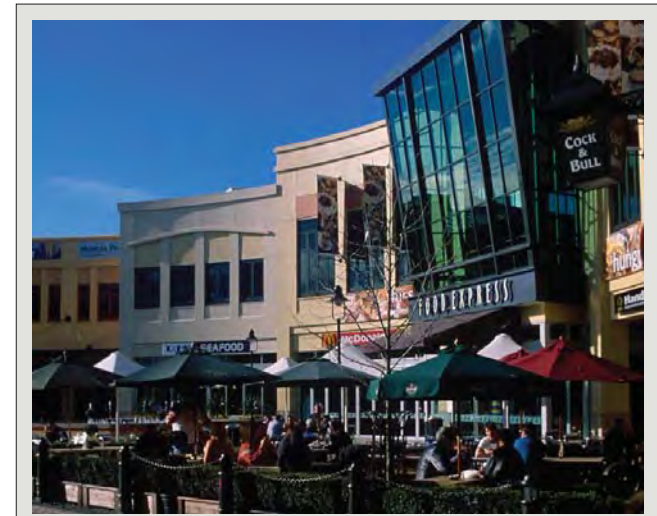
**Office Over Retail Parapet Breaks:** Breaks must occur every 60 linear feet or less, and be no less than 2'-0" in vertical change.



**Pedestrian Rooms:** Where possible 'pedestrian rooms' shall occur where sidewalks and setbacks are at their maximums. These spaces should be enhanced with street furniture and landscaping to encourage use.

**Materials and Facade Breaks:** Where possible materials/appearance should vary between facade breaks to create a diverse built environment.

**Vertical Material Diversity:** Materials should be used to differentiate between grade level uses and upper level uses.



**Balconies:** are encouraged for office typologies and should extend no greater than 8'-0" from the exterior facade wall, and not further than 5'-0" past the build to line.

**Setback Uses:** Where possible encourage the sidewalk space gained from required setbacks to be used functionally, i.e. for outdoor seating, dining or weather protection.



Office over retail setback minimums and maximums.



Office over retail - covered walkway.



# 4.3 Office Over Retail

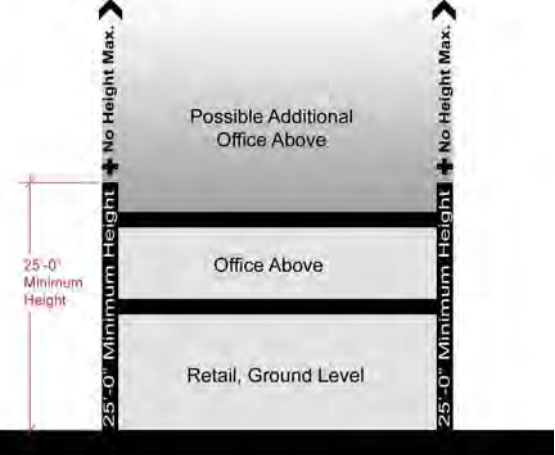
## Lot Coverage



**Requirements:**

60% Minimum Lot Coverage  
100% Maximum Lot Coverage

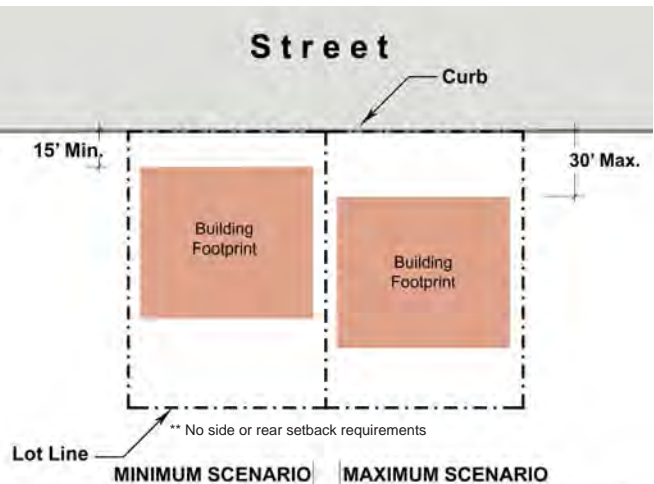
## Building Height Restrictions



**Requirements:**

25' Minimum Height  
(from ground to top of parapet)  
No Maximum Height Req.  
(from ground to top of parapet)

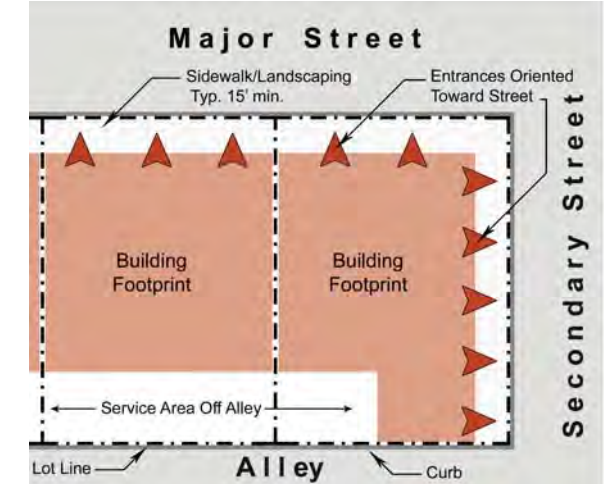
## Building Setback



**Requirements:**

15' Minimum Setback  
(front of building to front of curb)  
30' Maximum Setback  
(front of building to front of curb)

## Orientation / Service



**Requirements:**

Locate Building Entrances so they Orient  
Toward the Street or public realm  
Locate Service Areas Off Alleys and at the  
Rear of Buildings

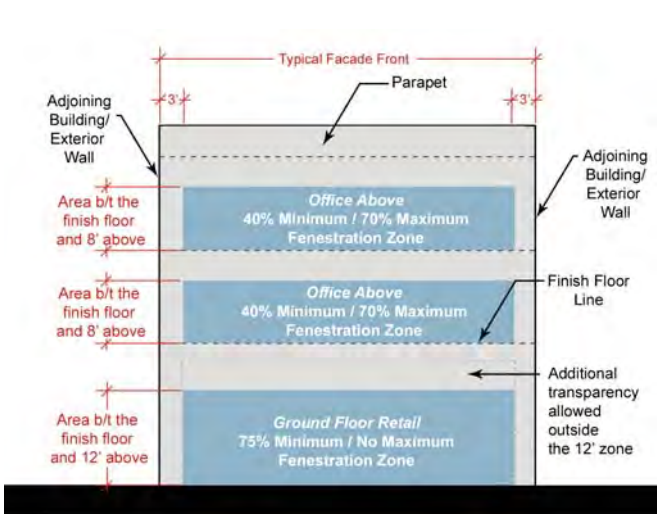
## Use Differentiation



**Requirements:**

- To express each use architecturally using color, material, size and proportion to effectively achieve a coherent and thoughtful composition that reflects the entirety of the building's uses

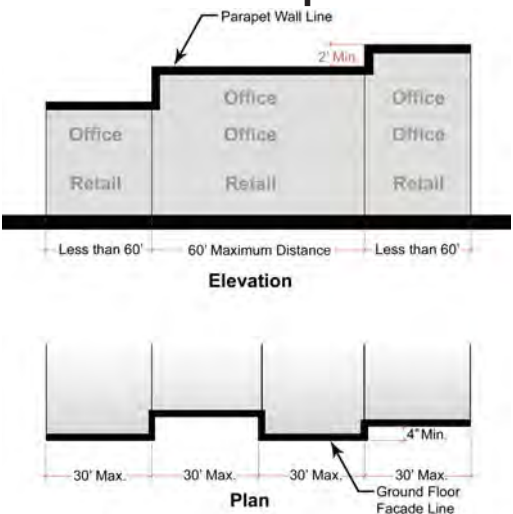
## Fenestration



**Requirements:**

**Ground Floor:** 75% Minimum Transparency within the Fenestration Zone  
**Upper Floor:** 40% Minimum / 70% Maximum Transparency within the Fenestration Zone  
See diagram for definition of Fenestration Zone

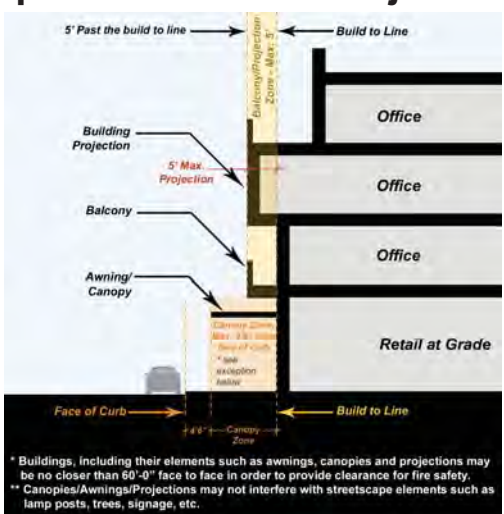
## Facade Break / Parapet



**Requirements:**

**Parapet Wall** should vary every 60 linear feet or less, to create visual interest.  
**Ground floor facade walls** must change plane every 30 linear feet or less, to prevent monotony.  
*Depth change not < 4", Height change not < 2'*

## Canopies/ Balconies/ Projections



**Requirements:**

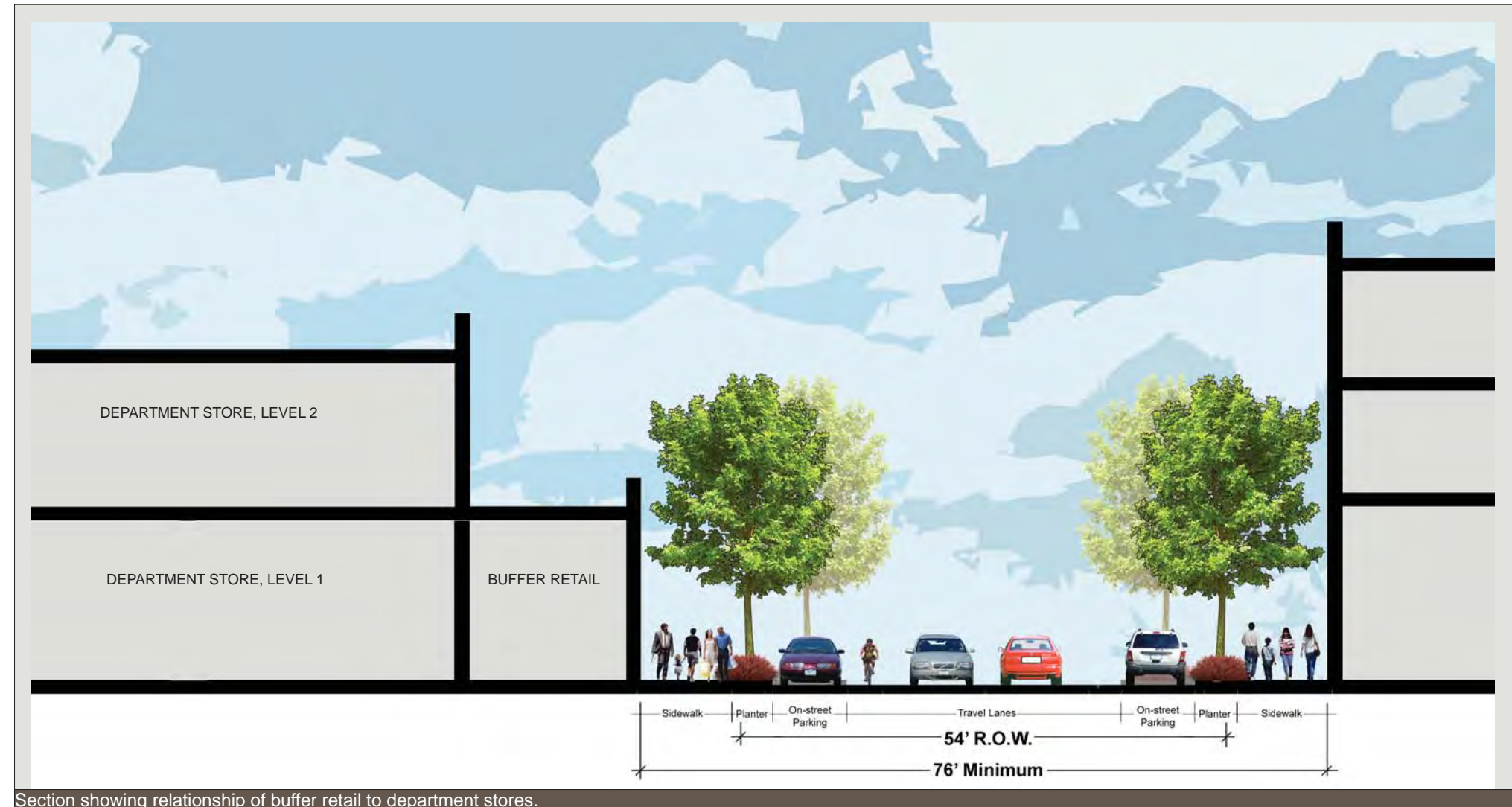
**Balconies / Building Projections** extend no more than 5' past the build to line.  
**Awnings / Canopies** may project no further than the back of curb, and may not obstruct lighting, landscaping, etc.



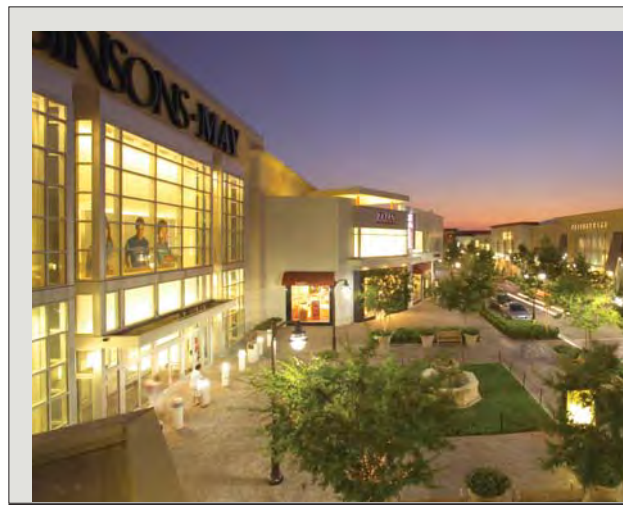
## 4.4 Department Stores/Buffer Retail

### Recommendations:

- Provide entry plaza to allow views to anchor stores from the street
- Provide parking drive aisles adjacent to the retail frontage
- Place service and employee parking to the rear of the building
- Provide street trees along driveways, drive aisles, and pedestrian connections
- Setbacks should be landscaped
- Screen loading areas and trash enclosures from public view with trellis, fencing, and/or landscaping
- Express primary entry with higher volume, special building materials and architectural details
- Encourage windows and display cases along pedestrian connections
- Screen mechanical equipment from view
- Buffer larger department stores with smaller retail shops to add interest and layering to the street frontage
- Create plazas where buffer retail feeds into the department store's main entrance
- Provide areas for sitting and congregating in plazas
- Provide street furniture, bicycle racks and shade for pedestrian use
- If a department store fronts both a street and a large surface lot, articulate both entrances as main entrances
- Variation in material and expression is encouraged
- Pedestrian crosswalks will be clearly articulated and highly visible at intersections where anchor stores are located to provide safe and convenient connections.



Section showing relationship of buffer retail to department stores.



Buffer retail in front of department store.



Buffer retail in front of department store.



Buffer retail in front of department store.



Buffer retail in front of department store.



## 4.4 Department Stores/Buffer Retail

### Department Stores with Buffer Retail:

Konterra Town Center East contains four major anchors - three department stores and one movie theater. These anchors are placed strategically within the site to create destination points that are then linked by other retail, office and residential uses creating a network of nodes and paths that engage and direct pedestrians throughout the site.

A few strategies were used to increase the visibility of the department stores and to maximize the walking distance that pedestrians will travel within the site to reach these destinations. First of all the two department stores to the north of the site are located at the northern termini of West Street and East Street. By placing the department stores at these locations they become highly visible to pedestrians walking along those secondary north/south streets, acting as both landmarks for way finding and also as destinations that will draw pedestrians to travel along those streets. Secondly the department store on the western side of Fashion Place and the movie theater on the eastern edge of Fashion Place act to anchor and define the edges of the site in the east/west directions. By locating the anchors in these locations the east/west edges of the site become clear and movement along this axis is encouraged. The placement of these anchors insures that pedestrians will travel throughout the site both in the north/south and east/west directions, by creating a de-

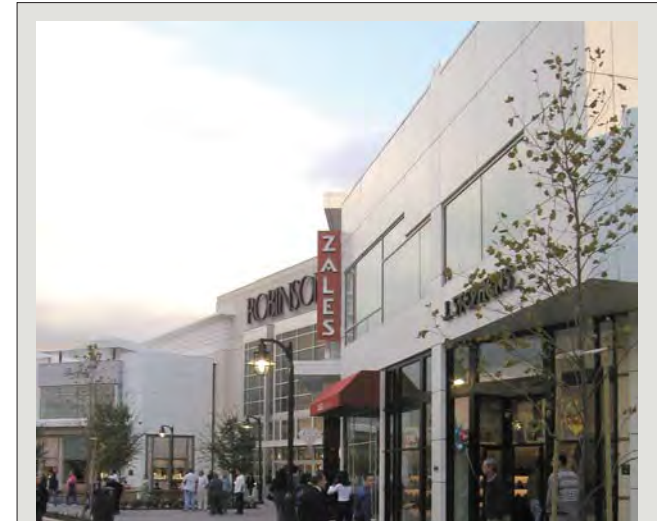
fined loop that connects them. With a large anchor at each corner of the loop pedestrians will easily be able to orient themselves and comfortably navigate the site.

A third strategy in place with the large anchors is masking their large linear distance with "buffer retail" which acts to break up the long, typically un-articulated, anchor store walls with small retail shops that sit directly in front of the anchors. These smaller retail shops help to create the necessary human scale that encourages pedestrians to walk further and adds visual stimulation to the built environment helping to activate the street edge.

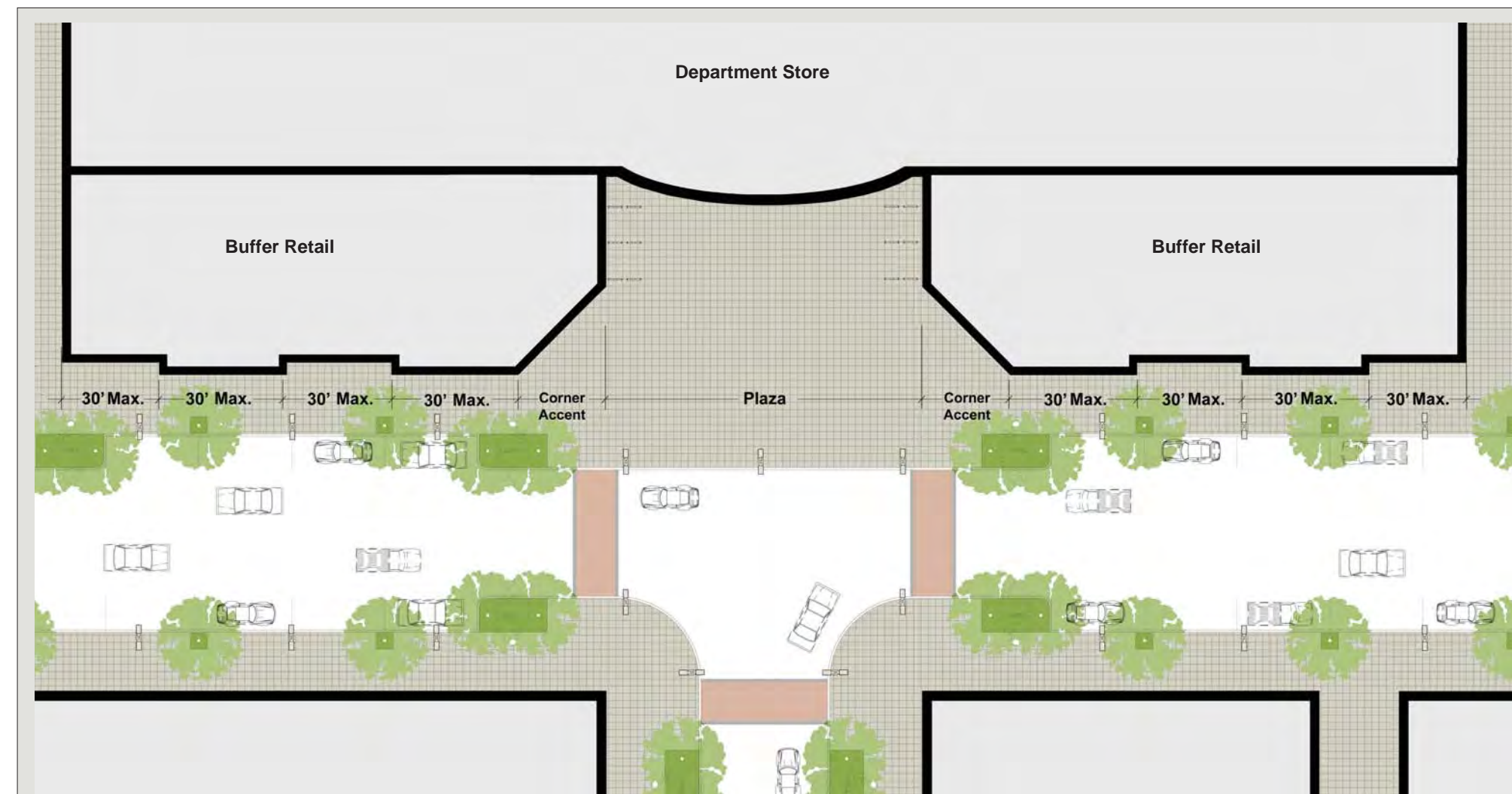
The scale of the buffer retail to the large anchors is enough to continue the rhythm of the streetscape experience but not so much as to block the views to, or the identity of, the larger anchor store. Typically the buffer retail is one story and the anchor is two stories allowing the anchors to be seen as a larger volume but not appear monotonous. The incorporation of the buffer retail also provides opportunities for public space in the form of plazas. The benefits of these plazas are two-fold, firstly they provide needed pedestrian areas for relaxation and gathering, and secondly they create enhanced entry ways to the larger anchors.



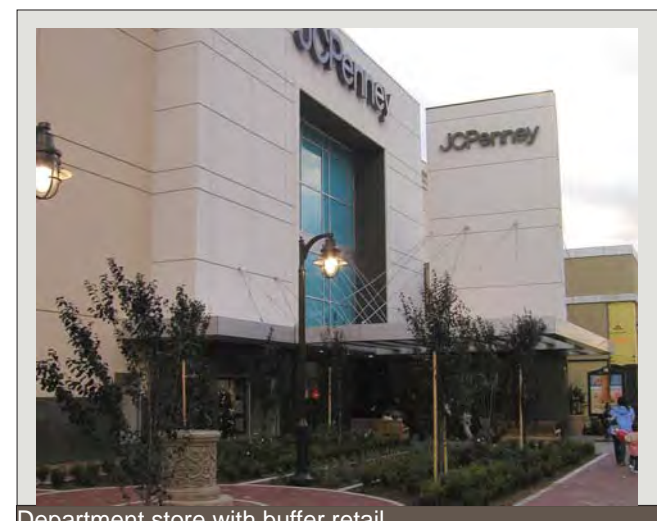
Department store with buffer retail.



Department store with buffer retail.



Plan showing buffer retail in front of department store.



Department store with buffer retail.



Aerial showing locations of buffer retail/department stores.



## 4.4 Department Stores/Buffer Retail

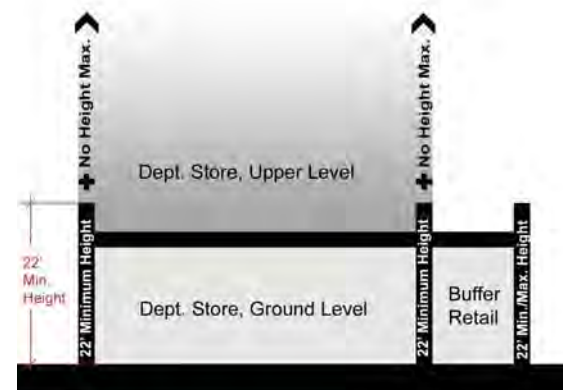
### Lot Coverage



#### Requirements:

60% Minimum Lot Coverage  
100% Maximum Lot Coverage

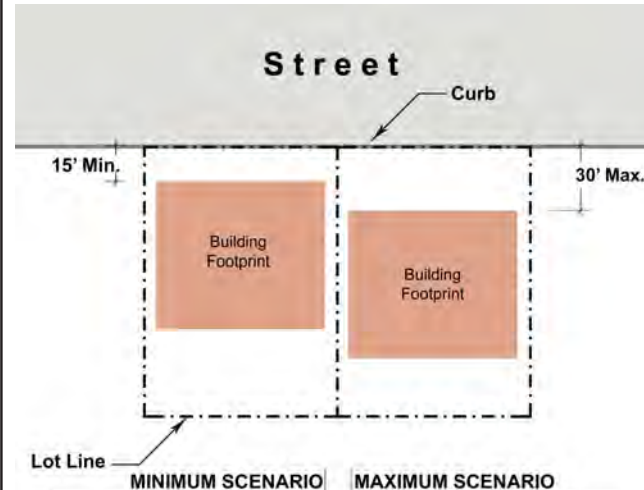
### Building Height Restrictions



#### Requirements:

22' Minimum Height  
(from ground to top of parapet)  
No Maximum Height Req.  
(from ground to top of parapet)

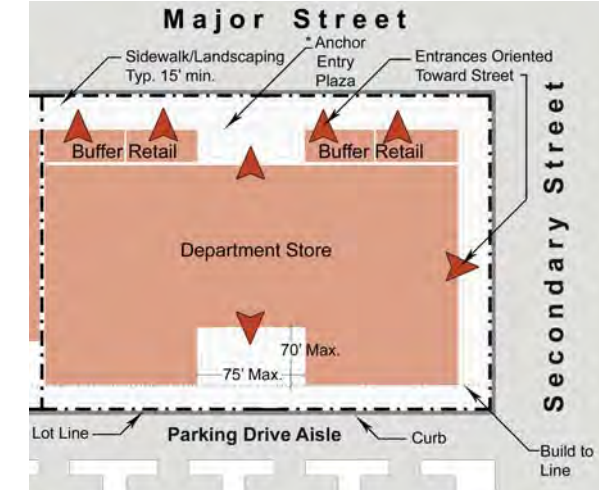
### Building Setback



#### Requirements:

15' Minimum Setback  
(front of building to front of curb)  
30' Max. Setback (excluding Anchor Entry Plazas)  
(front of building to front of curb)

### Orientation



#### Requirements:

Locate building entrances so they orient toward the street and major parking fields  
Locate service areas off alleys and at the rear of buildings

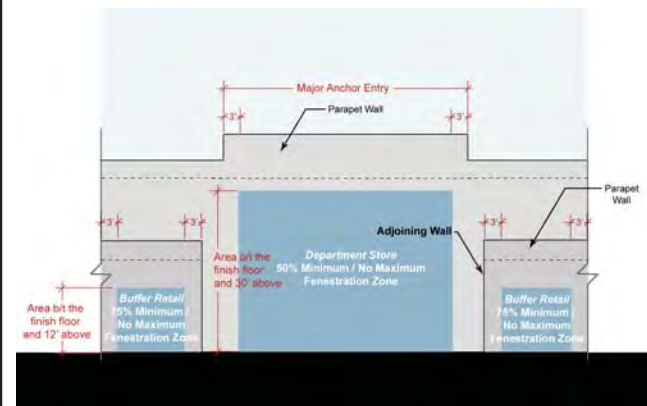
### Use Differentiation



#### Intent:

- To express each use architecturally using color, material, size and proportion to effectively achieve a coherent and thoughtful composition that reflects the entirety of the building's uses and mitigates the impact of one long continuous facade.

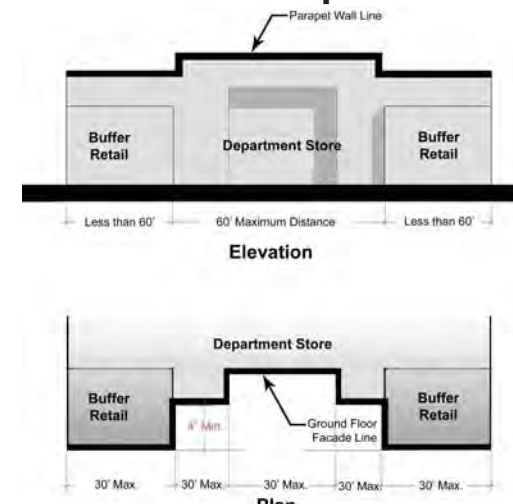
### Fenestration



#### Requirements:

**Dept. Store Entry:** 50% Minimum Transparency within the Fenestration Zone  
**Buffer Retail:** 75% Minimum Transparency within the Fenestration Zone  
See diagram for definition of Fenestration Zone

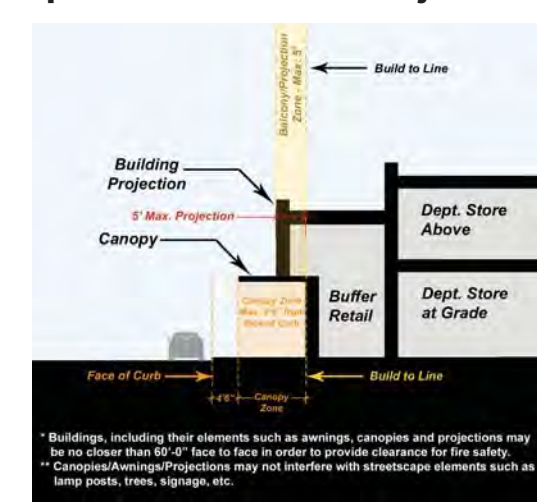
### Facade Break / Parapet



#### Requirements:

**Parapet Wall** should vary every 60 linear feet or less, to create visual interest.  
**Ground floor facade walls** must change plane every 30 linear feet or less, to prevent monotony.  
*Depth change not < 4", Height change not < 2'*

### Canopies/ Balconies/ Projections

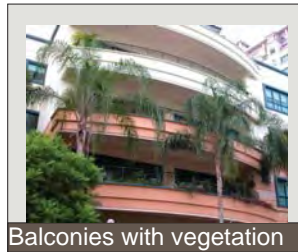


#### Requirements:

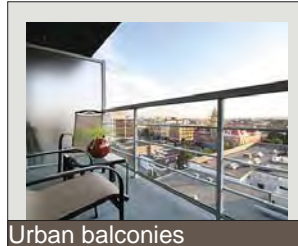
**Balconies / Building Projections** extend no more than 5' past the build to line.  
**Awnings / Canopies** may project no further than the back of curb, and may not obstruct lighting, landscaping, etc.



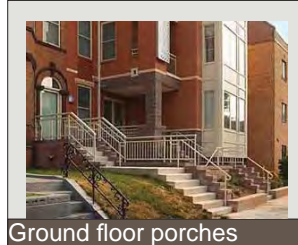
## 4.5 Stand Alone Residential



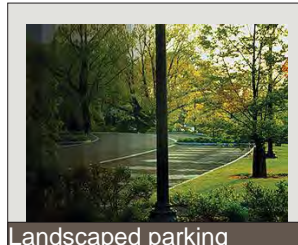
Balconies with vegetation



Urban balconies



Ground floor porches



Landscaped parking

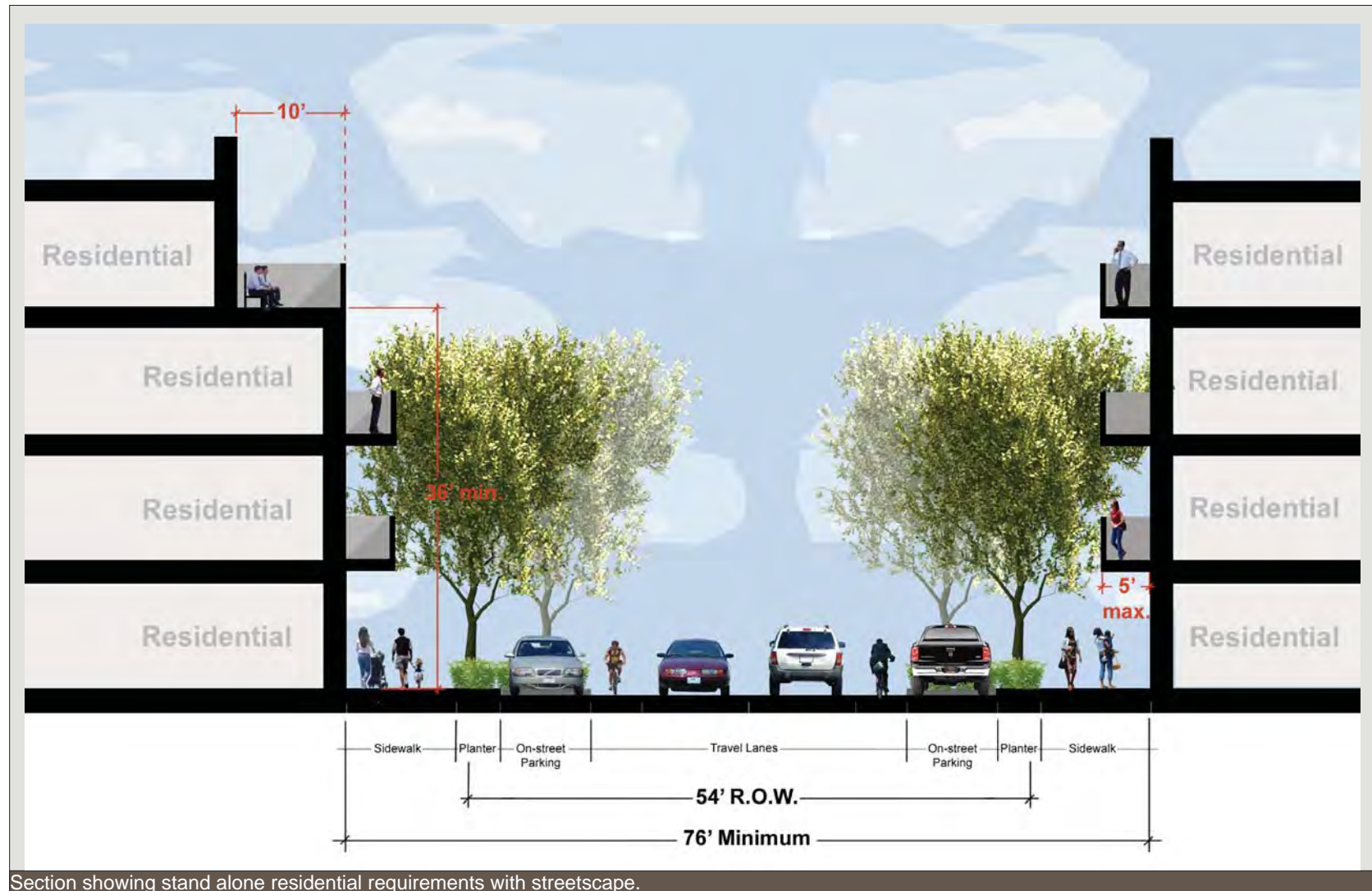
### Stand Alone Residential:

The downtown core of Konterra Town Center East contains two residential building typologies - residential over retail and stand alone residential. While the former is contained primarily in the center of the downtown the latter is generally located toward the southern and eastern perimeter edges of the downtown. Locating the stand alone residential in these areas encourages a future connection to further residential development across Perimeter Drive East where more multi-family residential as well as town homes are expected to develop over time. This location will lessen the impact of purely residential traffic within the core making the pedestrian environment safer and less congested.

The stand alone residential buildings in the Konterra Town Center East core are grouped together in twos and threes creating pockets of residential use that are horizontally rather than vertically integrated into the site. This arrangement essentially creates micro urban residential parks allowing the buildings to share parking and amenities in an efficient manner. By placing the residential buildings to the eastern and southern edges of the downtown it is possible to contain their impact on other uses, such as parking demand and traffic congestion, while still sharing an adjacency to the retail and office components which remain directly accessible by walking or driving.

By incorporating stand alone residential into Konterra Town Center East it gives the development and potential residents a choice of housing product. Where the residential over retail typology may appeal to someone looking for a truly 24 hour lifestyle positioned right in the center of the development, the stand alone retail may appeal more to someone who appreciates the convenience and proximity to the central core but who is looking for a certain amount of separation from the other uses with a more neighborhood feel.

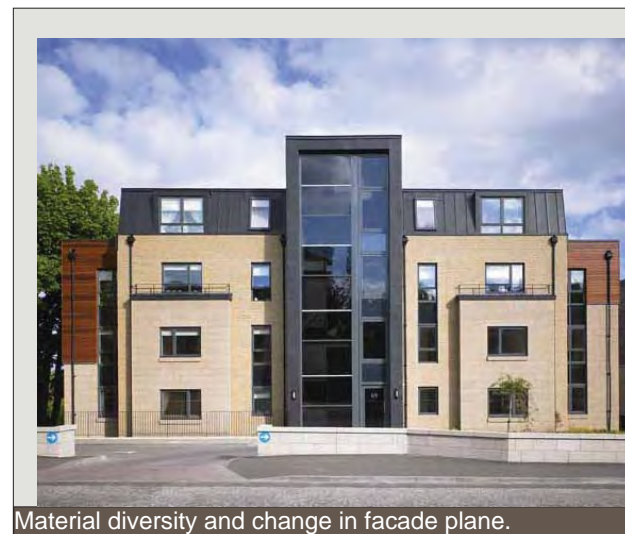
The stand alone residential in Konterra Town Center East has many of the same bulk restrictions as the residential over retail component; one exception is the required building height minimum. The stand alone residential is required to be a minimum of 36 feet high from the ground to the top of the parapet. The residential over retail required minimum height is 25 feet from the ground to the top of the parapet. This discrepancy allows the residential over retail to be as little as one story over the retail and requires the stand alone residential to be at minimum three stories in height. This height requirement therefore allows residential to exist in small quantities within the central downtown core in the form of residential over retail and also provides a mechanism for creating greater concentrations and densities of residential within the project by way of stand alone residential. This allows the greatest opportunity for mixing uses, both vertically and horizontally.



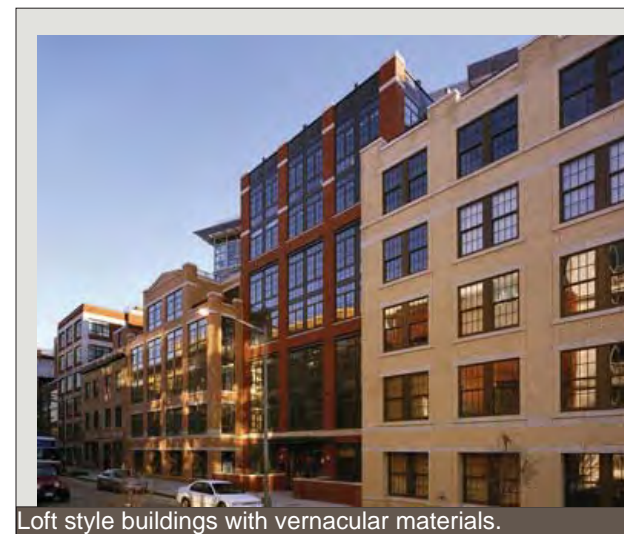
Section showing stand alone residential requirements with streetscape.



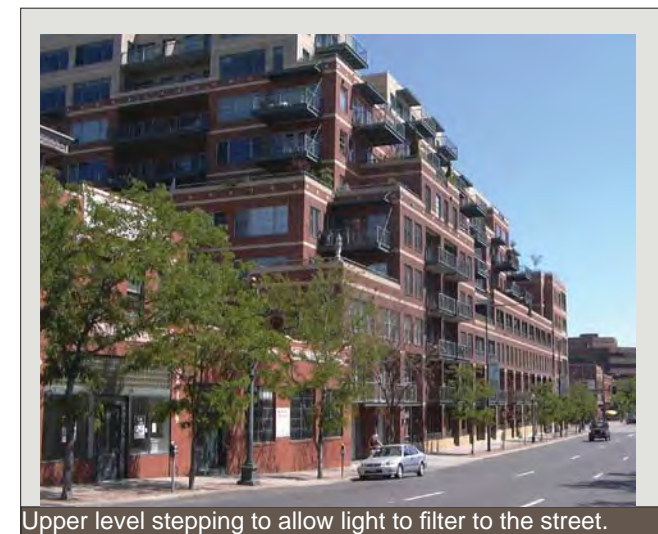
Fenestration and balconies sized for residential use.



Material diversity and change in facade plane.



Loft style buildings with vernacular materials.



Upper level stepping to allow light to filter to the street.



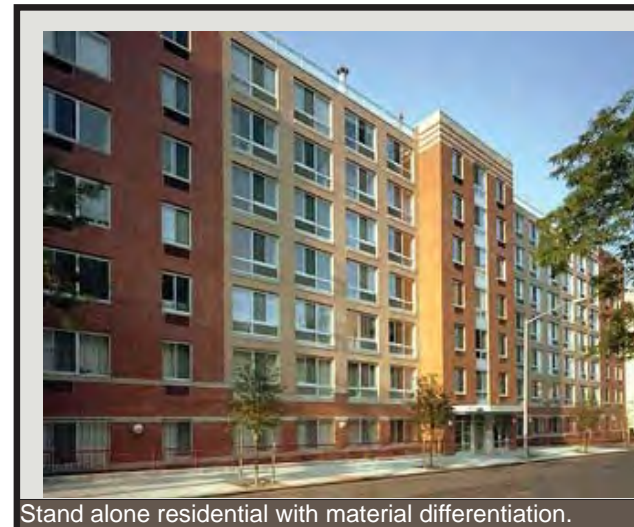
## 4.5 Stand Alone Residential

### Recommendations:

- Provide parking hidden from street, and away from public view
- Encourage use of on-street parking for visitor parking
- Encourage entry porches for ground floor units which have direct access to public streets with an 8 foot minimum clear width
- Encourage balconies for above grade units facing the street
- Screen trash enclosures from public view with a fence and landscaping
- Garages facing and accessible to public streets are prohibited
- Garage access from alleys or driveways located to the rear of lot required
- Required pedestrian connections from parking areas to building entries at public streets
- Minimize the number of access drives and curb cuts to parking
- Minimum building setback from the face of curb is 15'-0" to allow for consistent sidewalks and landscaping
- Maximum building setback from the curb is 30'-0" to allow for plazas and gathering places
- Minimum building height requirement is 36'-0" from the ground to the top of the parapet there is no height maximum
- Lot coverage can be at minimum 60% and at maximum 100%



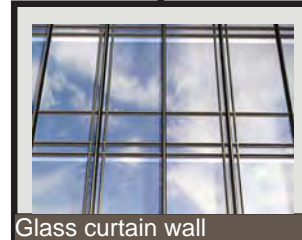
Contemporary stand alone residential building.



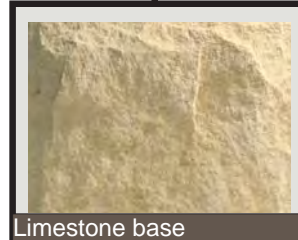
Stand alone residential with material differentiation.



Stand alone residential with material differentiation.



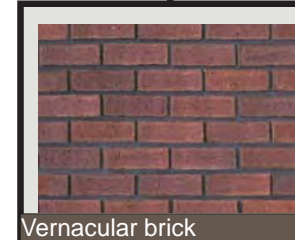
Glass curtain wall



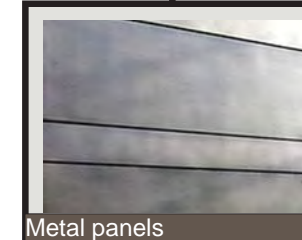
Limestone base



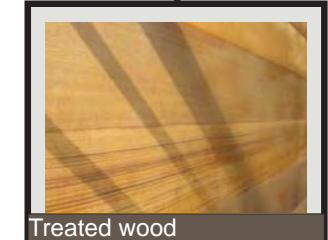
Stucco on upper floors



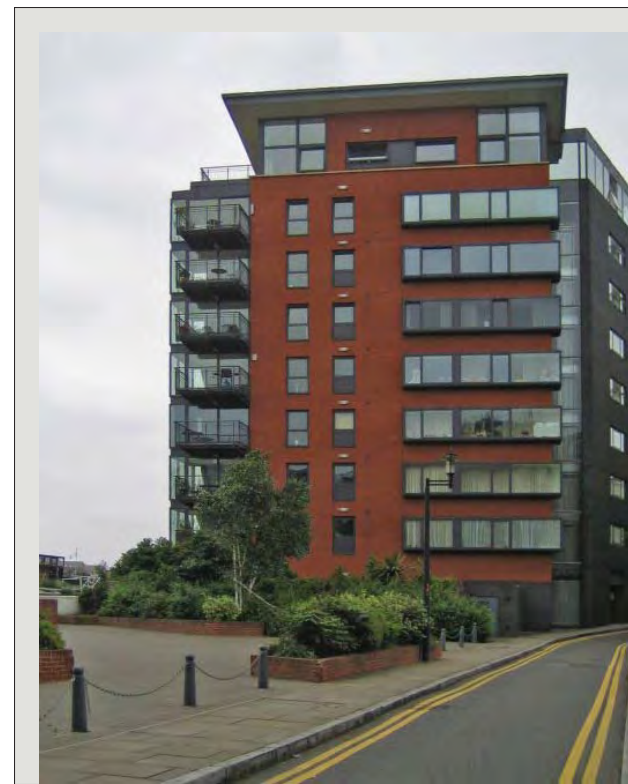
Vernacular brick



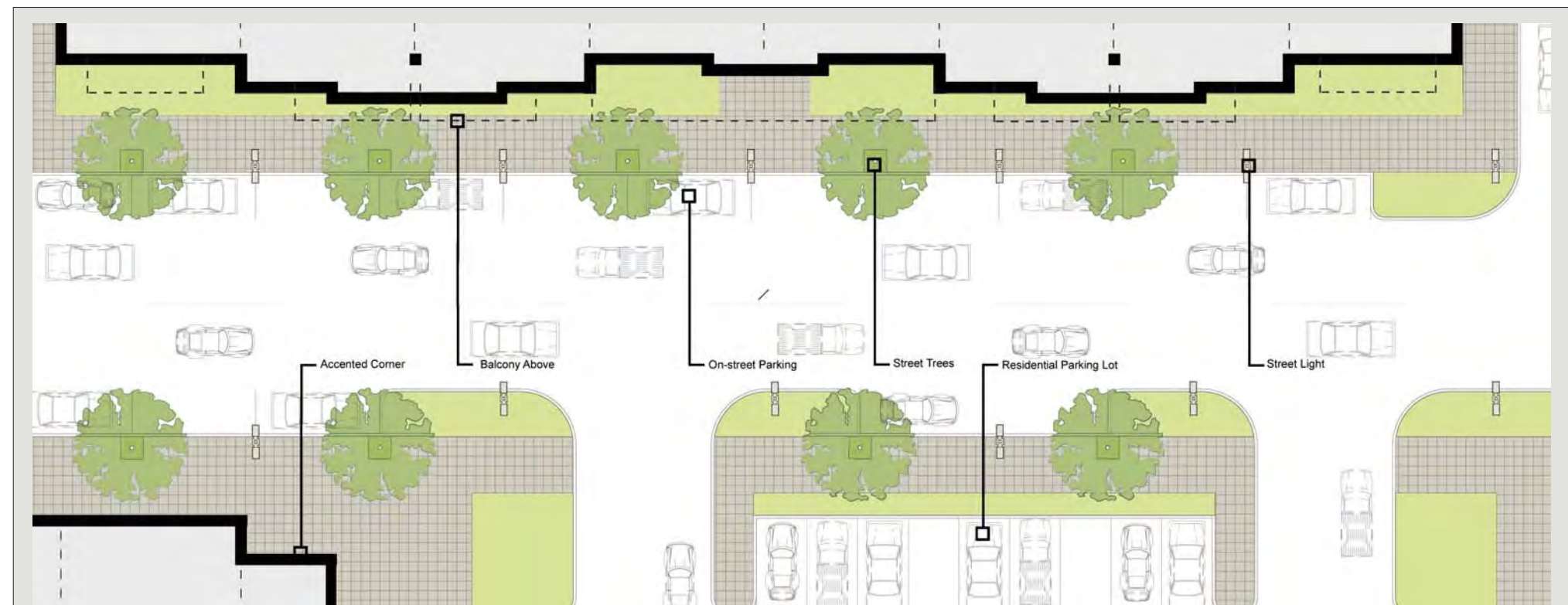
Metal panels



Treated wood



Stand alone residential with pocket park adjacent.



Plan of stand alone residential buildings grouped together.



4.5 Stand Alone Residential

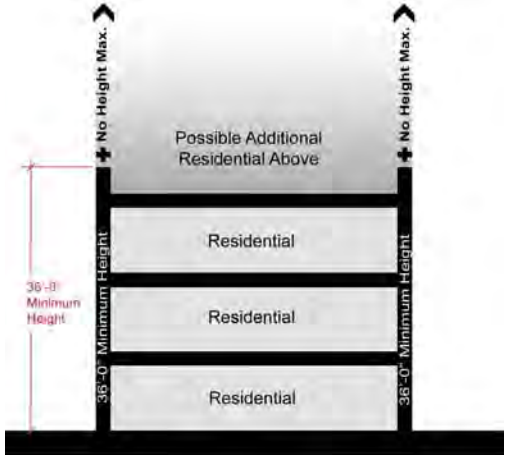
Lot Coverage



**Requirements:**

60% Minimum Lot Coverage  
100% Maximum Lot Coverage

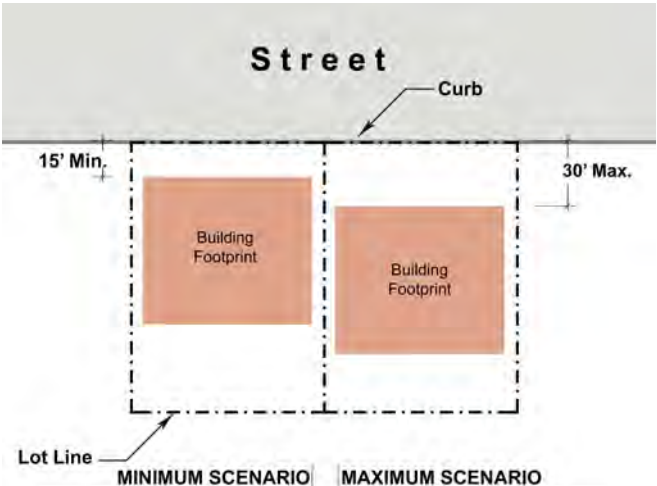
Building Height Restrictions



**Requirements:**

36' Minimum Height  
(from ground to top of parapet)  
No Maximum Height Req.  
(from ground to top of parapet)

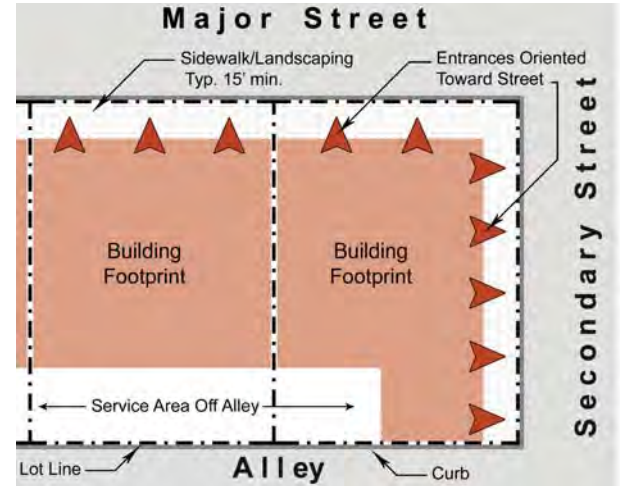
Building Setback



**Requirements:**

15' Minimum Setback  
(front of building to front of curb)  
30' Maximum Setback  
(front of building to front of curb)

Orientation / Service



**Requirements:**

Locate Building Entrances so they Orient  
Toward the Street or public realm  
Locate Service Areas Off Alleys and at the  
Rear of Buildings

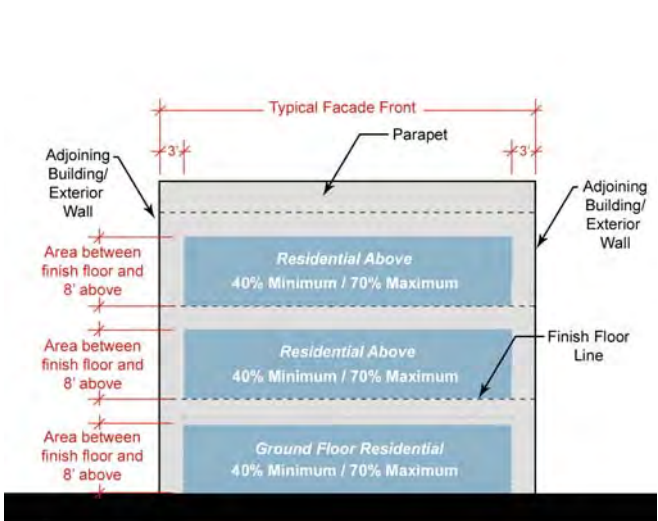
Entry Differentiation



**Intent:**

- To express the building's use architecturally using color, material, size and proportion to effectively achieve a coherent and thoughtful composition that informs the user of the building's use and where to enter.

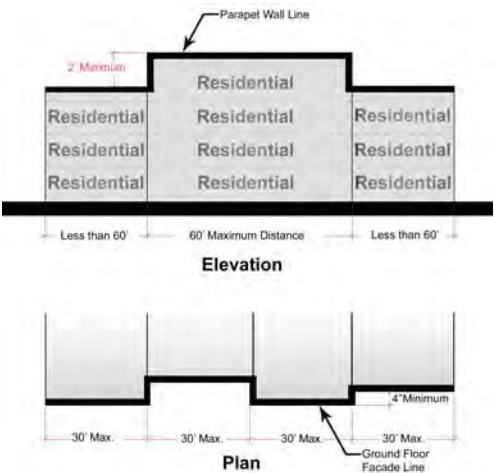
Fenestration



**Requirements:**

**Stand Alone Residential:** 40% Minimum / 70% Maximum Transparency within the Fenestration Zone.  
See diagram for definition of Fenestration Zone

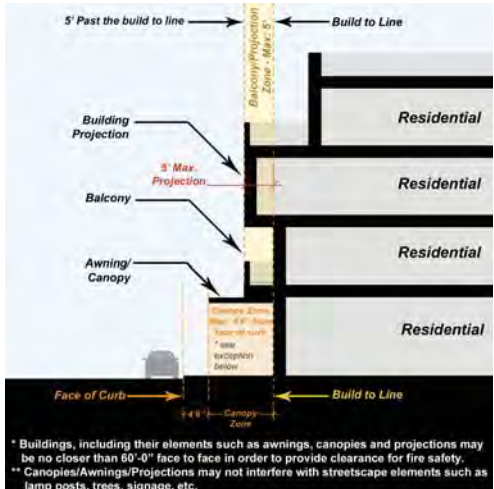
Facade Break / Parapet



**Requirements:**

Parapet Wall should vary every 60 linear feet or less, to create visual interest.  
Ground floor facade walls must change plane every 30 linear feet or less, to prevent monotony.  
Depth change not < 4", Height change not < 2'

Canopies/ Balconies/ Projections



**Requirements:**

**Balconies / Building Projections** extend no more than 5' past the build to line.  
**Awnings / Canopies** may project no further than the back of curb, and may not obstruct lighting, landscaping, etc.



## 4.6 Stand Alone Office



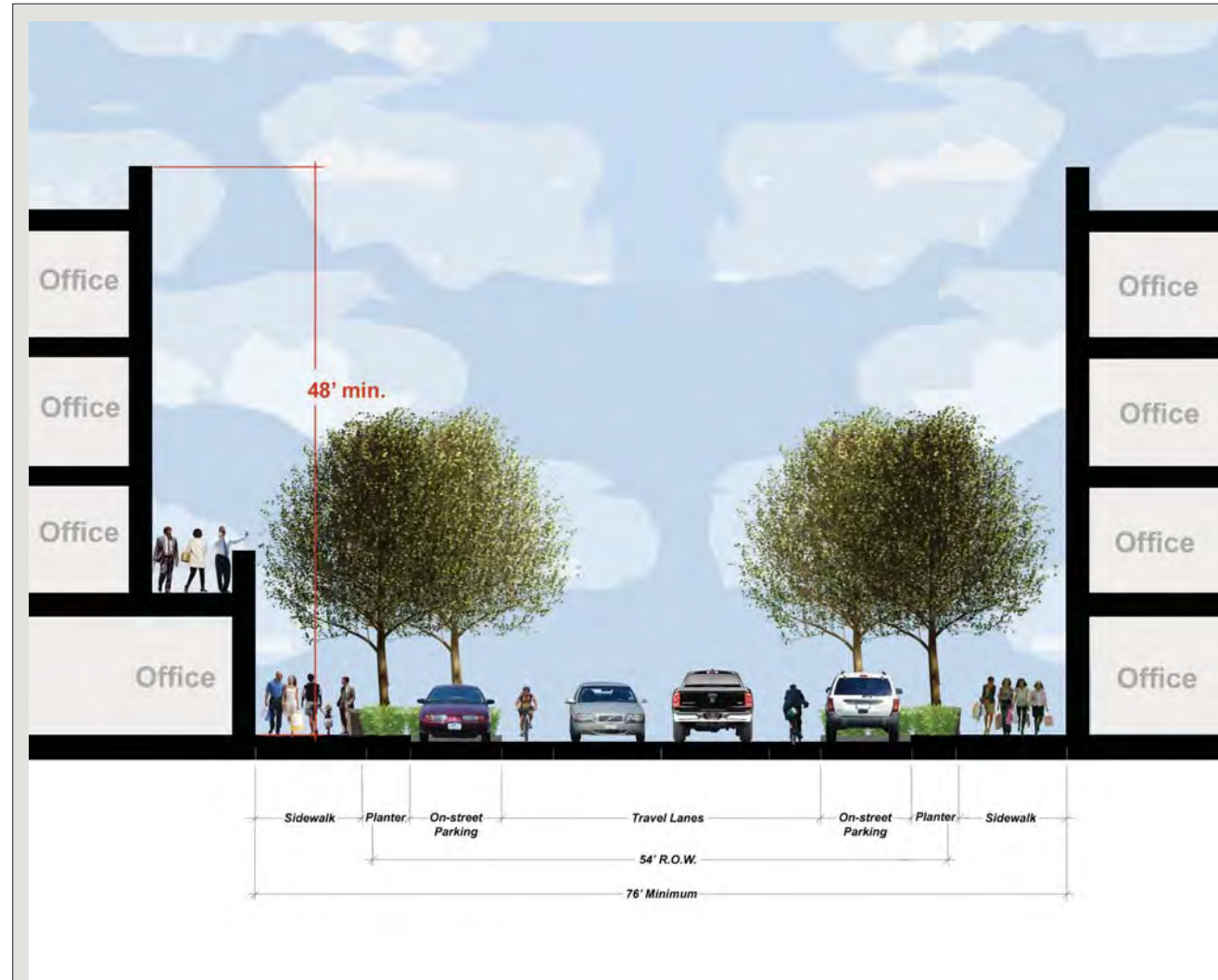
Stand alone office building.



Accented entryway of a stand alone office building.



Contemporary material for stand alone office building.



Section showing stand alone office with streetscape.



Opportunities for pedestrian rooms near office buildings.



Stand alone office building.

### Stand Alone Office:

Konterra Town Center East contains two types of office space - office over retail and stand alone office. While the former is contained primarily in the central core of the downtown the latter makes up the majority of available office space and is generally located toward the western perimeter edge of the downtown. Locating the stand alone office here encourages a future connection to further office development across Perimeter Drive West where it will be highly visible adjacent to Interstate 95. This location will lessen the impact of office traffic within the core making the retail and living environments safer and less congested.

The stand alone office buildings in the Konterra Town Center East core are grouped together in twos and threes creating pockets of office use that are horizontally rather than vertically integrated into the site. This arrangement essentially creates urban office parks allowing the buildings to share parking in an efficient manner. By placing the office to the western edge of the downtown it is possible to contain its impact on the other uses, such as parking demand and traffic congestion, while still sharing an adjacency to the retail and residential components which remain accessible by walking or driving.

Stand alone office is also beneficial to the project in terms of shared parking. By nature offices are typically used during the weekdays from a.m. to p.m.. At night and on the weekends the demand for office parking drops dramatically. This drop in office demand can be used to support the additional demand for retail and theater parking at night and on the weekends creating a symbiotic relationship between the uses and potentially lowering the number of parking spaces required to be built.

The stand alone office in Konterra Town Center East has many of the same bulk restrictions as the office over retail component; one exception is the required building height minimum. The stand alone retail is required to be a minimum of 48 feet high from the ground to the top of the parapet. The office over retail required minimum height is 25 feet from the ground to the top of the parapet. This discrepancy allows the office over retail to be as little as one story over retail and requires the stand alone office to be three to four stories in height. This height requirement therefore allows office to exist in small quantities within the central downtown core in the form of office over retail and also provides a mechanism for creating greater concentrations and densities of office within the project by way of stand alone office. This provides the greatest opportunity for mixing uses, both vertically and horizontally.



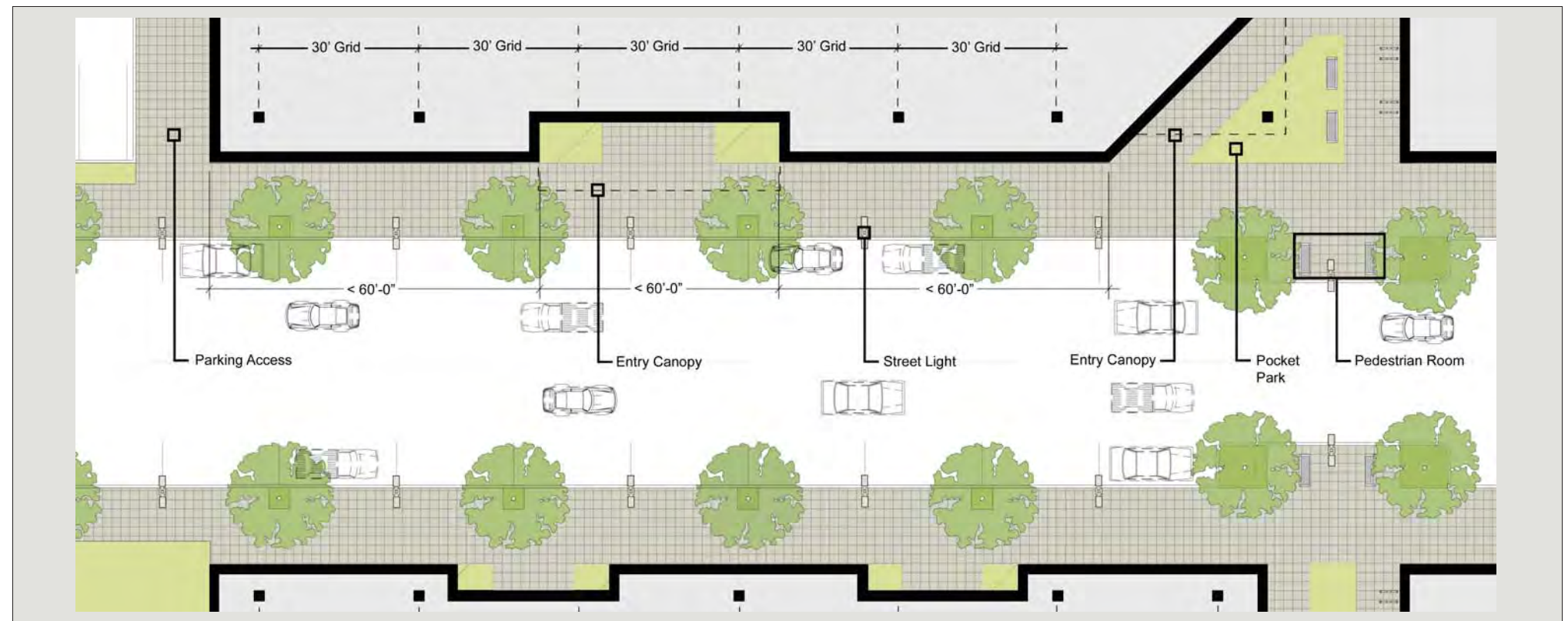
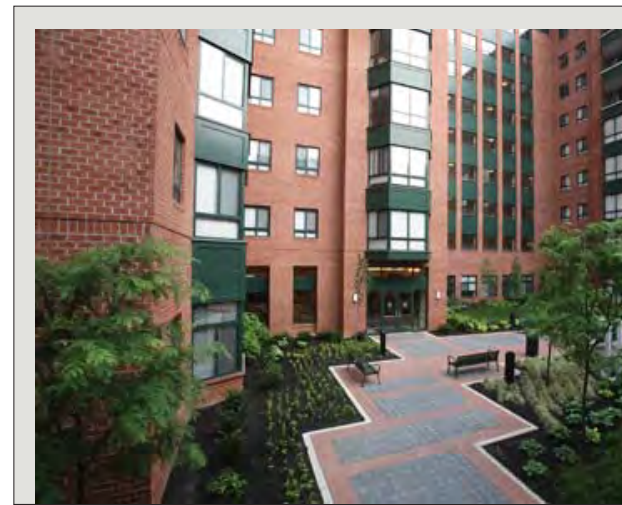
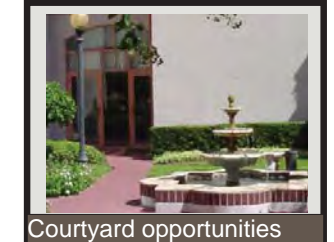
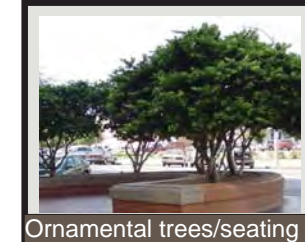
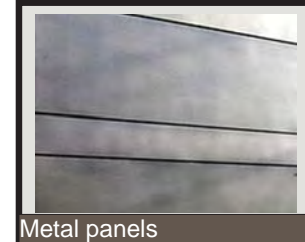
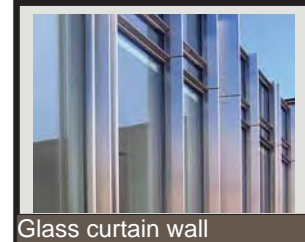
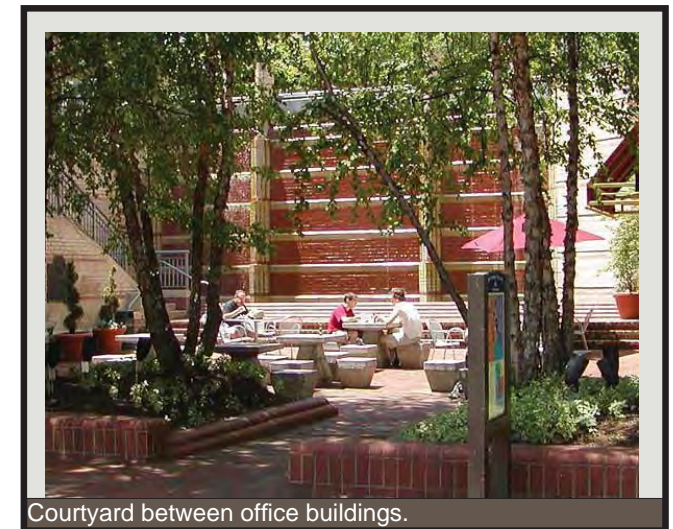
Contemporary stand alone office building.



## 4.6 Stand Alone Office

### Recommendations:

- Locate parking behind building or in parking structure
- Locate service/loading behind the building away from public view
- Locate main building entry at street or publicly accessible path way
- Provide pedestrian paths to parking structure and lot parking behind the building
- Provide pedestrian paths to adjacent office buildings
- Screen mechanical equipment from public view
- Express primary entry with higher volume, special building materials and architectural details
- Windows should reflect the building's structural grid
- Provide visitor drop-off areas and on-street parking at public building entries
- Encourage pedestrian connections from parking areas to building entries at public streets
- Provide street trees along parking lots, driveways, drive aisles and pedestrian connections
- Screen trash enclosures from public view with a fence and landscaping
- Provide street furniture, bicycle racks and shade for pedestrian use
- Office buildings will be set back a minimum of 15 feet from the face of curb to allow for wide continuous sidewalks
- Variation in material and expression is encouraged
- Pedestrian crosswalks will be clearly articulated and highly visible at intersections where to provide safe and convenient connections from parking to office buildings.





## 4.6 Stand Alone Office

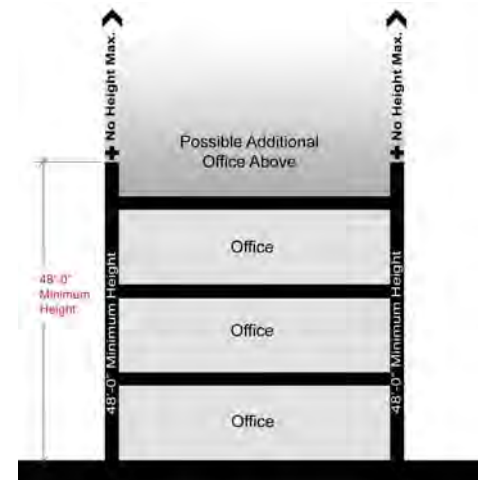
### Lot Coverage



#### Requirements:

60% Minimum Lot Coverage  
100% Maximum Lot Coverage

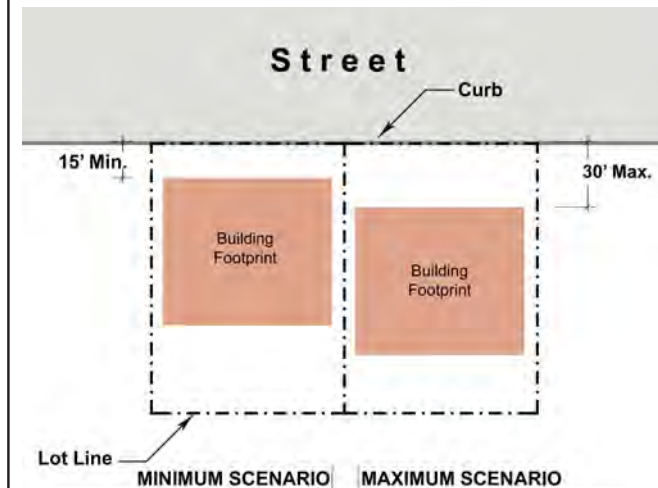
### Building Height Restrictions



#### Requirements:

48' Minimum Height  
(from ground to top of parapet)  
No Maximum Height Req.  
(from ground to top of parapet)

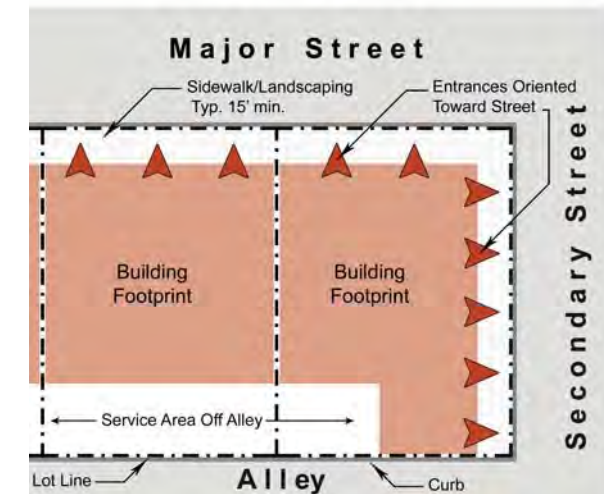
### Building Setback



#### Requirements:

15' Minimum Setback  
(front of building to front of curb)  
30' Maximum Setback  
(front of building to front of curb)

### Orientation / Service



#### Requirements:

Locate Building Entrances so they Orient  
Toward the Street or public realm  
Locate Service Areas Off Alleys and at the  
Rear of Buildings

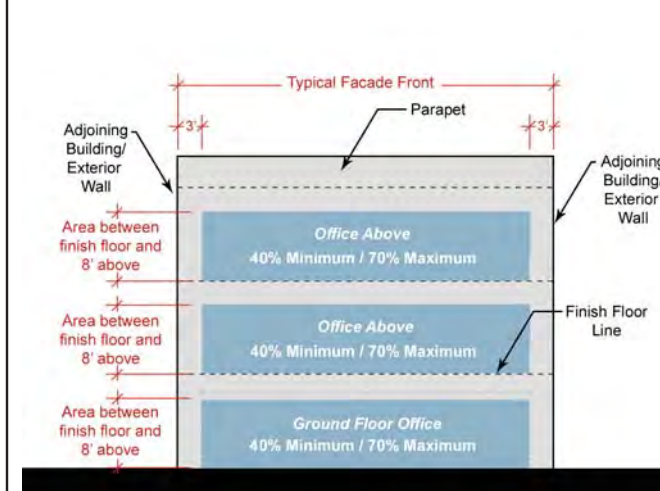
### Entry Differentiation



#### Intent:

- To express the building's use architecturally using color, material, size and proportion to effectively achieve a coherent and thoughtful composition that informs the user of the building's use and where to enter.

### Fenestration

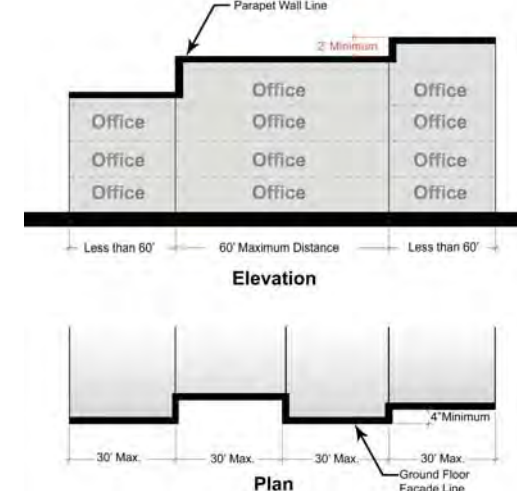


#### Requirements:

**Stand Alone Office:** 40% Minimum / 70% Maximum Transparency within the Fenestration Zone.

See diagram for definition of Fenestration Zone

### Facade Break / Parapet



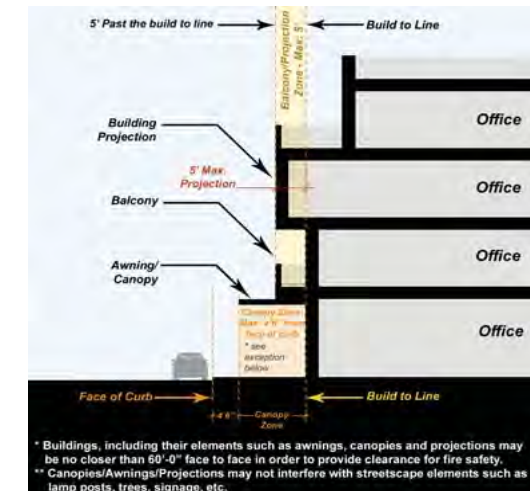
#### Requirements:

Parapet Wall should vary every 60 linear feet or less, to create visual interest.

Ground floor facade walls must change plane every 30 linear feet or less, to prevent monotony.

Depth change not < 4", Height change not < 2'

### Canopies/ Balconies/ Projections



#### Requirements:

**Balconies / Building Projections** extend no more than 5' past the build to line.

**Awnings / Canopies** may project no further than the back of curb, and may not obstruct lighting, landscaping, etc.



## 4.7 Other Building Types

### Intent:

In order to maintain flexibility within Konterra Town Center East it is necessary to provide parameters for 'other building types' in addition to those described previously in this section as there may be exceptions/ additions to this list in the future. In the event that a building does not fit into a specific building typology outlined in this chapter it will qualify as an "Other Building Type" and therefore must conform to the basic standards in this section which are intended to protect and maintain the desired appearance and character of Konterra Town Center East.

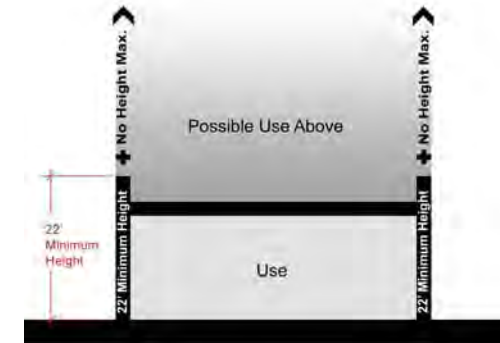
### Lot Coverage



#### Requirements:

**60% Minimum Lot Coverage**  
**100% Maximum Lot Coverage**

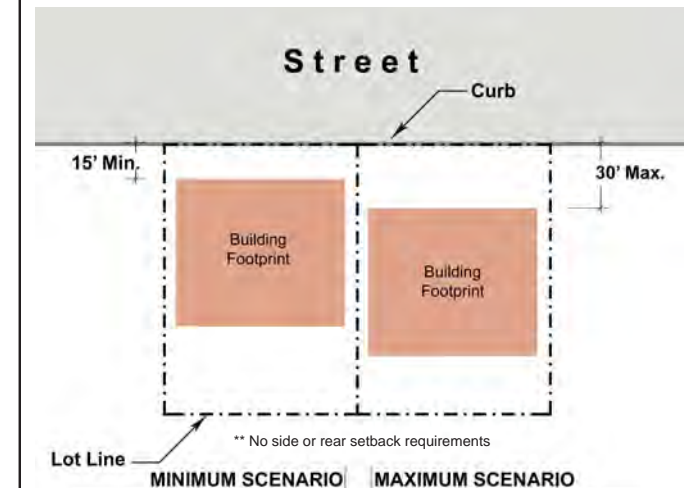
### Building Height Restrictions



#### Requirements:

**22' Minimum Height**  
 (from ground to top of parapet)  
**No Maximum Height Req.**  
 (from ground to top of parapet)

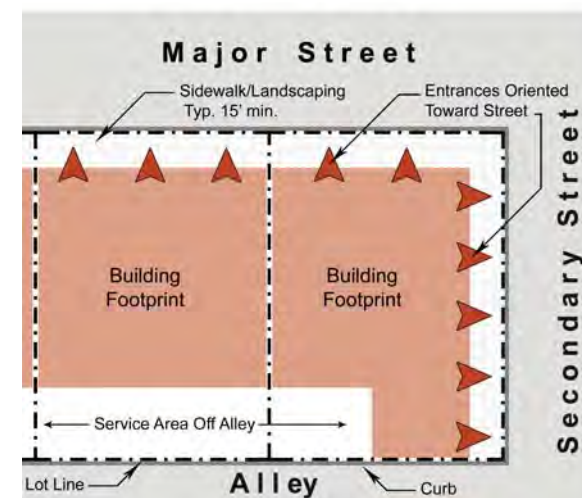
### Building Setback



#### Requirements:

**15' Minimum Setback**  
 (front of building to front of curb)  
**30' Maximum Setback**  
 (front of building to front of curb)

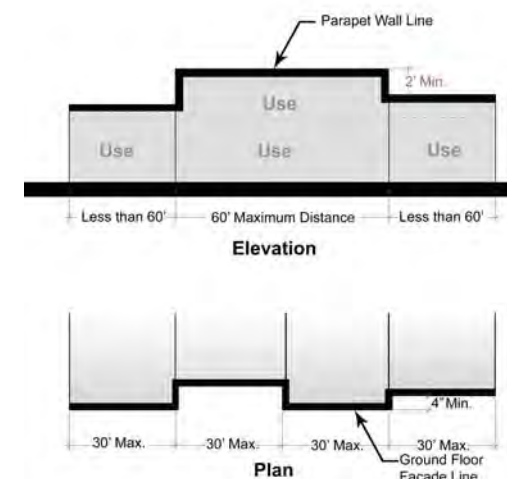
### Orientation / Service



#### Requirements:

**Locate Building Entrances so they Orient Toward the Street or public realm**  
**Locate Service Areas Off Alleys and at the Rear of Buildings**

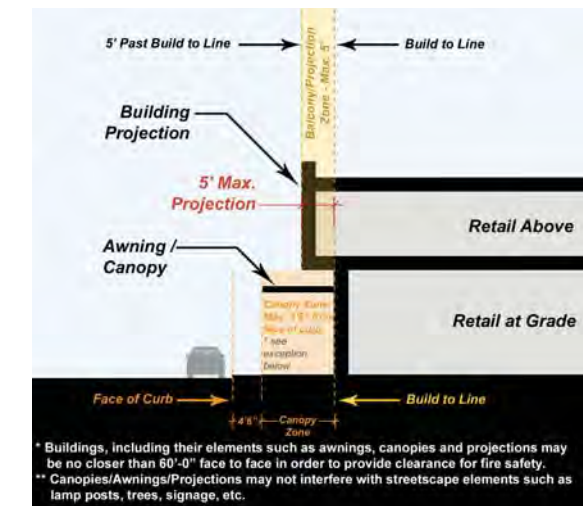
### Facade Break / Parapet



#### Requirements:

**Parapet Wall** should vary every 60 linear feet or less, to create visual interest.  
**Ground floor facade walls** must change plane every 30 linear feet or less, to prevent monotony.  
*Depth change not < 4", Height change not < 2'*

### Awnings/ Balconies/ Projections



#### Requirements:

**Balconies / Building Projections** extend no more than 5' past the build to line.  
**Awnings / Canopies** may project no further than the back of curb, and may not obstruct lighting, landscaping, etc.



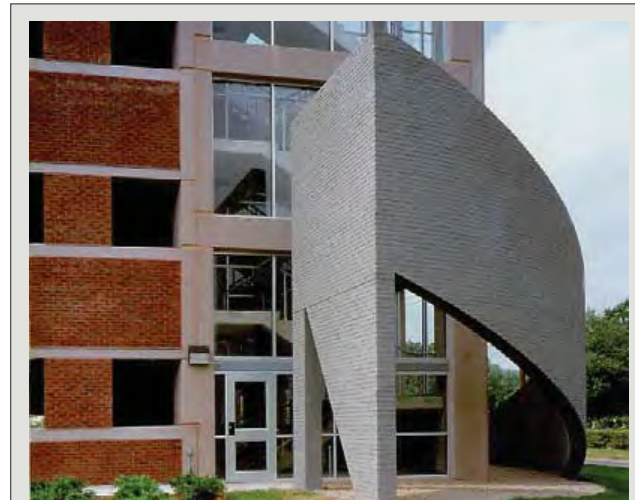
## 4.8 Parking Structures

### Intent:

Konterra Town Center East will need to incorporate parking structures over time as the development grows. Locating parking structures to the interior of blocks where possible will be beneficial in several ways. By locating the parking structures mid-block it is possible to maximize the density of cars in desirable proximity to the destination of the visitor or resident. Parking structures also minimize the visual impact of congestion lessening the need for surface parking lots which can be unattractive and which imply a more suburban experience. By incorporating the parking structures into the interior of the block the visual impact of the structure itself is minimized allowing shop fronts, office and residential buildings to be the focal point of the pedestrian experience. While the impact of parking structures should be minimal, it is important that they are easy to locate and navigate as well as safe. Lighting, signage, and transparency will be crucial to this issue and will be carefully considered in the design of the parking structures. Although the intent is to mask the parking structures behind the retail, office and residential uses, it will not always be possible to do so completely. In such cases it is important that the facade visible from the public realm be articulated in a way that it enhances the overall experience of the site. A street facing facade on a parking structure will be held to the same quality standard as a non-parking structure facade in the same location.

### Recommendations:

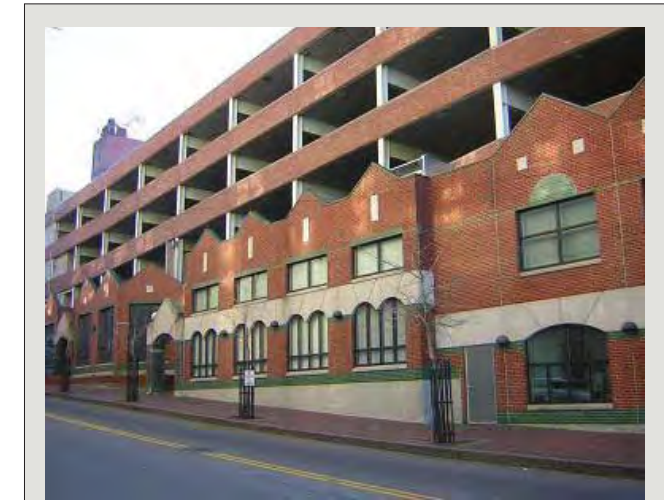
- Parking structures will not dominate the block in which they are part, they must be no greater in height than the highest building on that block.
- Where parking structures are visible to the public they will be designed and articulated in a way which enhances the pedestrian realm.
- High quality materials and techniques will be used in the design and execution of parking structures so that they enhance the overall experience of the project.
- Signage and lighting will be designed to make the parking structure experience an easy, safe and satisfying one.
- Curb cuts will be kept to a minimum and areas where cars and pedestrians cross will be carefully articulated and enhanced to raise awareness of the overlap.
- Parking stalls will typically be 9'-0" x 18'-0"
- Drive aisles and ramps will typically be 24'-0" in width to allow cars to easily pull into and out of spaces.



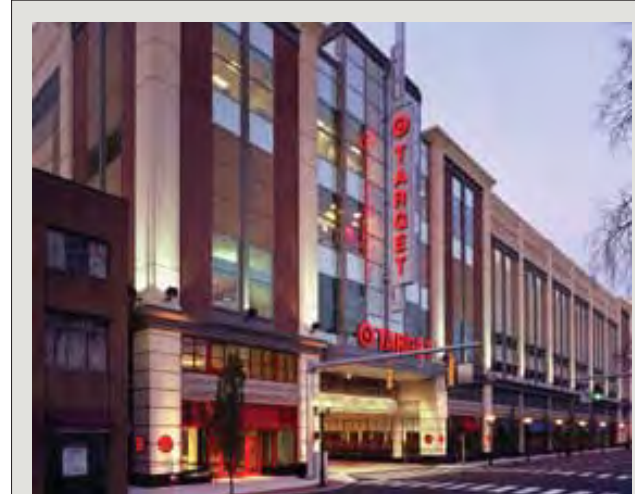
Celebrated parking structure entry.



Articulated parking structure facade.



Articulated parking structure facade.



Mid-block parking entry.



Mid-block parking structure exposed to public view on one side.



Section through mid-block parking structure.



# 5.1 Landscape Concept

The Landscape Framework Plan describes the major landscape components and significant planting designs for Konterra Town Center East. The design of the Landscape Framework Plan is inspired by the area's history as a mill town and illustrates the weaving of the landscape tapestry that is the heritage of the region's impressive cultural past.

The planting palette for Konterra Town Center East was chosen to establish a strong, memorable landscape complementing the region's existing plant inventories, streetscapes and planting themes. The Landscape Framework Plan creates a refuge of nature for an enclave serving as a premier gateway of Prince George County. Konterra Town Center East is destined to become the area's new growth center due to its ideal location within the Baltimore Washington Corridor.

The Landscape Framework Plan, in concert with the architectural master plan for Konterra Town Center East, shall deliver a unique aesthetic within the City of Laurel, Maryland. Konterra Town Center East, part of a larger 132-acre planned community, shall contain mixed-use, residential and retail uses and represents the symbolic and actual heart of this gateway community.

The 488-acre Konterra Town Center East site is bordered on all four sides by highways. The I-95 to the west, the Contee Road inter-change to the north, Virginia Manor Road to the east, and a proposed inter county connector to the south.

The core 130-acre Town Center site shall consist of a mix of uses woven together by roadways designed on a grid system that follows natural drainage patterns and the topography of the site. The grid system shall facilitate comfortable access in and out of the site and create a cohesive pedestrian environment. The environs, 358-acres surrounding the core area shall consist of commercial office, research and development, residential and open space.

The rectilinear grid of the site plan provides the Landscape Framework Plan with flexible building blocks averaging 600 feet by 500 feet, reinforcing the alignment of the greater precincts of Konterra Town Center East running from the northeast to the southwest. The main street concept allows for a mix of uses and a diversity of spaces within the urban plan, including open-air plazas and outdoor restaurants and parks. The landscape is intended to provide a community amenity supporting the town center's architectural design.

The Landscape Framework Plan describes the landscape concepts and general planting designs for Konterra Town Center East. Plant materials were selected based on the landscape heritage of the region and existing inventories within Prince George County.

The 358-acre environs landscape envelopes an enclave of mixed-use commercial office, research and development, residential and open space. The proposed perimeter development has an outward orientation that is both physically and visually integrated with the existing adjacent development and community improvements. As such, the environs landscape supports and addresses the need for coordinated access, exposure and view sheds into project.

As the development of Konterra Town Center East matures, views of significant buildings and landmarks will be promoted and preserved from the highways. The scale of development shall transition to the north, east and south, through the phased development plan, to meet the surrounding uses

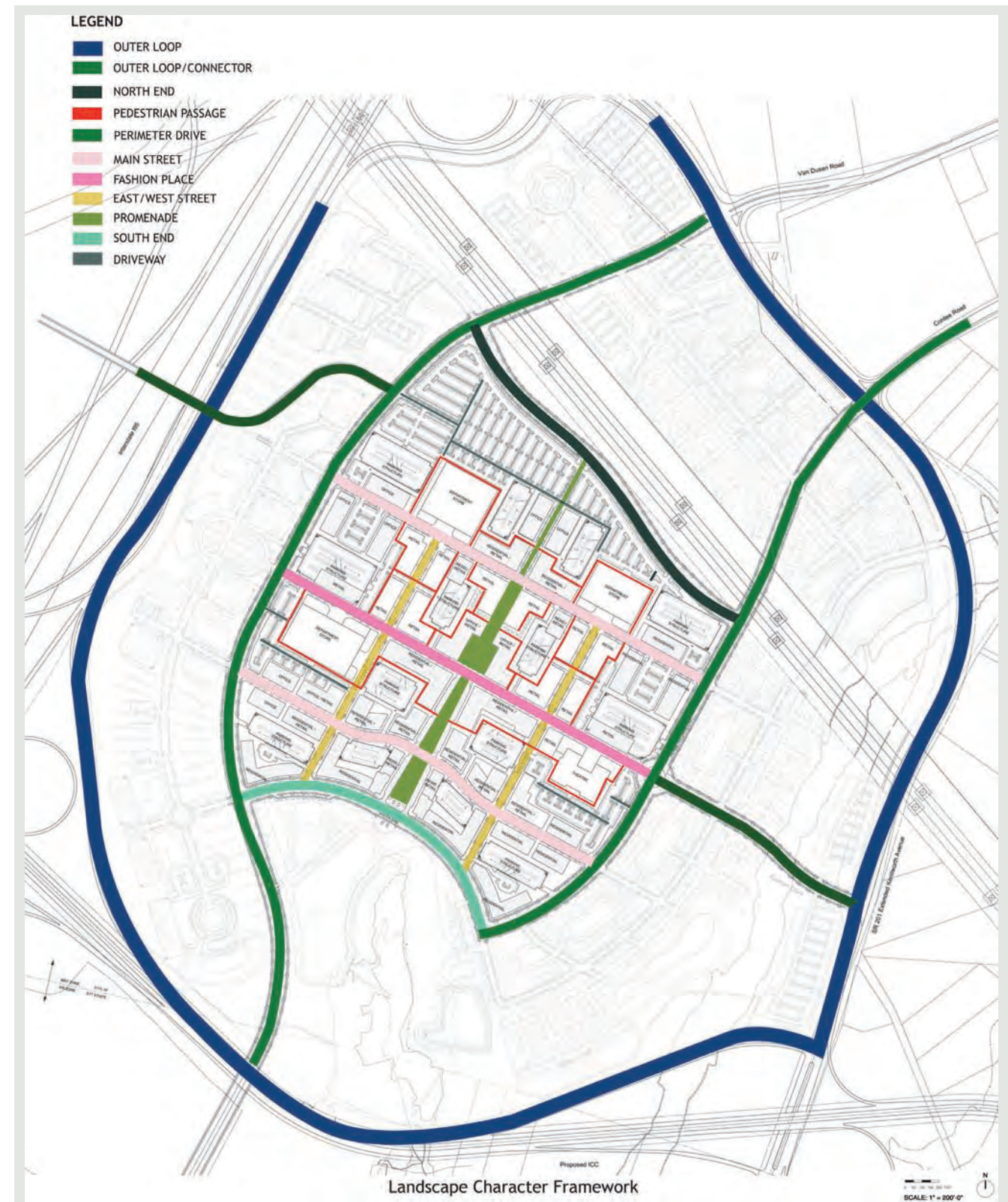
along the roads encircling the site. From the core these buildings shall command views of the surrounding countryside. The Landscape Framework Plan responds to these anticipated conditions by providing a choreography of evergreen and deciduous plant materials to compose the landscape.

The streetscape consists of a series of hierarchical layers designed to complement the unique character of each street based on the architectural plan and to enhance the pedestrian experience. Planting is used for shade, color, rhythm, skyline vistas to frame distant view sheds from outside and inside the community, understory planting for pedestrian scaling and compatibility with the architectural character. Individual garden areas on grade and on structure within the Konterra Town Center East shall address specific themes based on the region's landscape heritage.

The different tree and groundplane species create a hierarchy on the project scale and variety at the human scale where trees are used as both landmarks and focal points. In the core of the project the plant material shall be used to identify particular zones and linked open spaces along the streets.

Four landscape typologies are identified in the Framework Plan: 1) Perimeter Landscapes, 2) Streetscapes, 3) Core Plazas, Courts and Gardens and 4) Core Parking Lots and Interior Courts.

**Note:** The first permit plans for sites located within the adjoining blocks that front on the Main Streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of trees, street furniture and lighting fixtures for that street on which the project site is fronting.

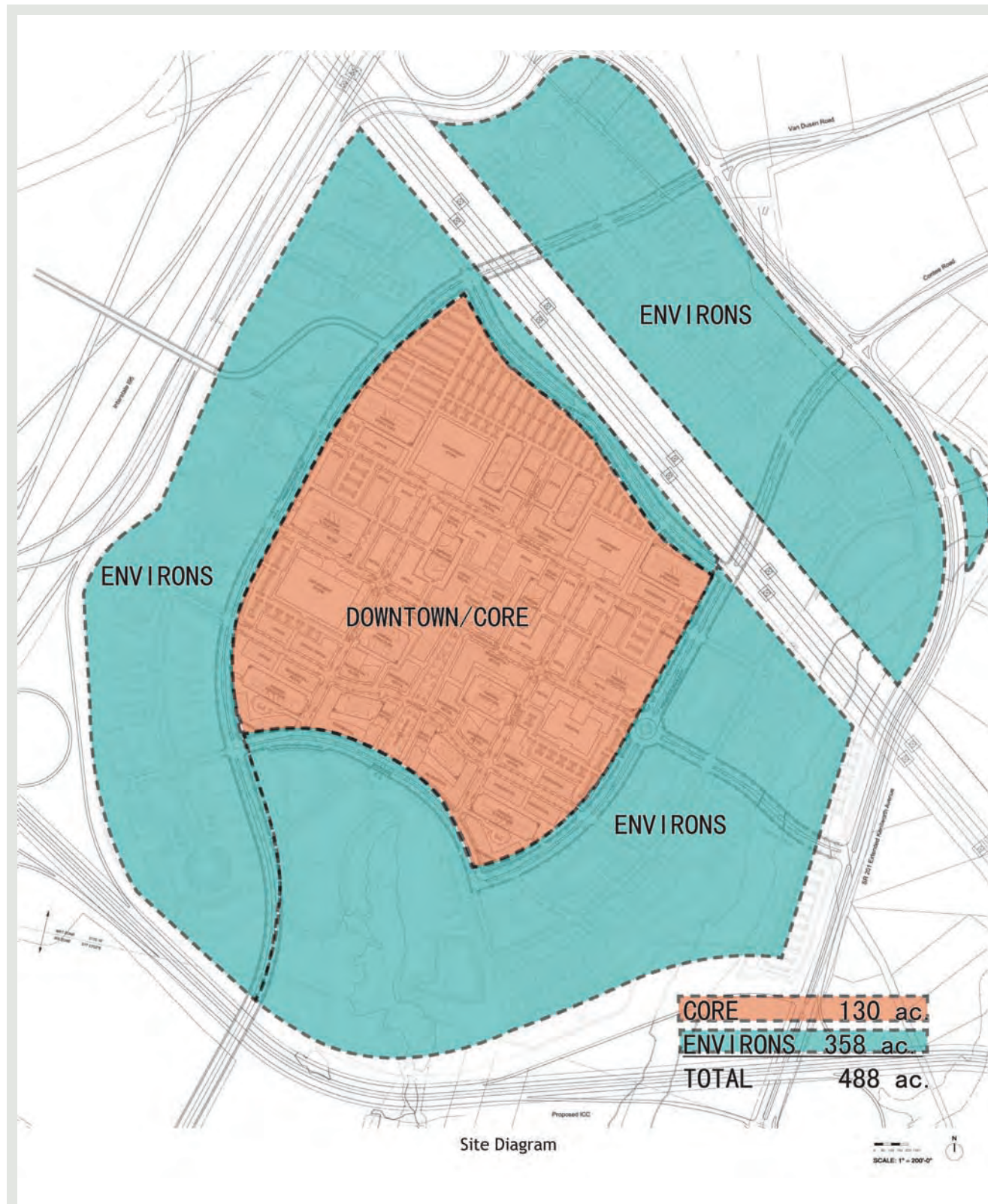


Landscape Concept Diagram

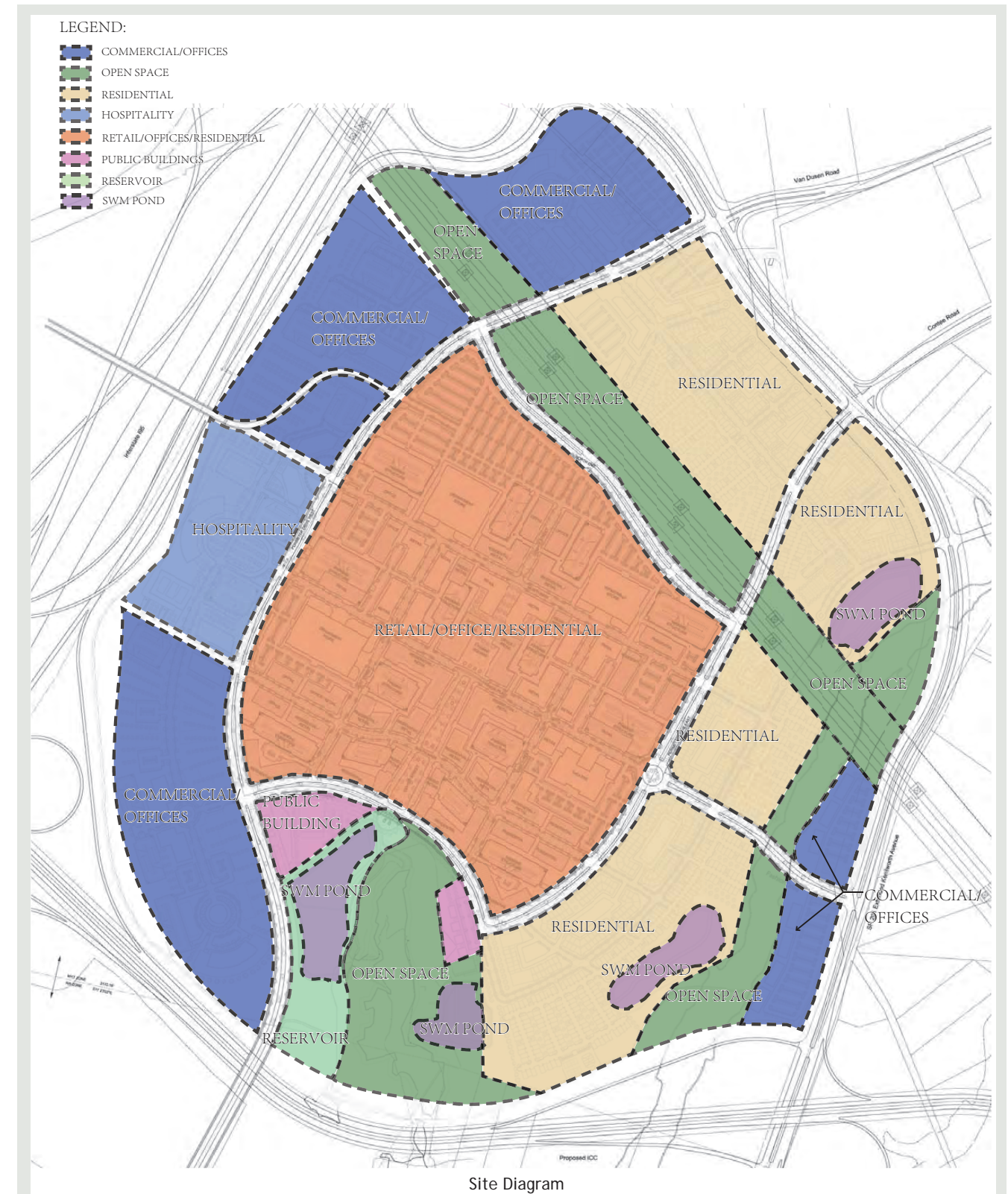
Landscape Concept Diagram



## 5.1 Landscape Concept



Landscape Concept Diagram



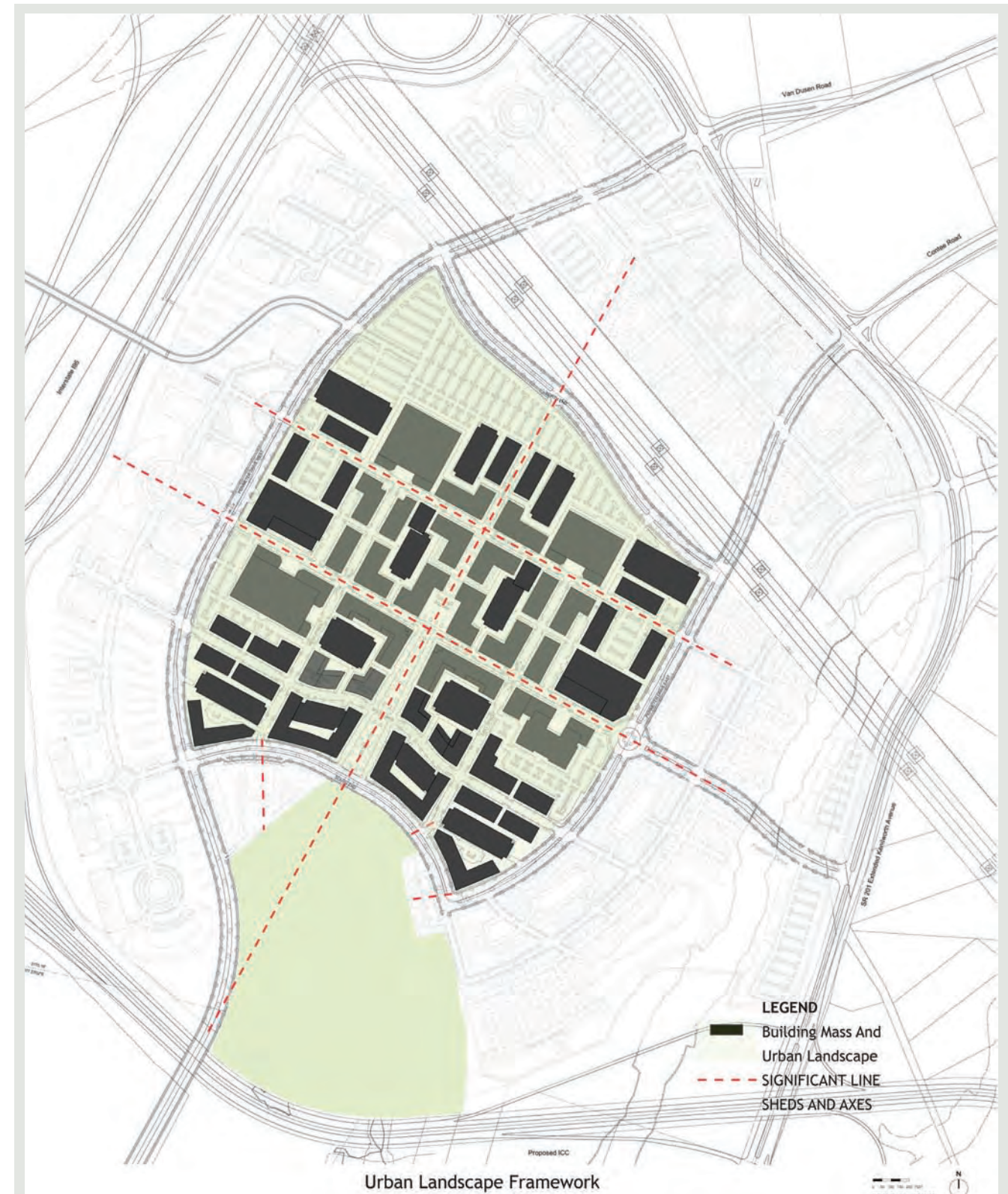
Landscape Concept Diagram



## 5.1 Landscape Concept



Landscape Concept Diagram



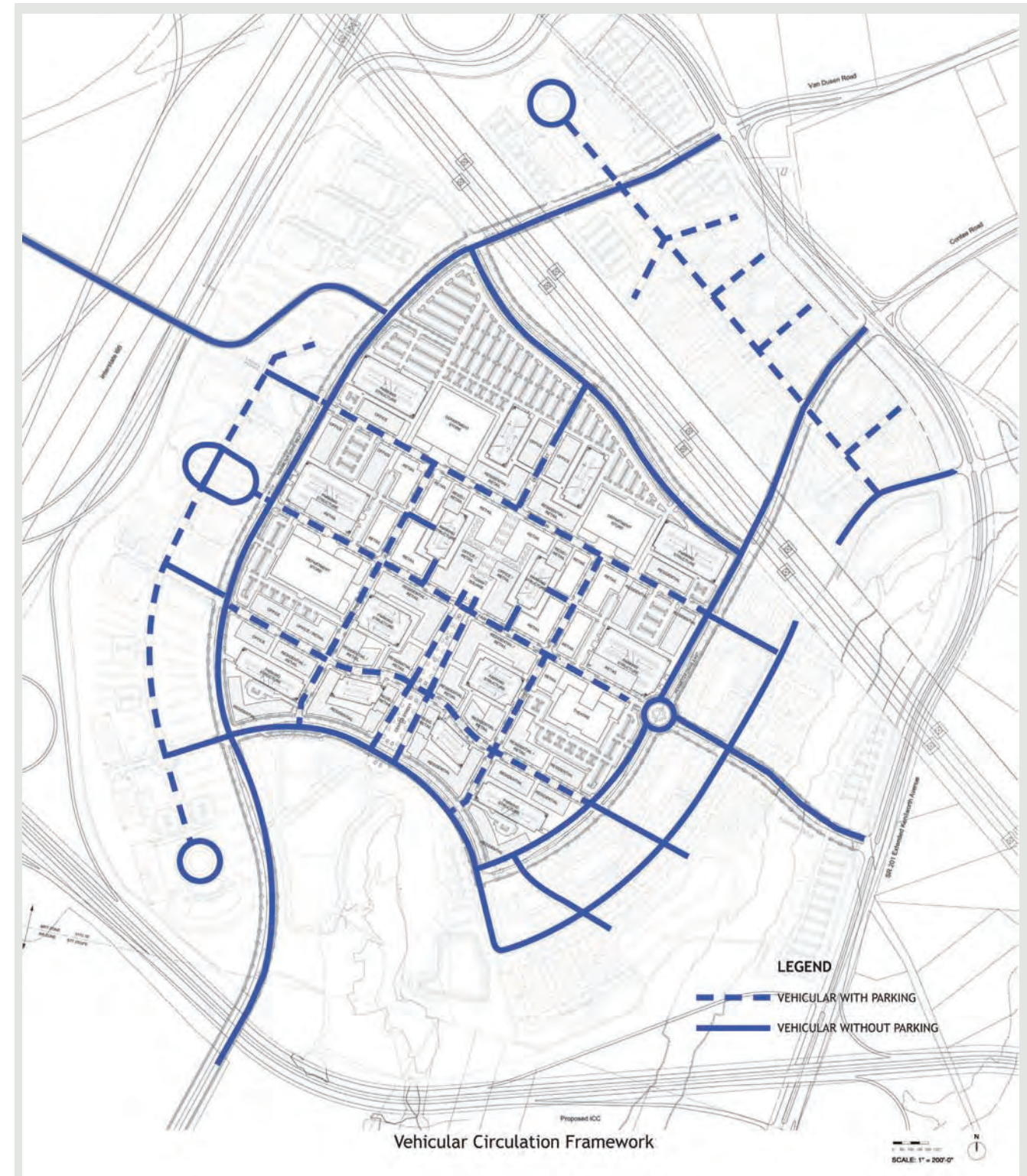
Landscape Concept Diagram



## 5.1 Landscape Concept



Landscape Concept Diagram



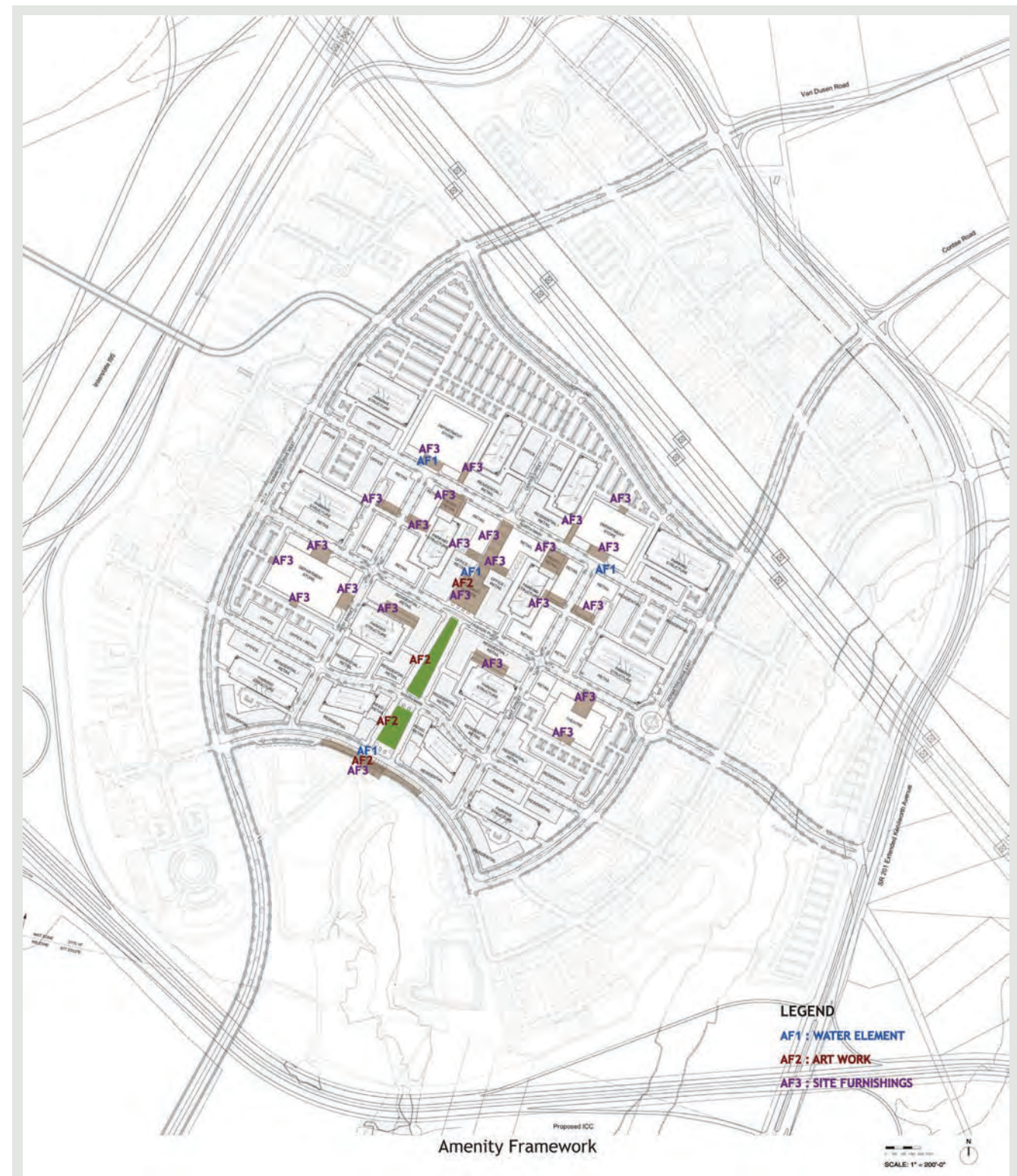
Landscape Concept Diagram



## 5.1 Landscape Concept



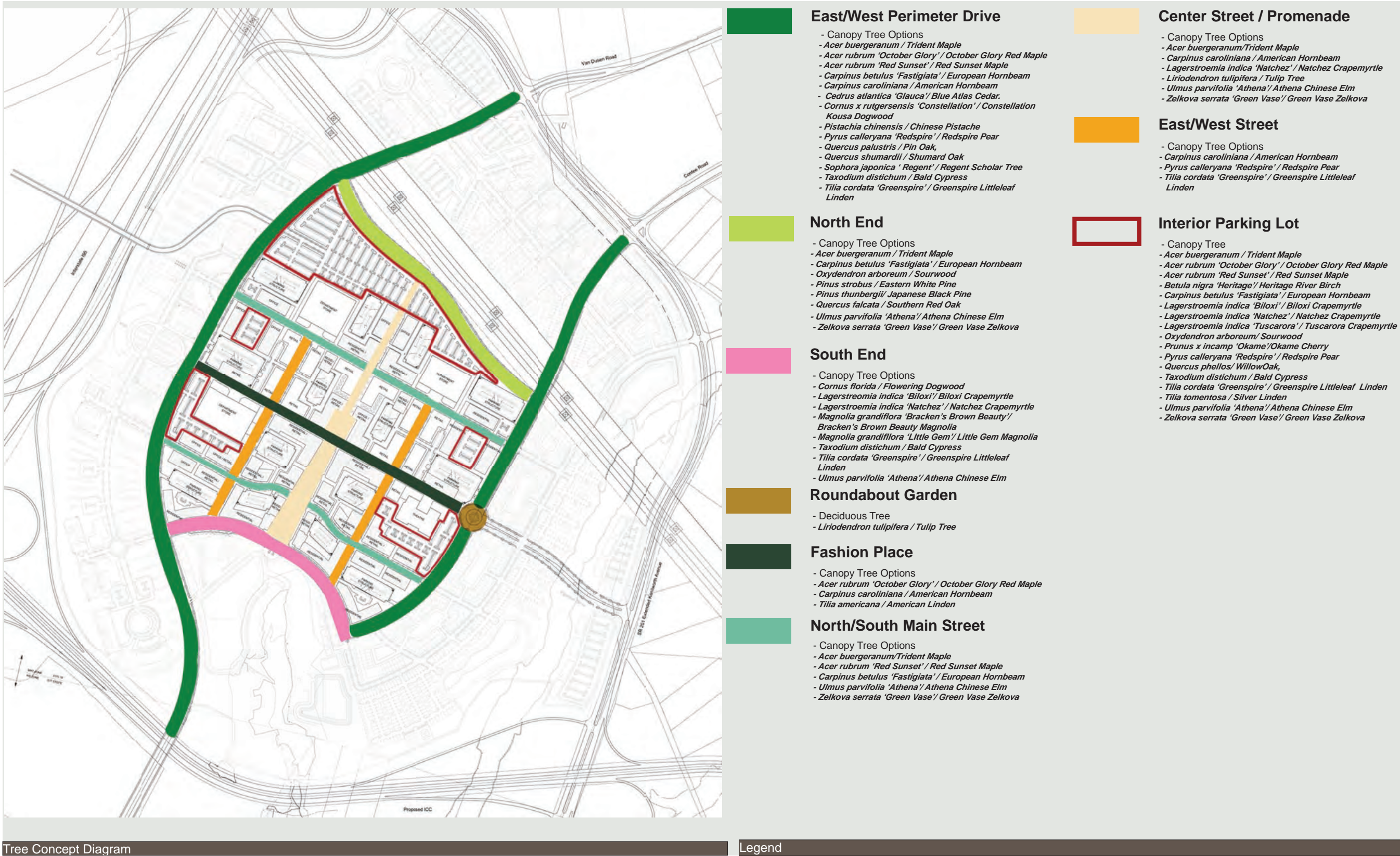
Landscape Concept Diagram



Landscape Concept Diagram



5.1 Landscape Concept



Tree Concept Diagram

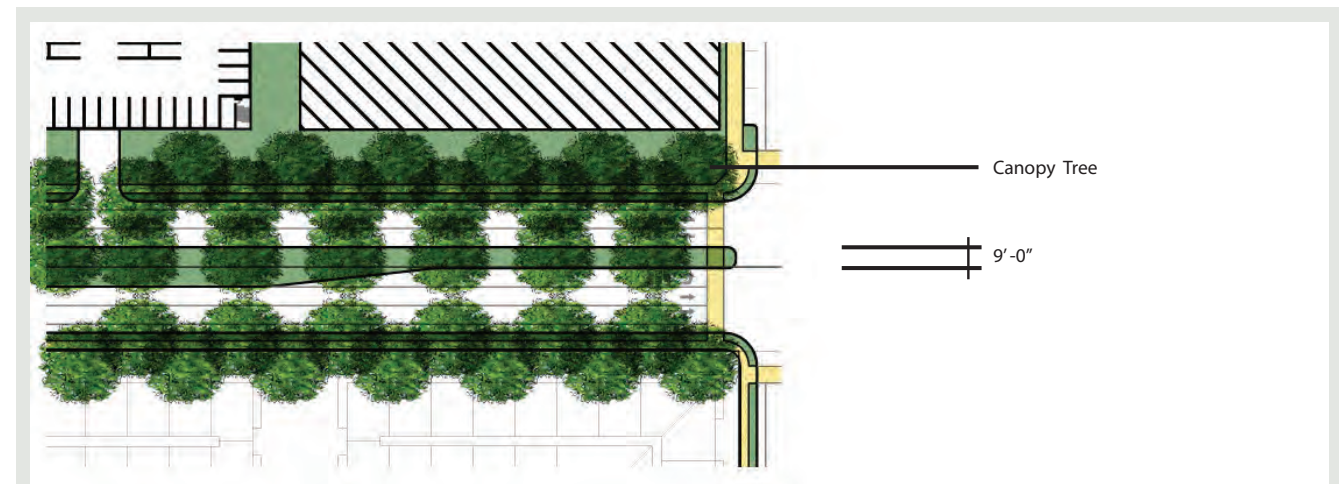
Legend



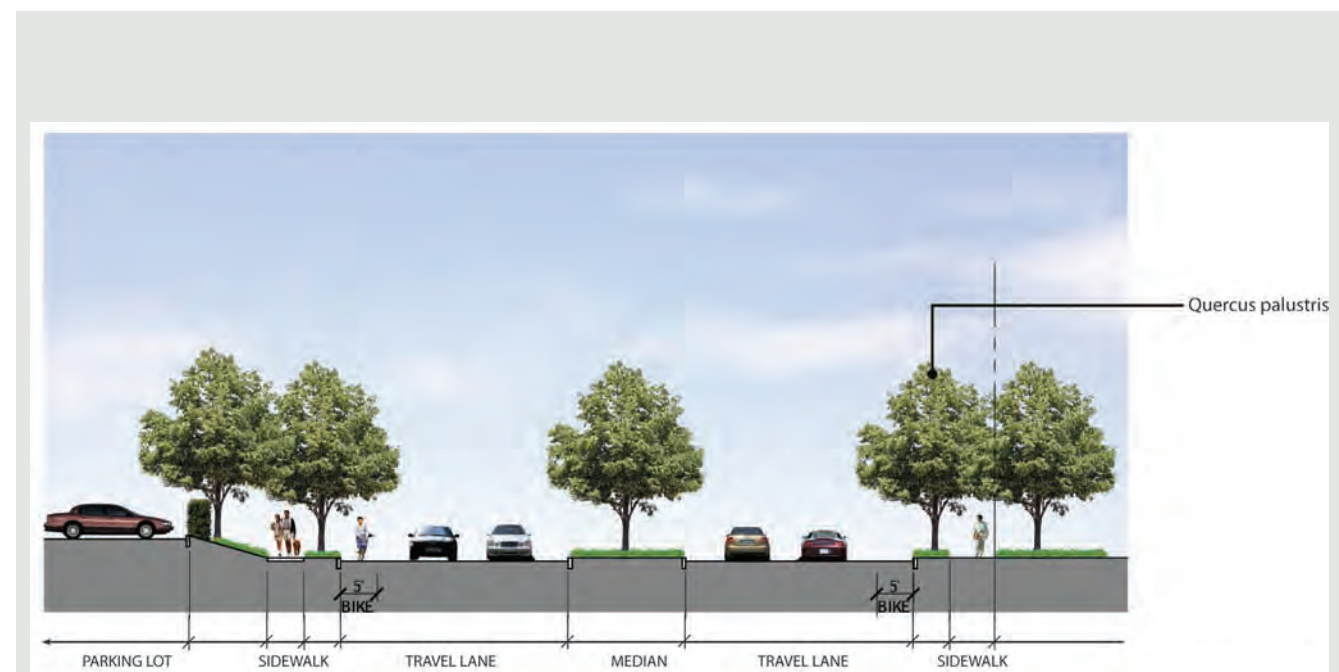
## 5.2 Street Landscape - East / West Perimeter Drive



Segment Location



Plan



Cross Section



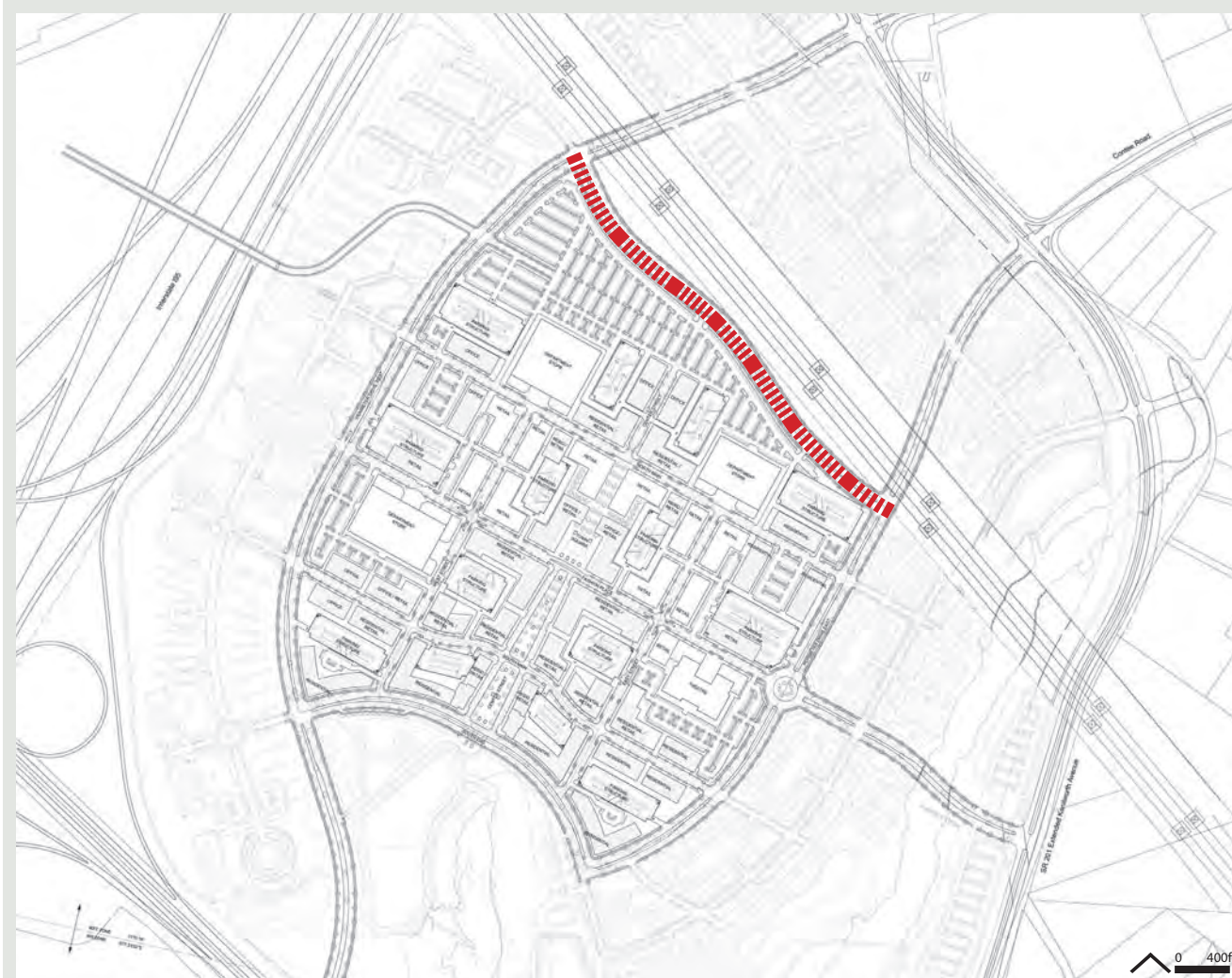
## 5.3 Street Landscape - North End



*Quercus falcata* / Southern Red Oak



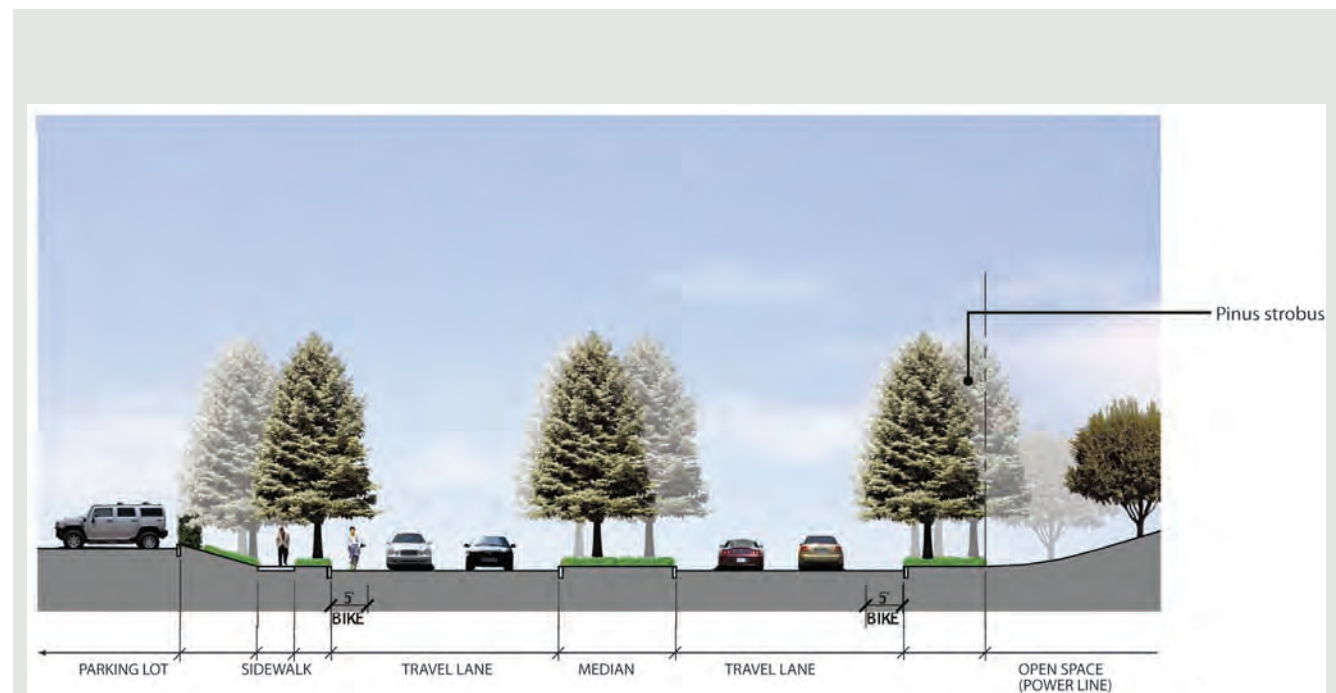
*Pinus strobus* / Eastern White Pine



Segment Location



Plan



Cross Section



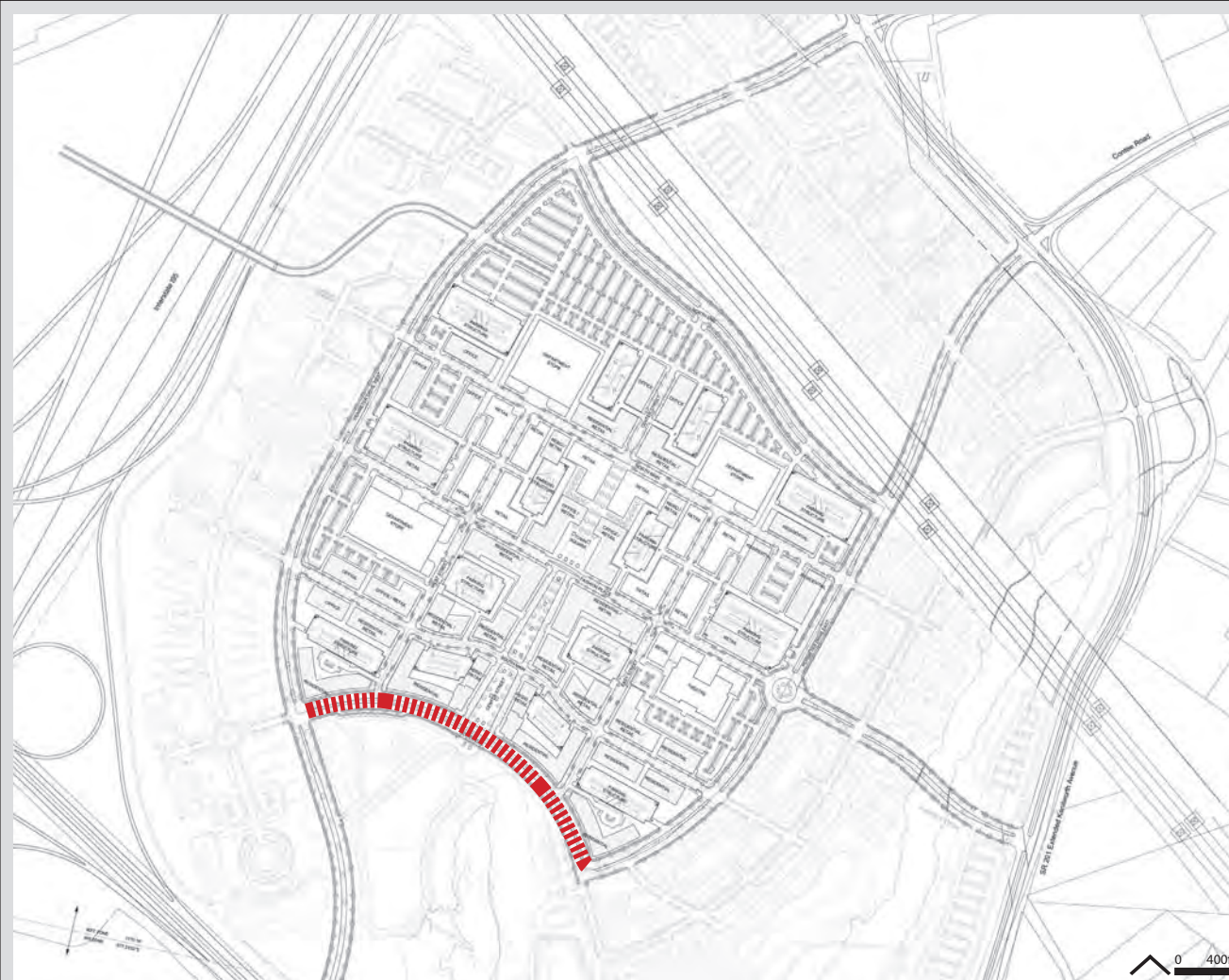
## 5.4 Street Landscape - South End



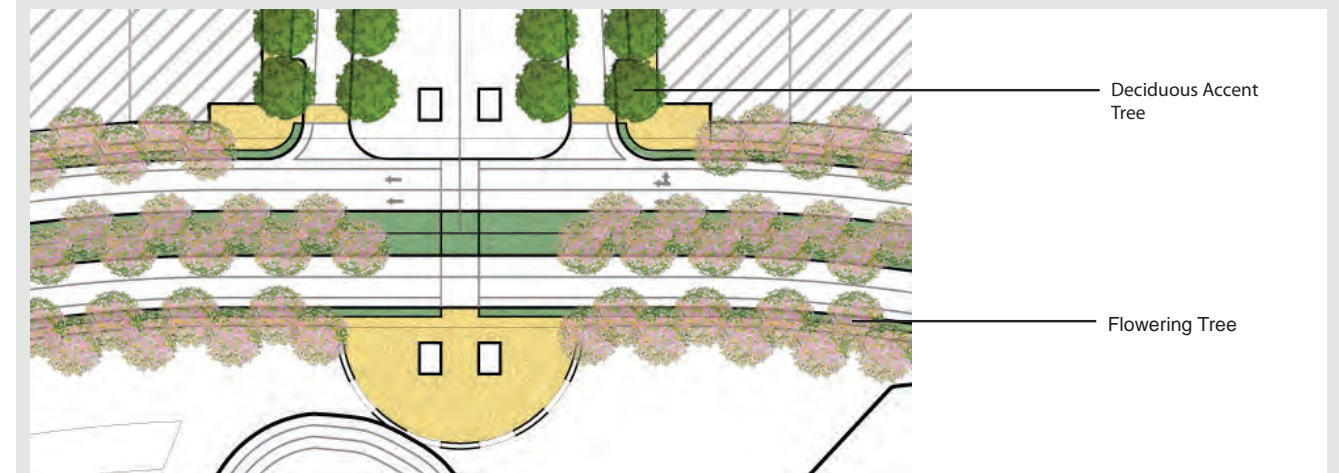
*Lagerstroemia indica* 'Natchez' / Natchez Crape myrtle



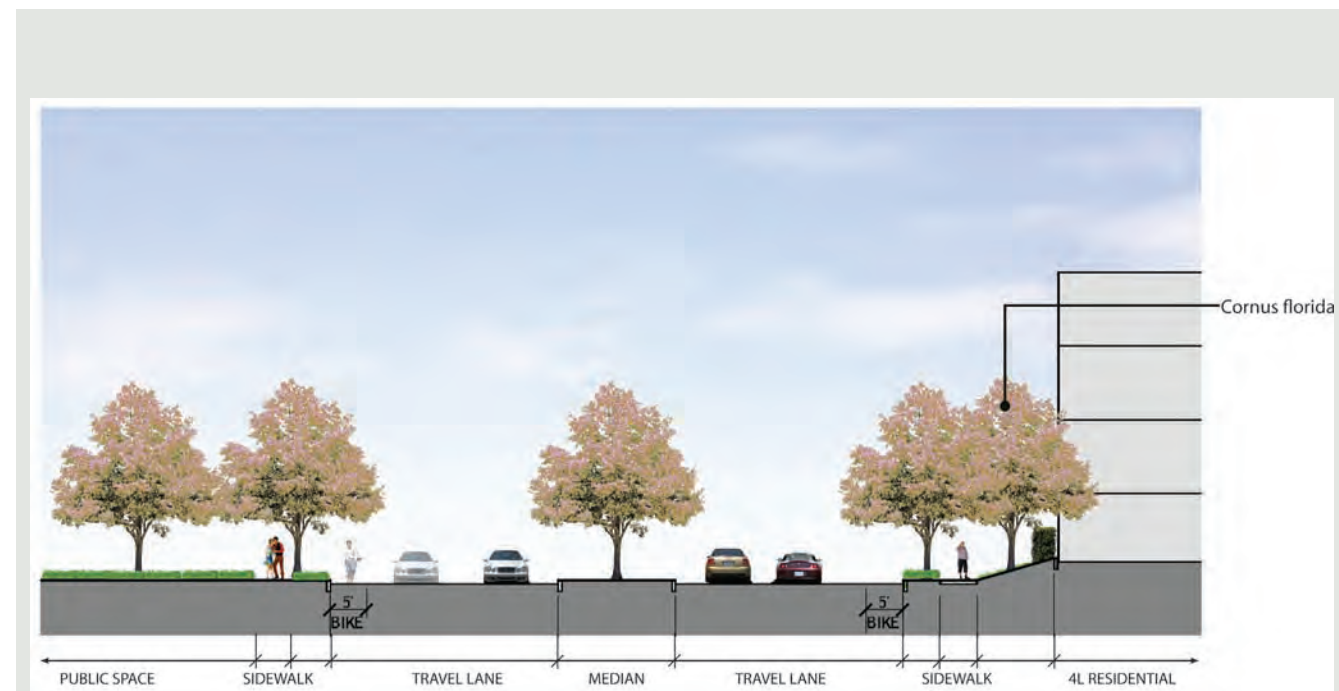
*Cornus florida* / Flowering Dogwood



Segment Location



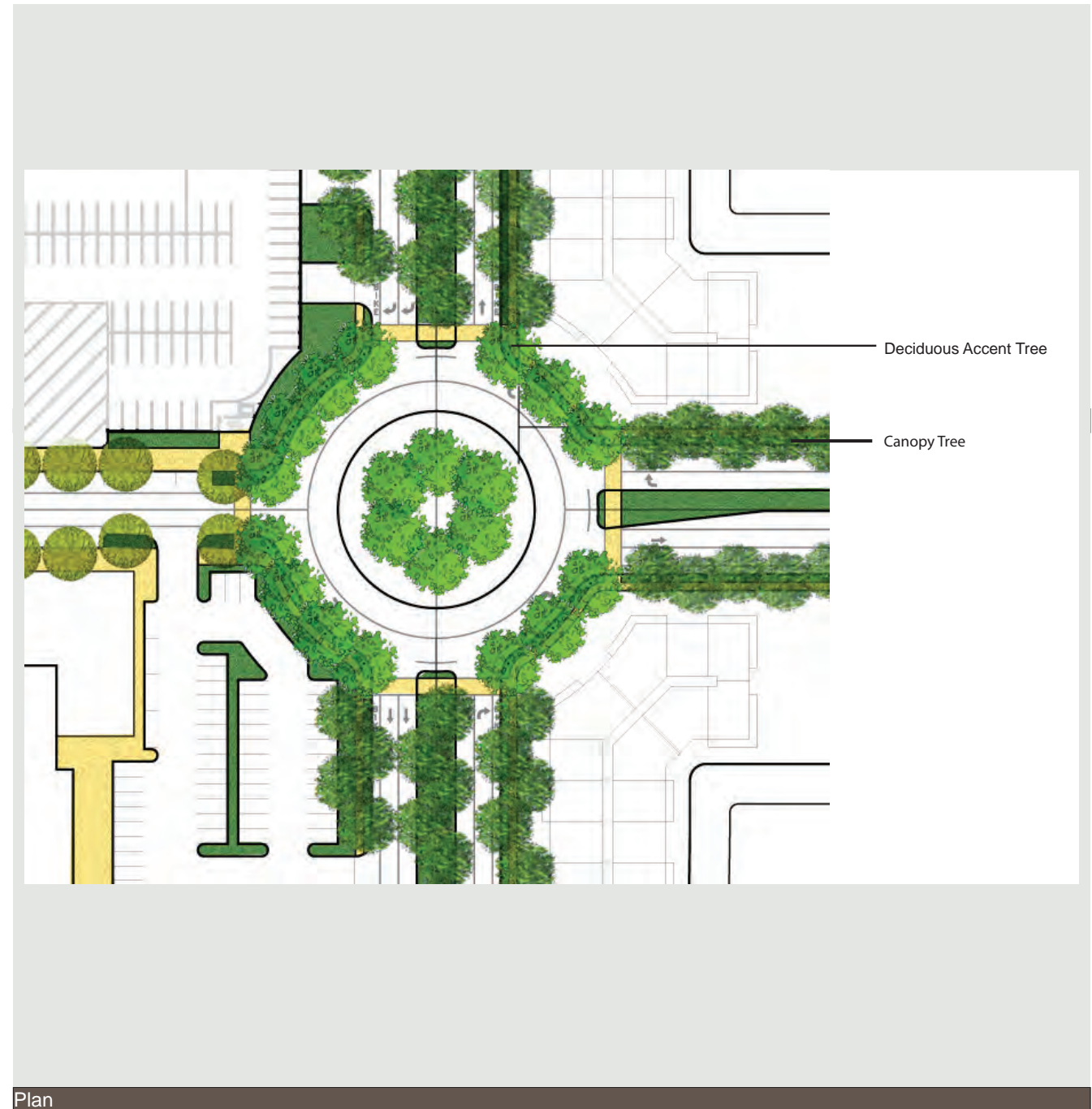
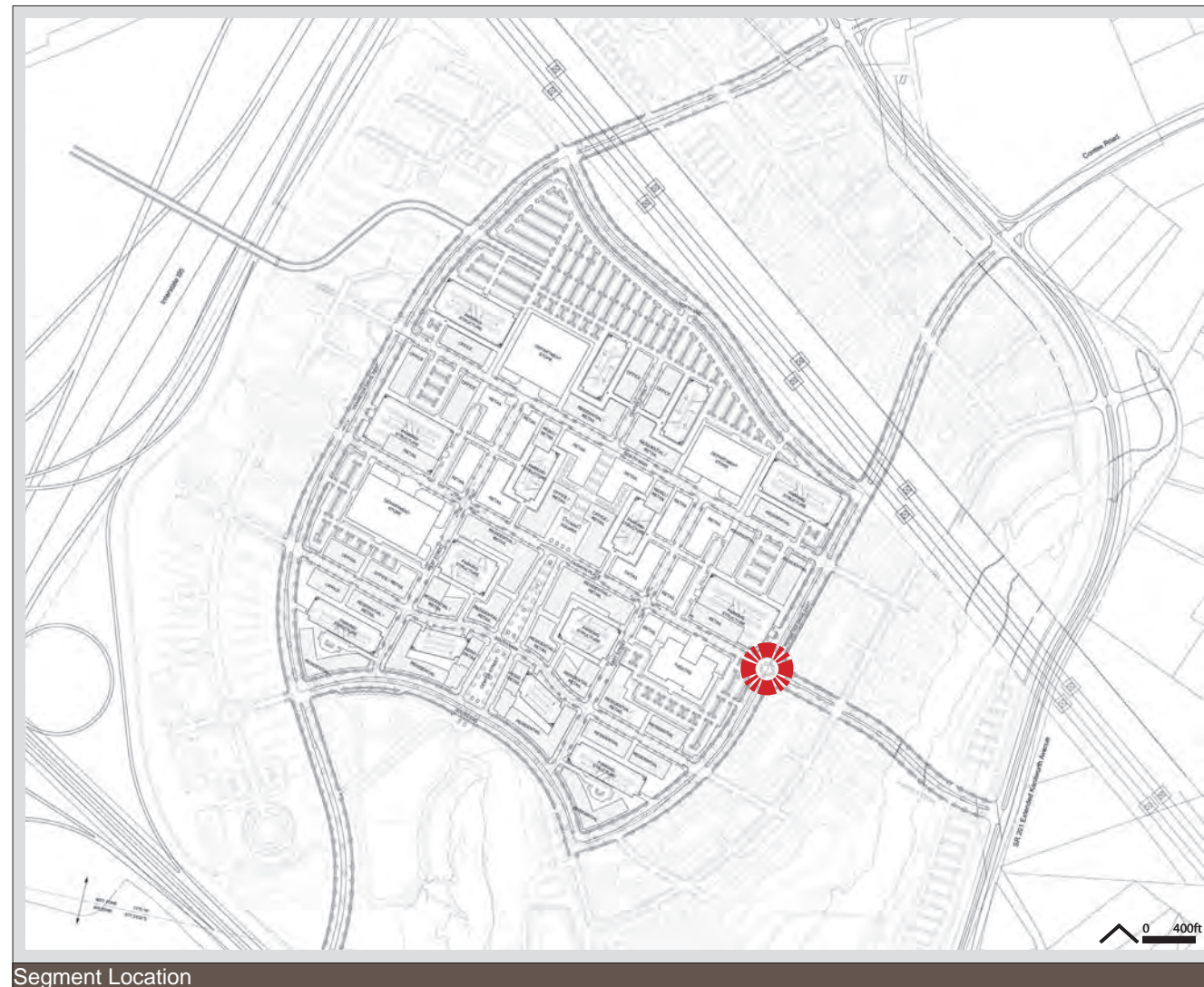
Plan



Cross Section



## 5.5 Street Landscape - Roundabout Garden at East Perimeter Drive

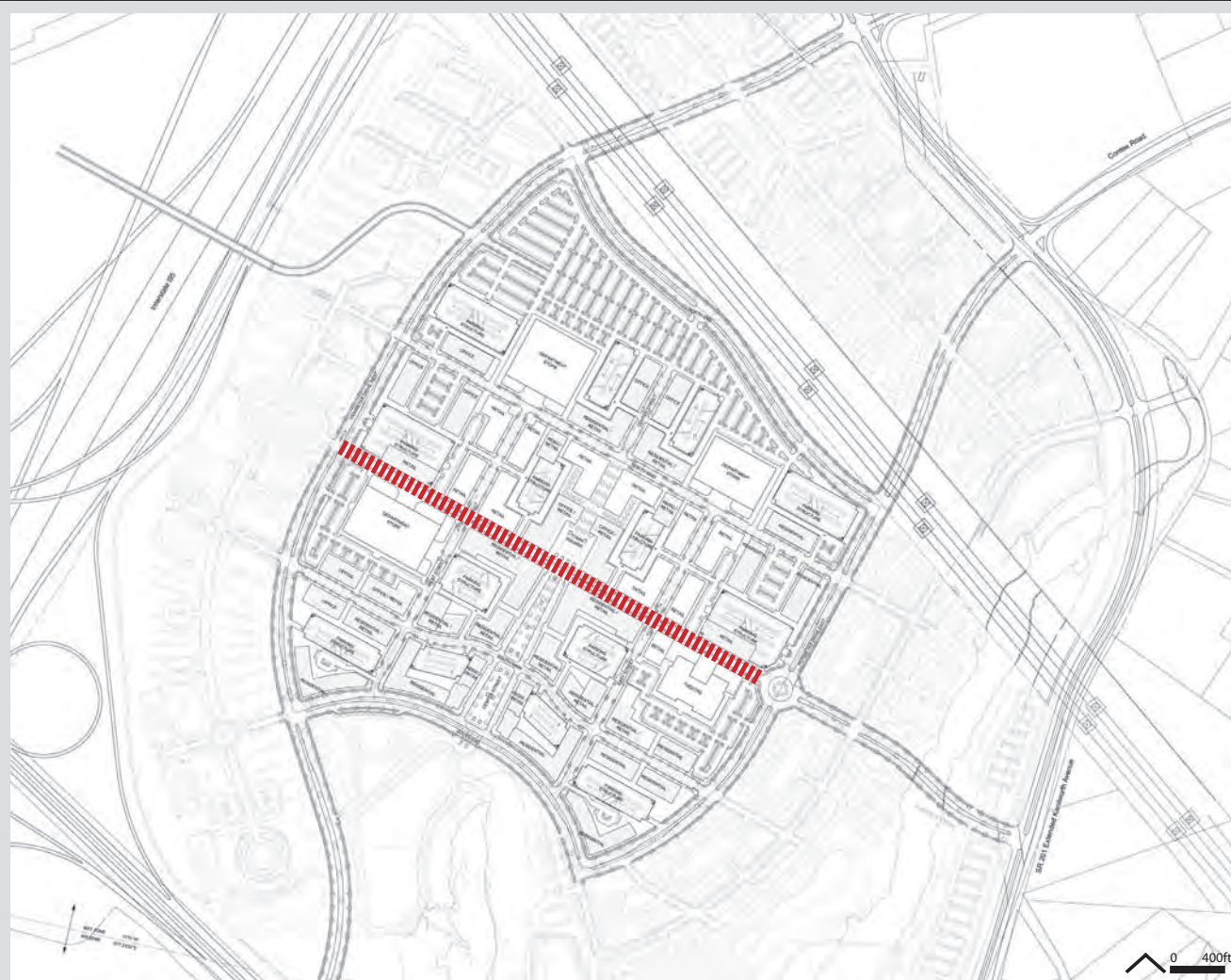




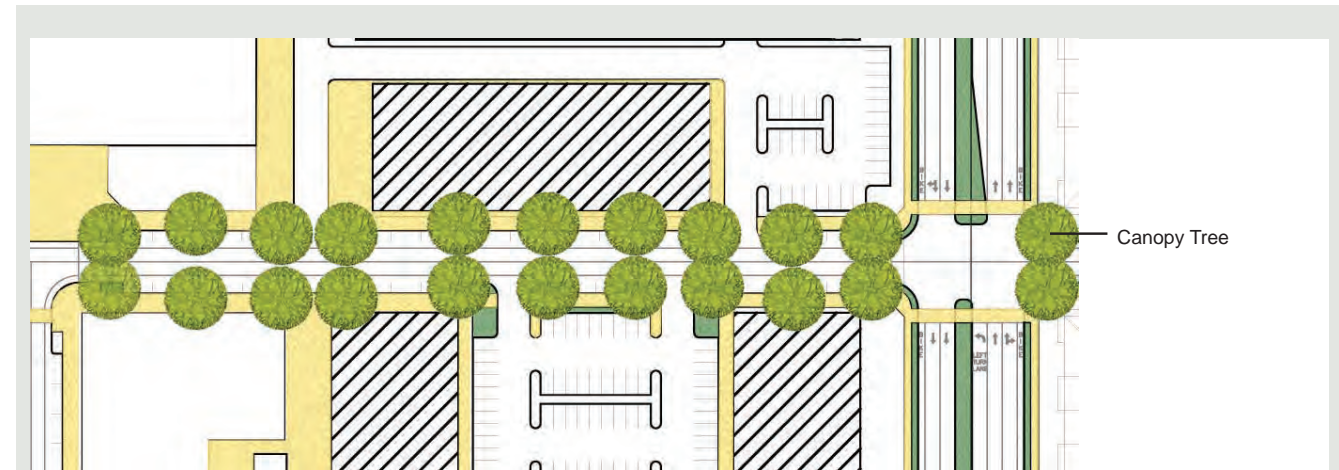
## 5.6 Street Landscape - Fashion Place



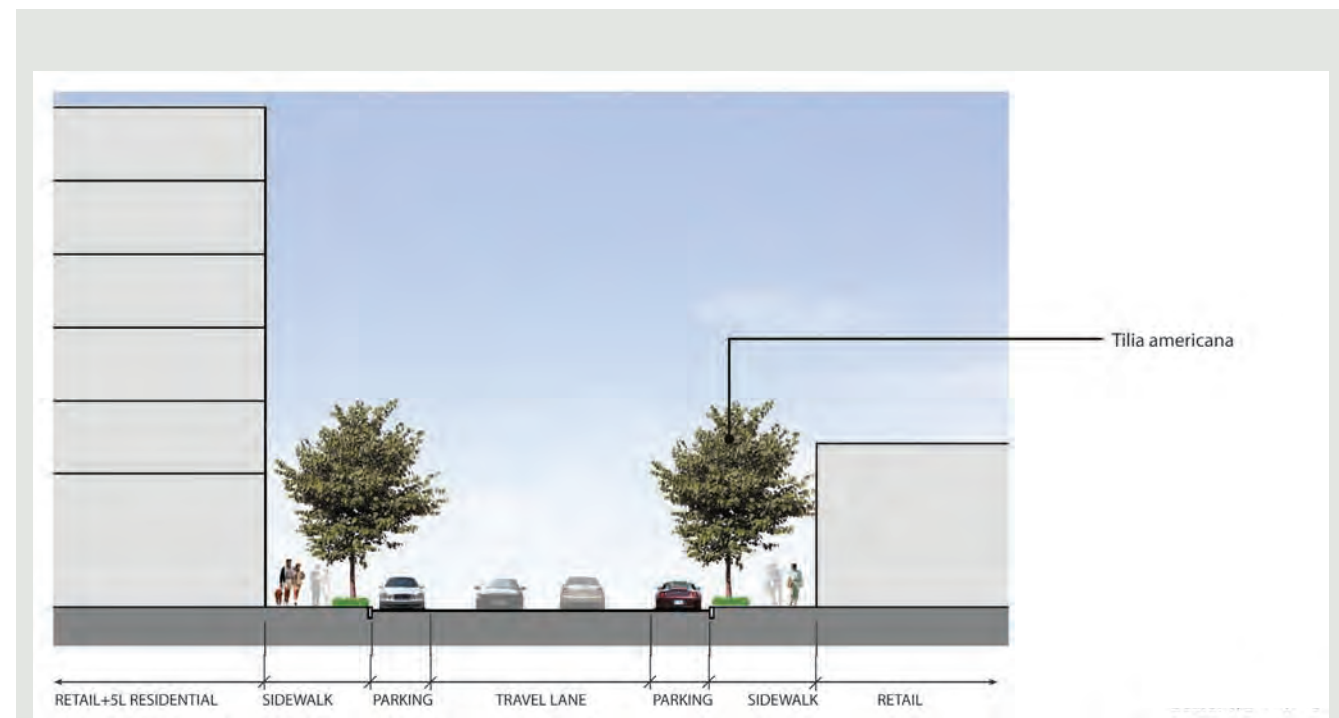
*Acer rubrum* 'October Glory' / Red Maple *Carpinus caroliniana* / American Hornbeam *Tilia americana* / American Linden



Segment Location



Plan



Cross Section



## 5.7 Street Landscape - North / South Main Street



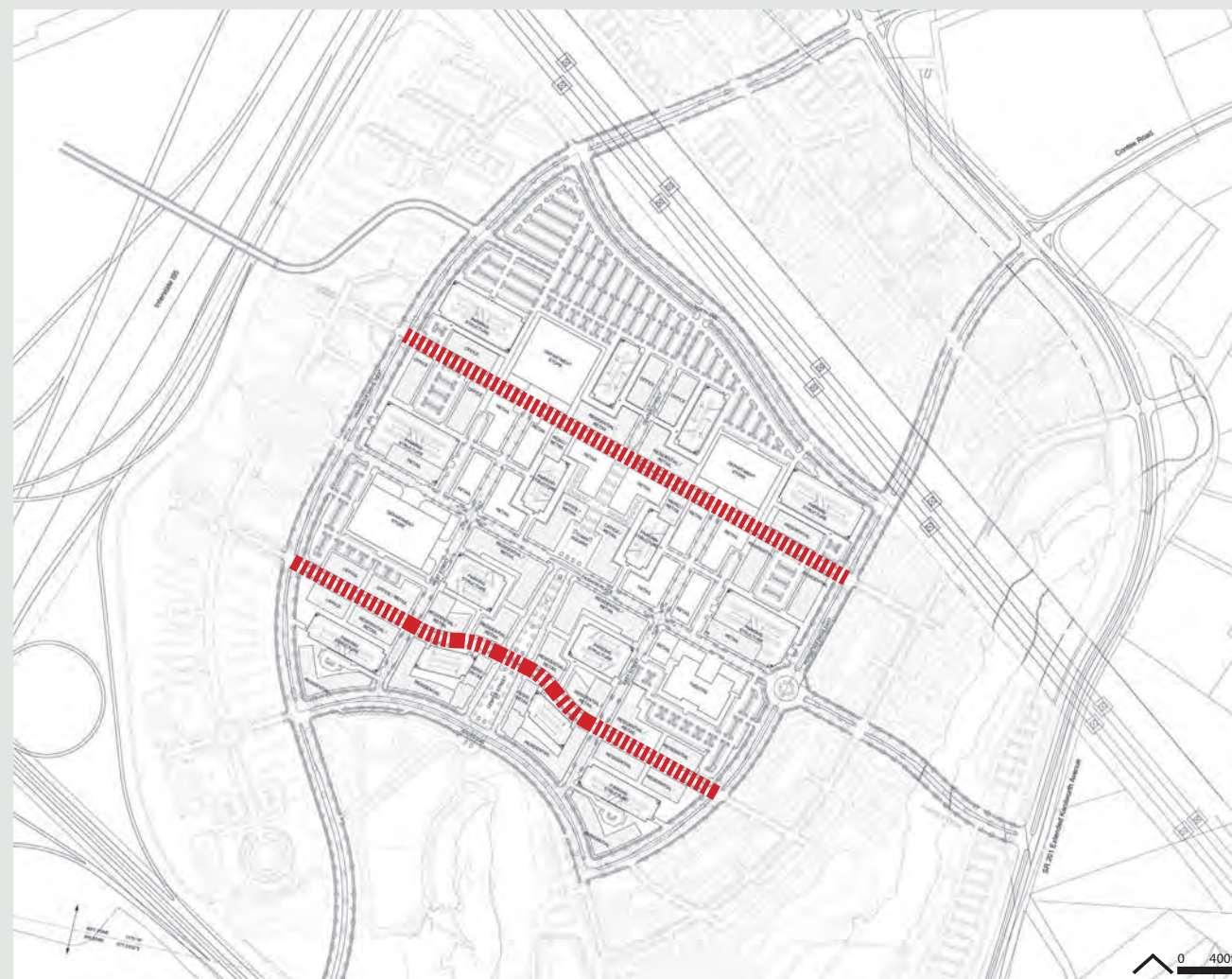
*Ulmus parvifolia 'Athena' /*  
Athena Chinese Elm



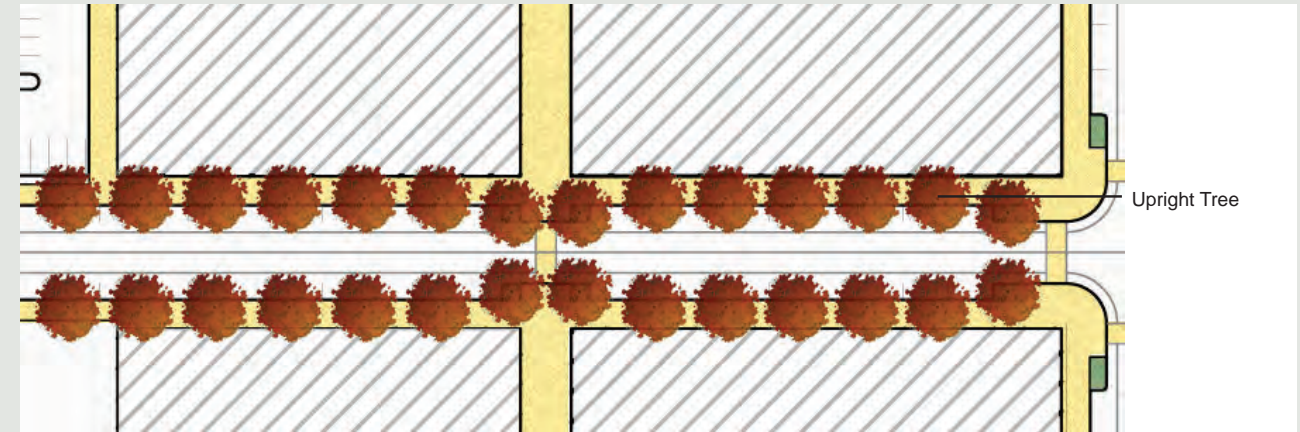
*Zelkova serrata 'Green Vase' /*  
Green Vase Zelkova



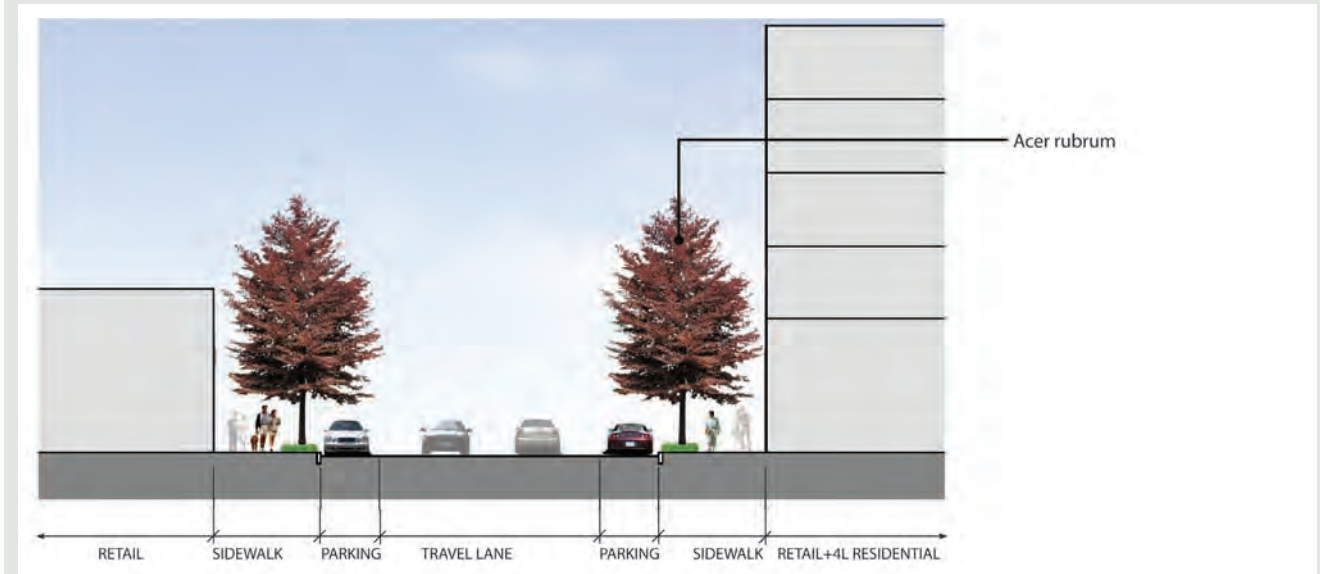
*Acer rubrum /* Red Maple



Segment Location



Plan



Cross Section



## 5.8 Plaza Landscape - Center Street - Formal Option Overall Illustrative



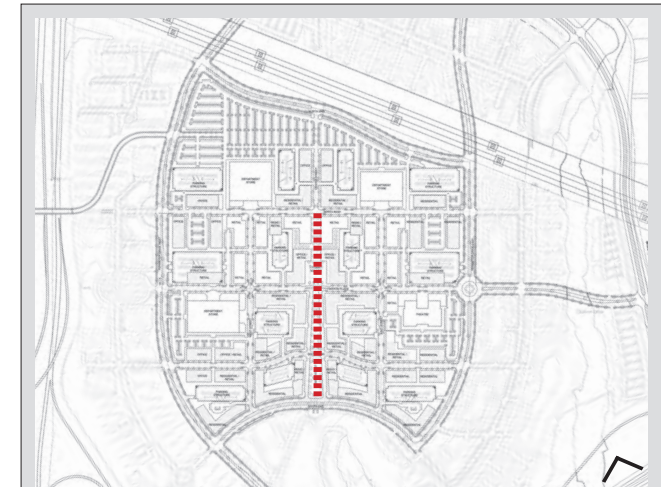
Open Space Character



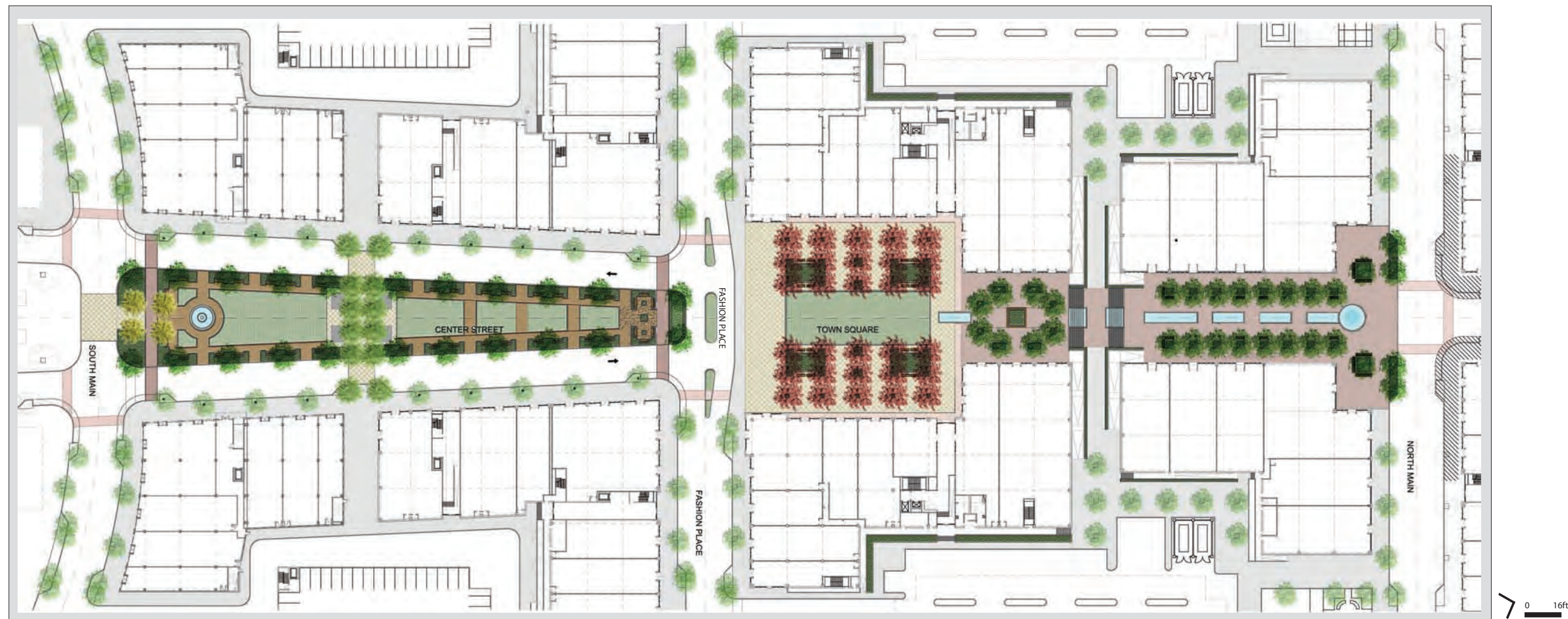
Turf Plaza



Accent Tree Bosque



Location Plan



Plan



## 5.8 Plaza Landscape - Center Street - Informal Option Overall Illustrative



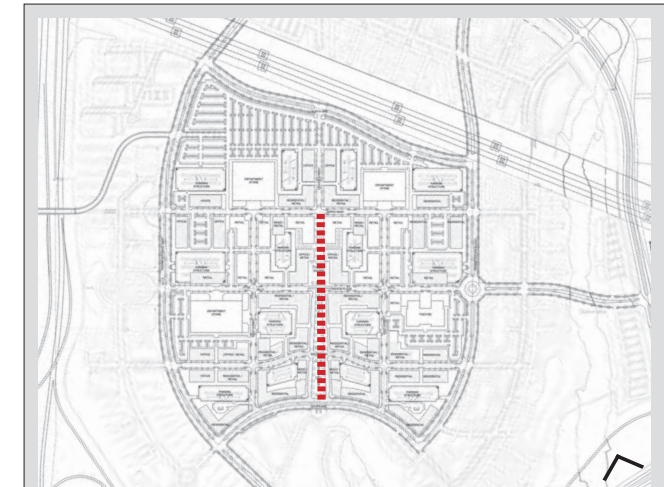
Plaza Character



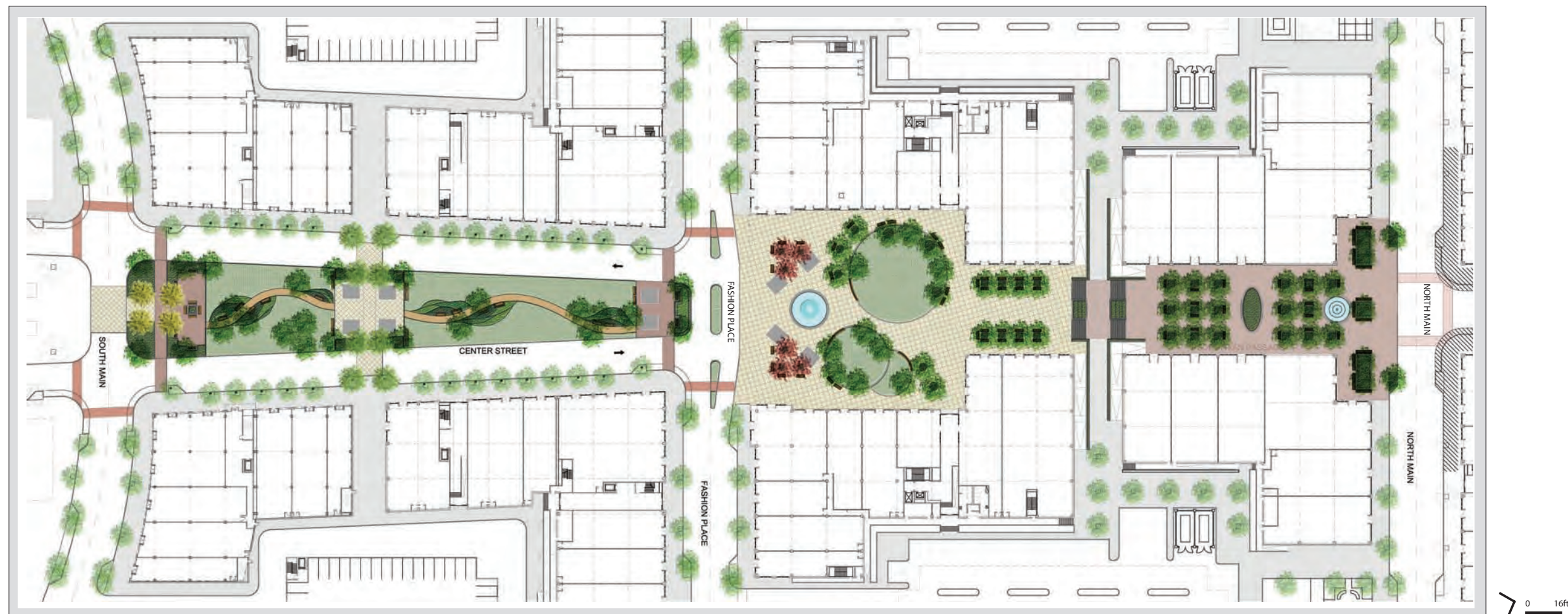
Turf with Seatwalls



Open Space Character




Location Plan




Plan



### 5.8.1 Plaza Landscape - The Promenade Formal Option Hardscape



Open Space Paths




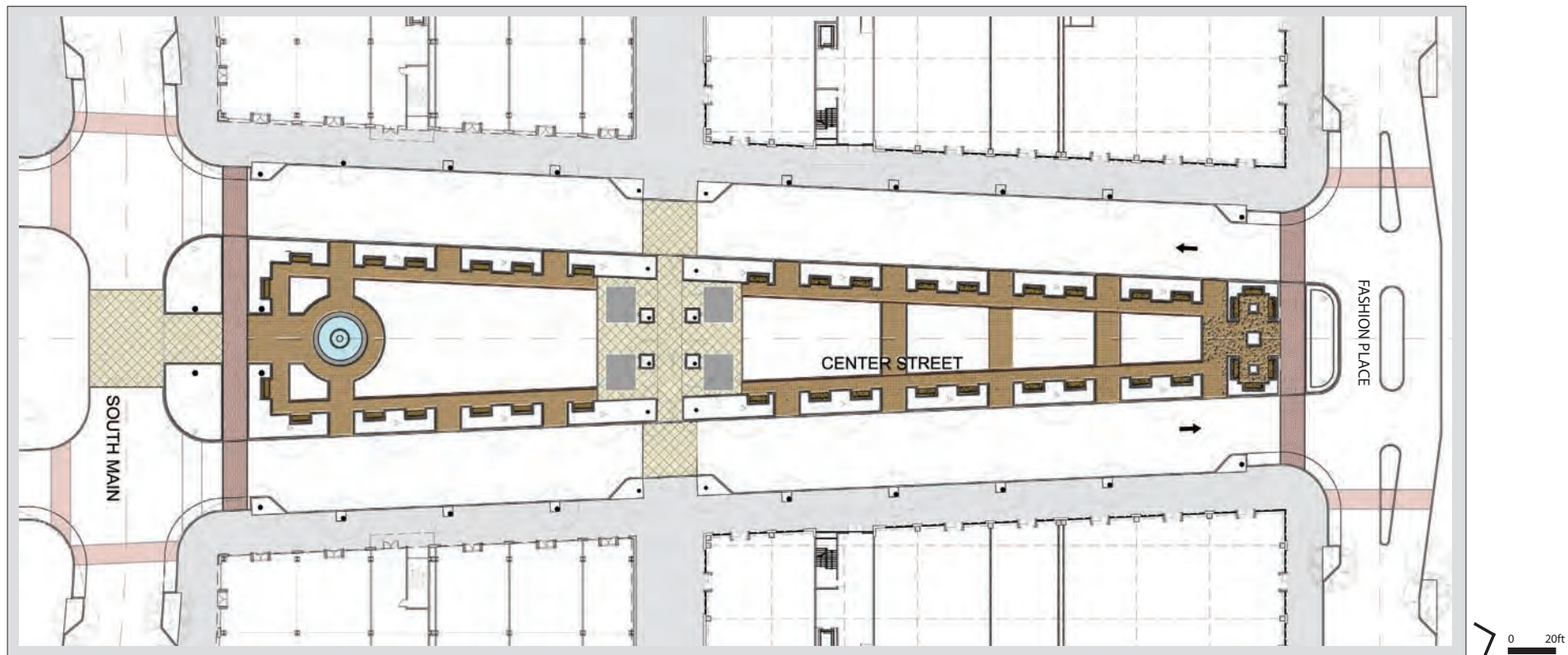
Open Space Turf Panels

**Hardscape Character**

The hardscape in the Promenade combines all paving type materials such as bricks, stone pavers, precast unit pavers and enhanced concrete. Decomposed granite or stone paths provide the main circulation around the perimeter of the space and create connections through the turf areas. A paved plaza in the center of the space allows for kiosks or other programming elements. Crosswalks are enhanced to highlight the pedestrian crossings from the sidewalks into this central space.

**Site Furniture**

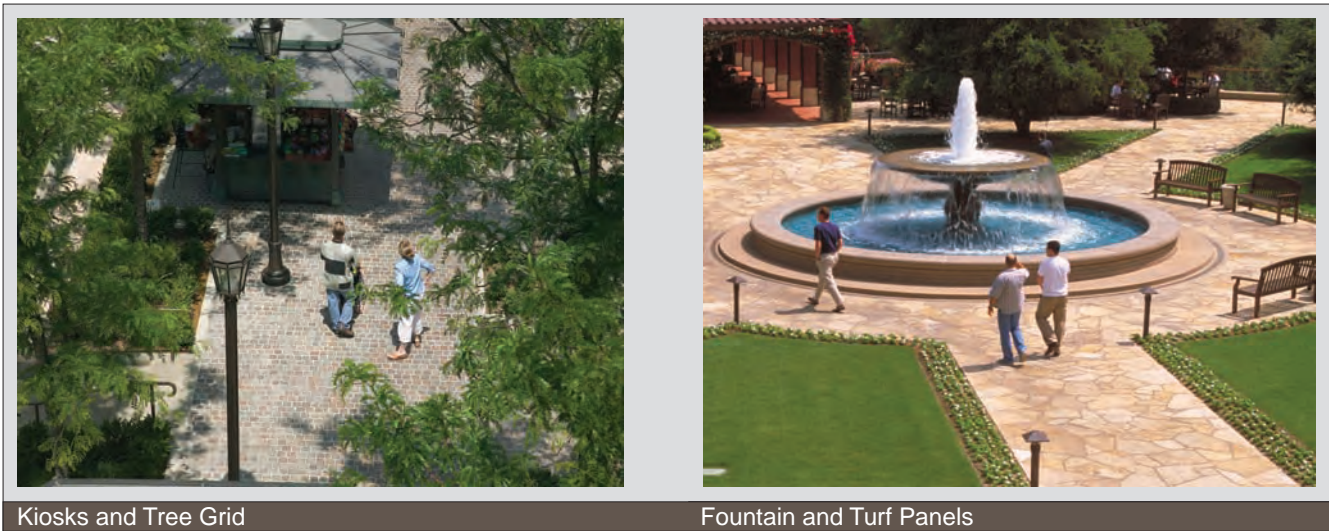




Plan



### 5.8.1 Plaza Landscape - The Promenade Formal Option Planting



**Planting Character**

In a formal planting approach, the Promenade trees may be placed around the perimeter to serve as an edge to the open gathering space in the core. Allee's of accent trees define key pedestrian paths and gathering areas. Water features, specialty planter seating and kiosks may occur at focal points within the plan. Turf panels with pedestrian crossings comprise the central space providing a myriad of programming opportunities to occur.

**Canopy Tree Options**

- <i>Acer buergeranum</i> /Trident Maple	
- <i>Carpinus caroliniana</i> /American Hornbeam	
- <i>Lagerstroemia indica</i> 'Natchez'/Natchez Crape	Myrtle
- <i>Liriodendron tulipifera</i> /Tulip Tree	
- <i>Tilia cordata</i> 'Greenspire'/Greenspire Littleleaf	Linden
- <i>Ulmus parvifolia</i> 'Athena'/Athena Chinese Elm	
- <i>Zelkova serrata</i> 'Green Vase'/Green Vase Zelkova	

**Shrub Options**

- <i>Abelia</i> 'Edward Goucher'/Edward Goucher Abelia	
- <i>Azalea</i> x <i>Conlee</i> /Autumn Amethyst Azalea	
- <i>Azalea</i> x <i>Conleb</i> /Autumn Embers Azalea	
- <i>Azalea</i> x <i>Roblem</i> /Autumn Starlite Azalea	
- <i>Azalea</i> x <i>Conlep</i> /Autumn Twist Azalea	
- <i>Berberis thunbergii</i> /Cromson Pygmy Barberry	
- <i>Cornus sericea</i> 'Cardinal'/Redtwig Redosier	Dogwood
- <i>Euonymus kiautschovicus</i> /Manhattan Euonymus	
- <i>Forsythia intermedia</i> /Forsythia	
- <i>Prunus laurocerasus</i> 'Otto luyken'/Otto Luyken	Laurel
- <i>Taxus baccata</i> 'Repandens'/Repandens Yew	
- <i>Viburnum carlesii</i> /Korean Spice Viburnum	
- <i>Taxus baccata</i> 'Repandens'/Repandens Yew	
- <i>Viburnum plicatum</i> 'Tomentosum'/Doublefile	Virburnum

*List represents a selection of plants, others may be considered.*



Plan



## 5.8.1 Plaza Landscape - The Promenade Informal Option Hardscape



Pathway Paving Character

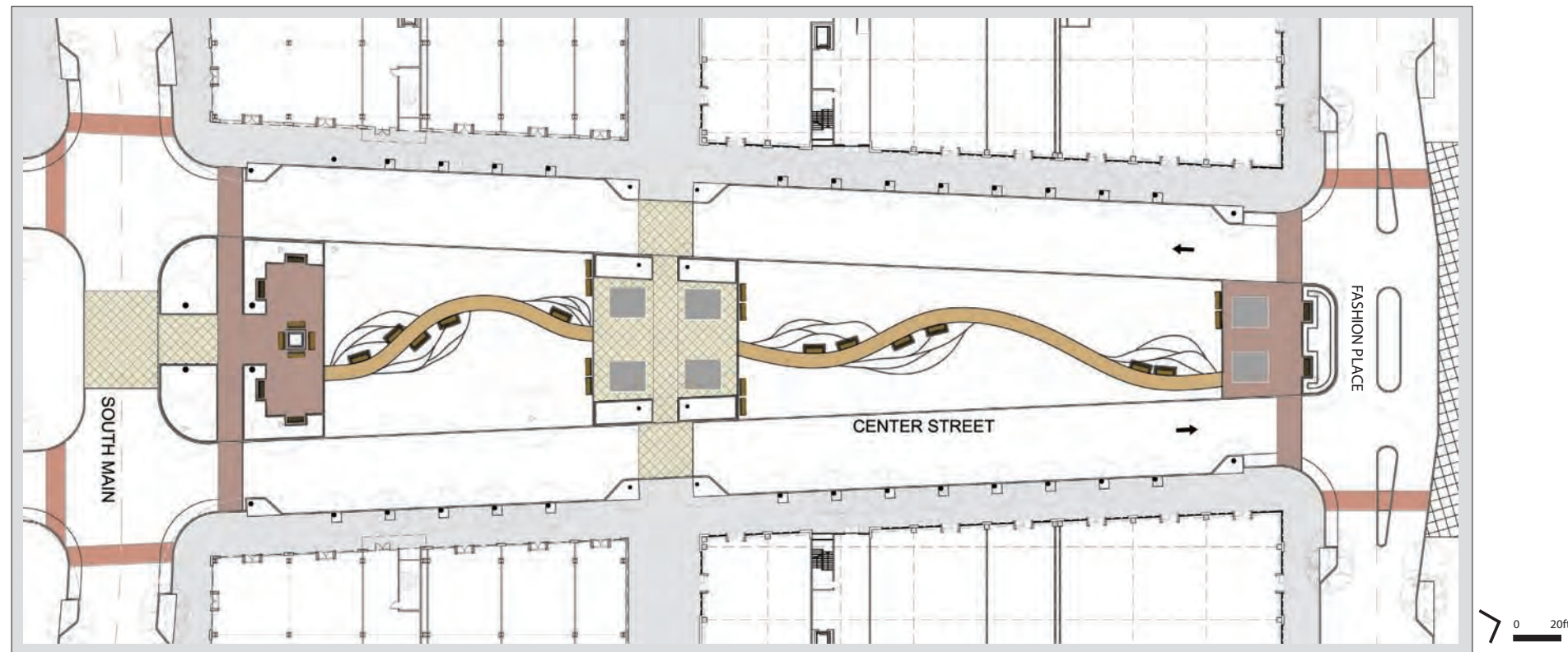


Kiosks with Accent Paving

### Hardscape Character

Informal paths of decomposed granite or stone provide a pedestrian pathway through the greenspace, linking the plaza spaces. The plazas may be paved with brick, stone, precast unit pavers, and/or enhanced concrete that occur throughout the site. The paved plazas throughout the Promenade provide programming opportunities for gathering. Enhanced pavers highlight crosswalks for better pedestrian navigation to and from this center space.

### Decomposed Granite Paths



Plan



# 5.8.1 Plaza Landscape - The Promenade Informal Option Planting



### Planting Character

Meandering pedestrian pathways through the Promenade greenspace create an informal planting scheme. This dominant turf space with accent planting and tree clusters allows for both passive and active events. Accent planting occurs around seating and gathering areas along the main path. Small plaza spaces, framed with canopy trees, anchor the greenspace with kiosk and water feature opportunities.

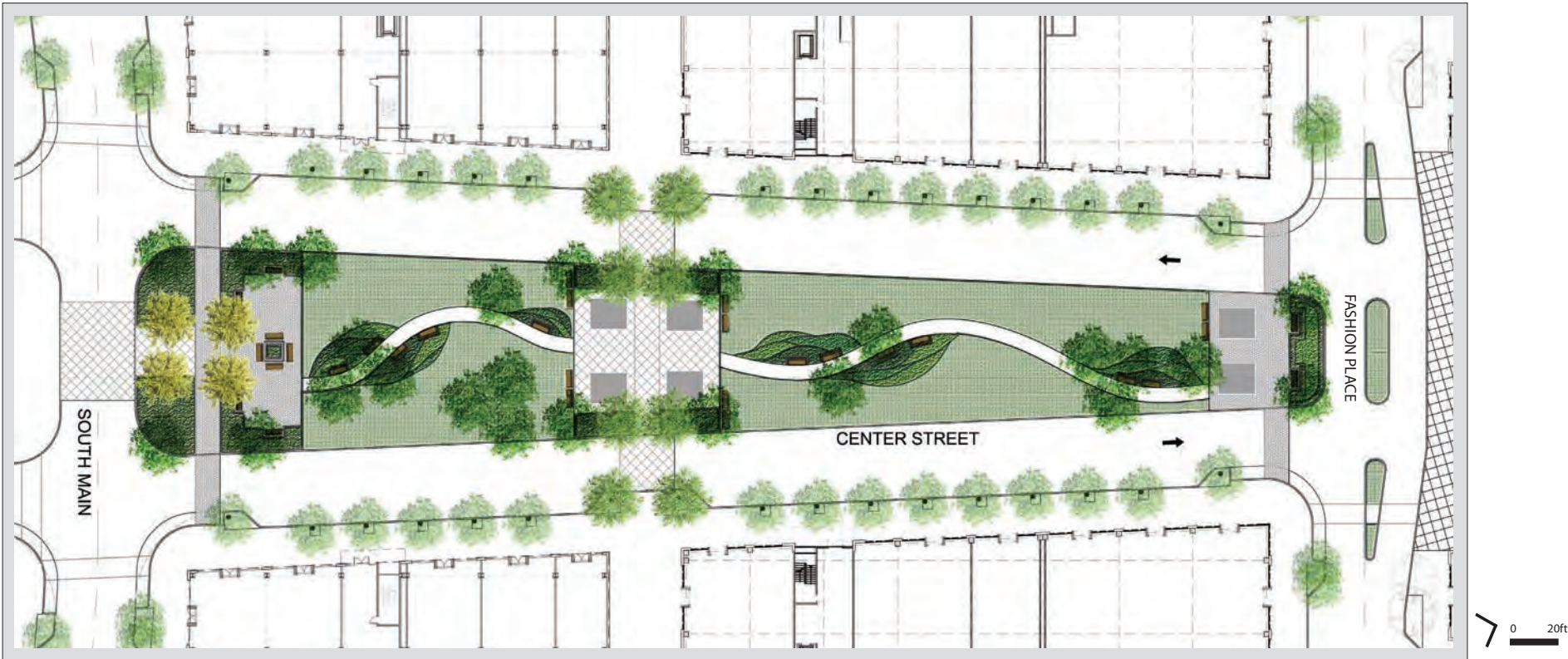
### Canopy Tree Options

- <i>Acer buergeranum</i> /Trident Maple	
- <i>Carpinus caroliniana</i> /American Hornbeam	
- <i>Lagerstroemia indica</i> 'Natchez'/Natchez Crape	Myrtle
- <i>Liriodendron tulipifera</i> /Tulip Tree	
- <i>Tilia cordata</i> 'Greenspire'/Greenspire Littleleaf	Linden
- <i>Ulmus parvifolia</i> 'Athena'/Athena Chinese Elm	
- <i>Zelkova serrata</i> 'Green Vase'/Green Vase Zelkova	

### Shrub Options

- <i>Abelia</i> 'Edward Goucher'/Edward Goucher Abelia	
- <i>Azalea</i> x <i>Conlee</i> /Autumn Amethyst Azalea	
- <i>Azalea</i> x <i>Conleb</i> /Autumn Embers Azalea	
- <i>Azalea</i> x <i>Roblem</i> /Autumn Starlite Azalea	
- <i>Azalea</i> x <i>Conlep</i> /Autumn Twist Azalea	
- <i>Berberis thunbergii</i> /Cromson Pygmy Barberry	
- <i>Cornus sericea</i> 'Cardinal'/Redtwig Redosier	Dogwood
- <i>Euonymus kiautschovicus</i> /Manhattan Euonymus	
- <i>Forsythia intermedia</i> /Forsythia	
- <i>Prunus laurocerasus</i> 'Otto luyken'/Otto Luyken	Laurel
- <i>Taxus baccata</i> 'Repandens'/Repandens Yew	
- <i>Viburnum carlesii</i> /Korean Spice Viburnum	
- <i>Taxus baccata</i> 'Repandens'/Repandens Yew	
- <i>Viburnum plicatum</i> 'Tomentosum'/Doublefile	Virburnum

List represents a selection of plants, others may be considered.



Plan



# 5.8.2 Plaza Landscape - Town Square Formal Option Hardscape and Planting



Stepped Fountain



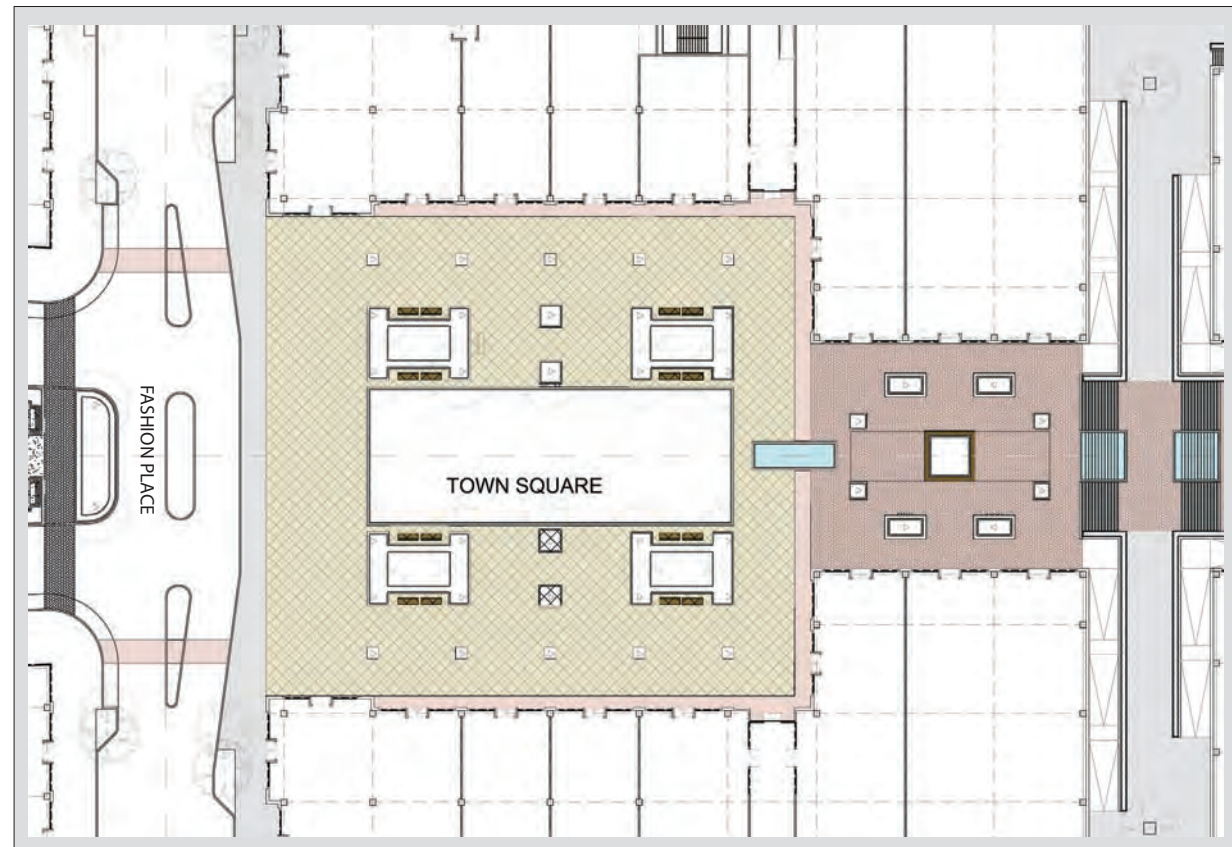
Plaza Character

## Hardscape and Planting Character

The Town Square is defined by accent tree bosques and tree canopy groupings. Gathering locations and specialty seating are emphasized with raised planters and/or specialty groundplane planting. At the center of Town Square, a turf zone allows for a variety of programming options. Brick, stone and/or precast unit pavers create patterns on the primary groundplane and enhance the pedestrian experience. Water features create focal points in the square and stepping fountains link transition spaces where grade changes occur.

See Promenade Landscape sheet for a selection of planting materials

## Site Furniture



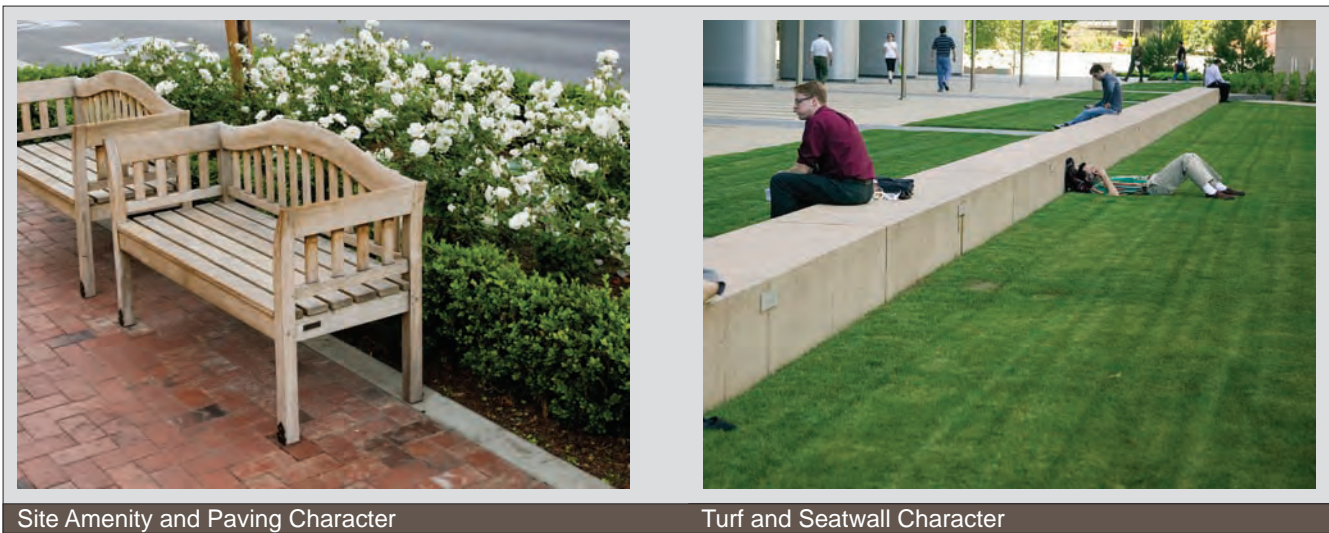
Hardscape Plan



Planting Plan



### 5.8.2 Plaza Landscape - Town Square Informal Option Hardscape and Planting

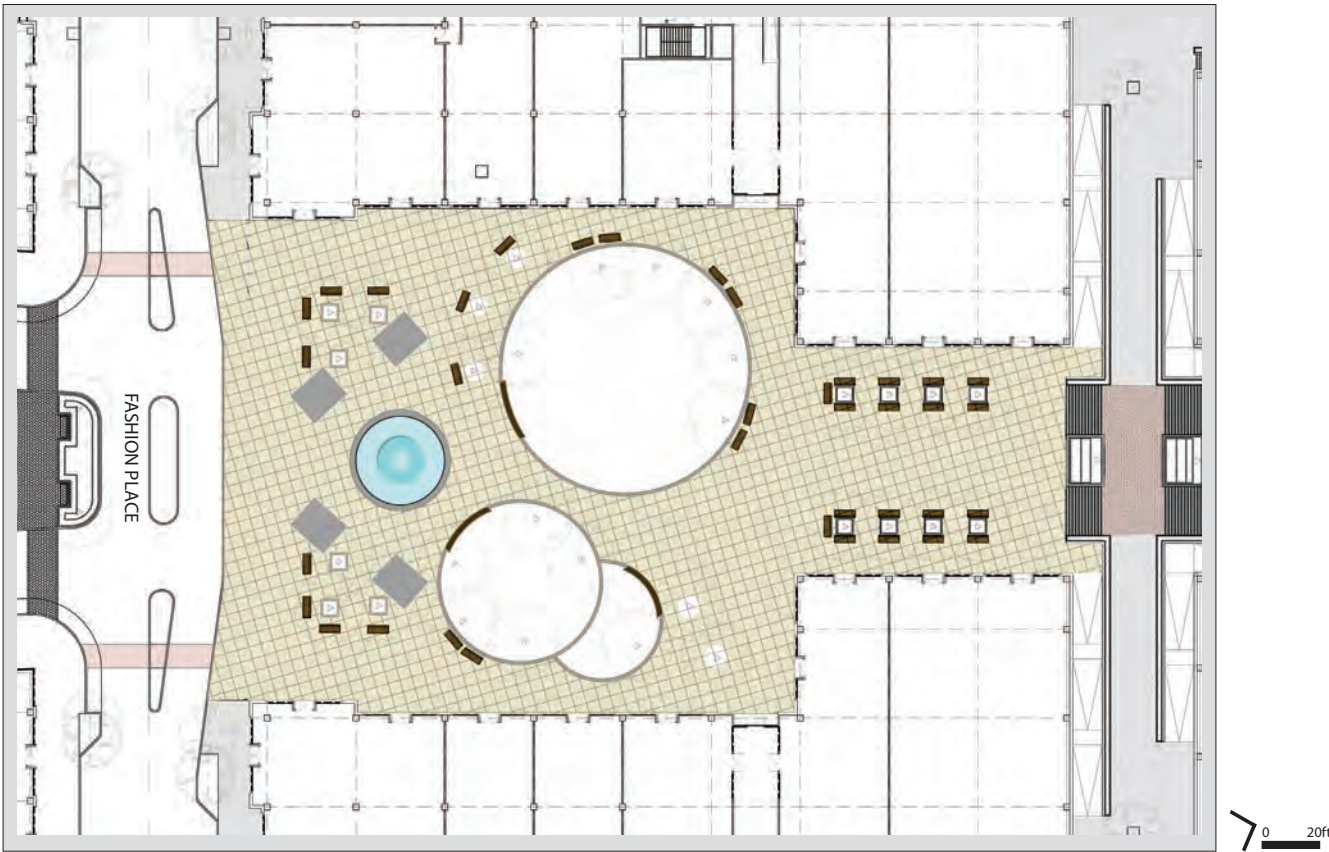


#### Hardscape and Planting Character

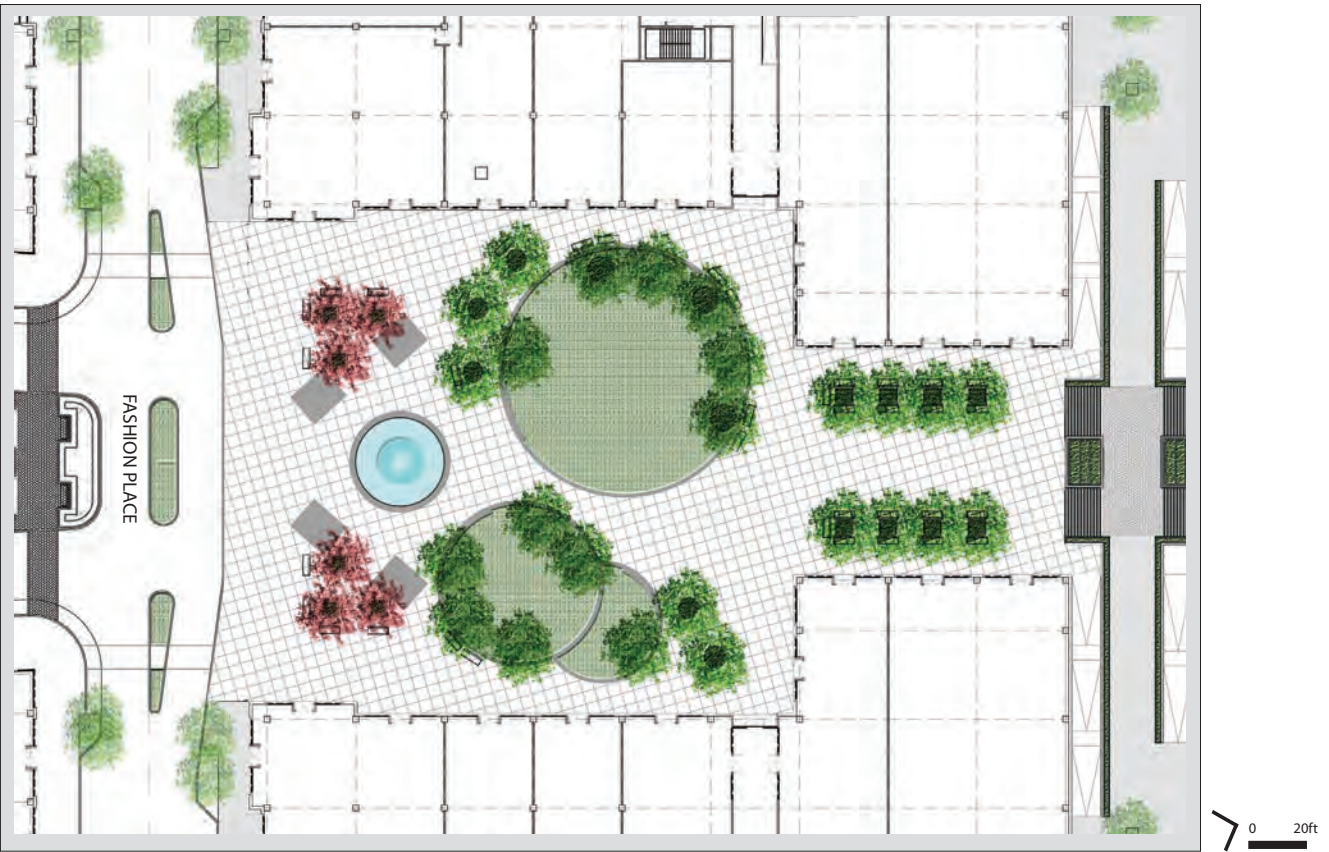
Casual groups of canopies and accent trees loosely frame the Town Square. Both clustered and individual turf panels are framed by segmented seatwalls, creating gathering opportunities and event programming. A prominent water feature serves as a focal point in Town Square, and can be seen from the road. Plaza paving materials may include brick, stone and/or precast unit pavers. Opportunities for kiosks occur at the plaza entry.

See Promenade Landscape sheet for a selection of planting materials

#### Site Furniture



Hardscape Plan



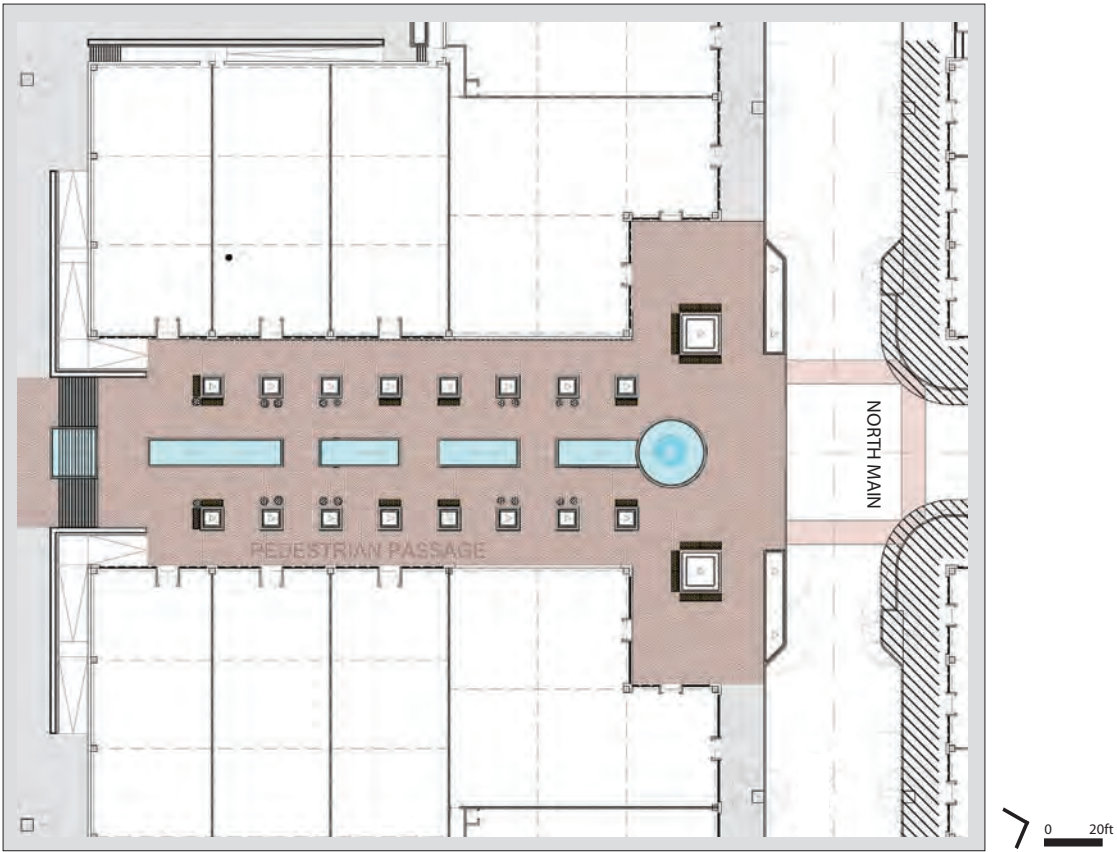
Planting Plan



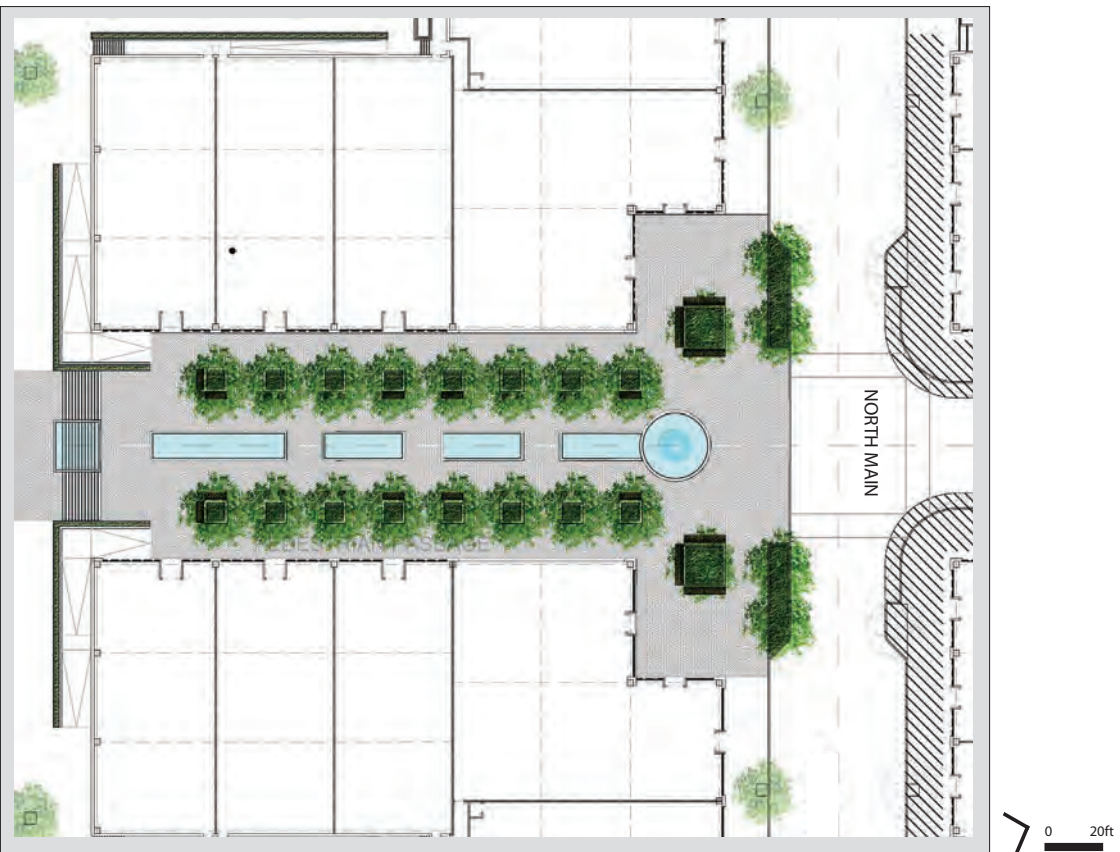
### 5.8.3 Plaza Landscape - Pedestrian Passage Formal Option Hardscape and Planting



Hardscape and Planting Character	Site Furniture
<p>A wide allee of canopy trees with small groundplane planters and/or tree grates frame the Pedestrian Passage. Centered on the allee is a series of water elements that terminate on a central water feature. Breaks in planting and water elements allows for pedestrian circulation around and through the passage. Paving is comprised of brick, stone, and/or precast unit pavers, which links this space to the Town Square. Seating and site amenities occur along the central allee spine and around accent raised planters at the plaza opening.</p> <p>See Promenade Landscape sheet for a selection of planting materials</p>	



Hardscape Plan



Planting Plan



### 5.8.3 Plaza Landscape - Pedestrian Passage Informal Option Hardscape and Planting



Site Amenity and Paving Character



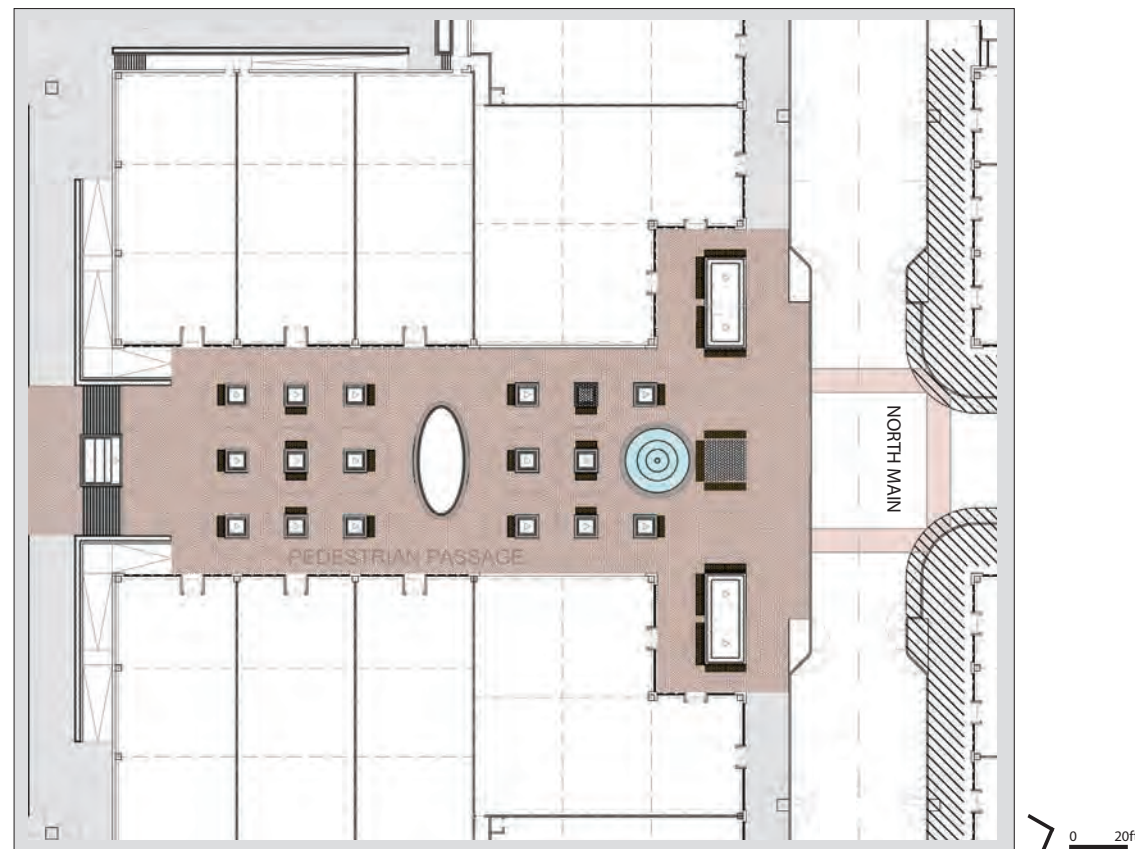
Tree Bosque

#### Hardscape and Planting Character

Tree bosques with groundplane planters and seating create small gathering areas throughout the pedestrian passage. The center of the passage allows for a focal point such as a raised planter with specialty planting. The placement of trees and planters allows for easy pedestrian circulation between shops. The Pedestrian Passage is comprised primarily of brick, stone, and/or precast unit pavers that link this space to the Town Square. A water feature at the plaza entry can draw pedestrians into the space.

See Promenade Landscape sheet for a selection of planting materials

#### Site Furniture



Hardscape Plan



Planting Plan



## 5.8.4 Plaza Landscape - The Promenade Formal Option Section



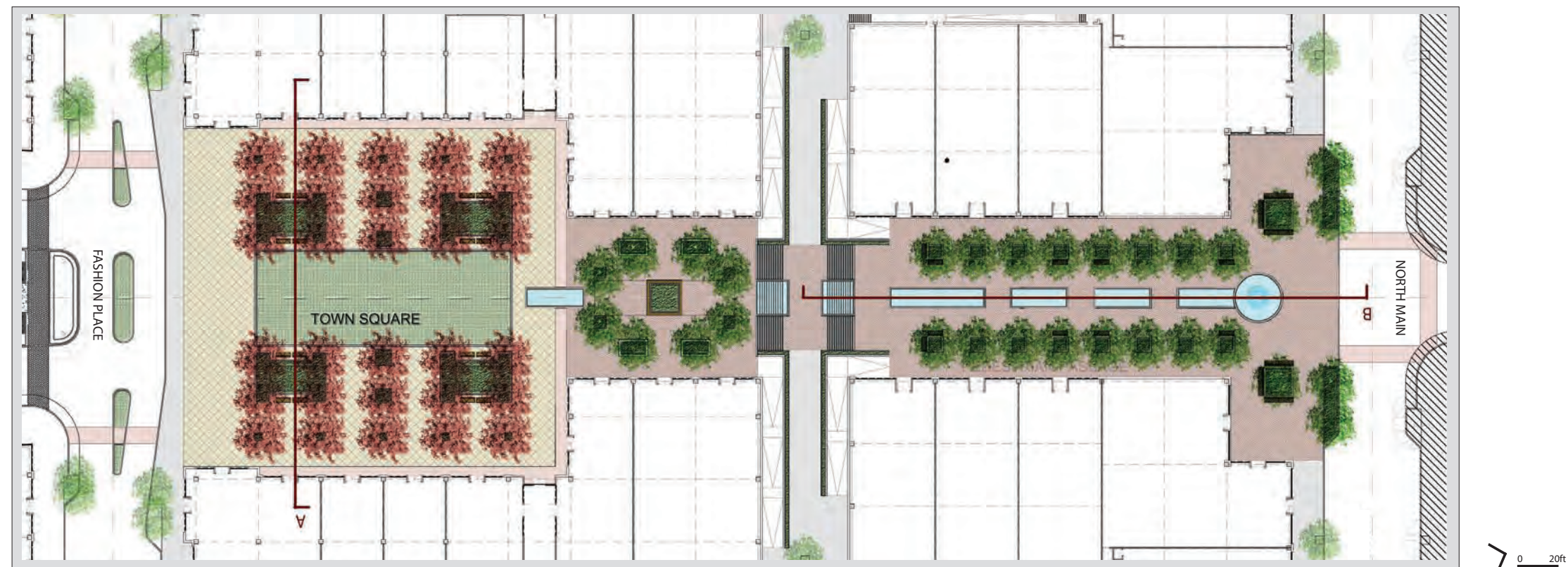
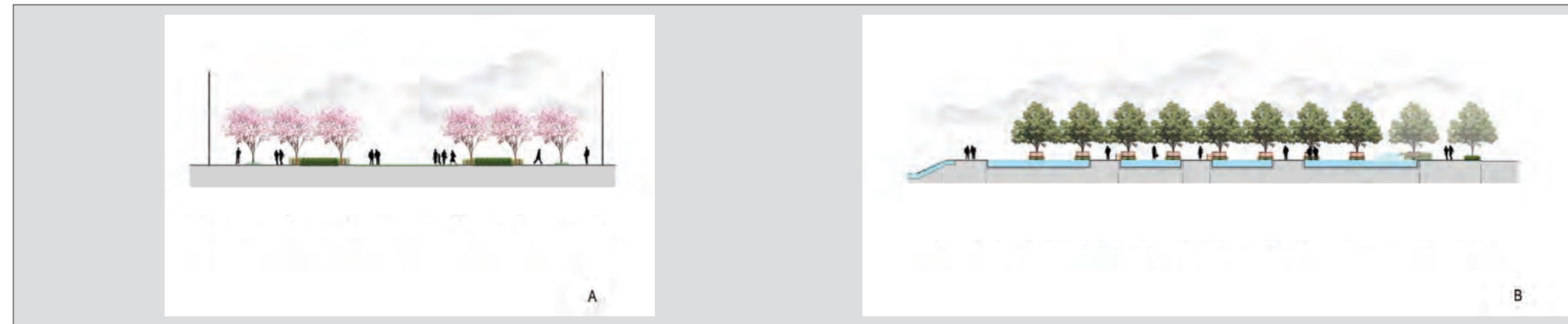


## 5.8.4 Plaza Landscape - The Promenade Informal Option Sections



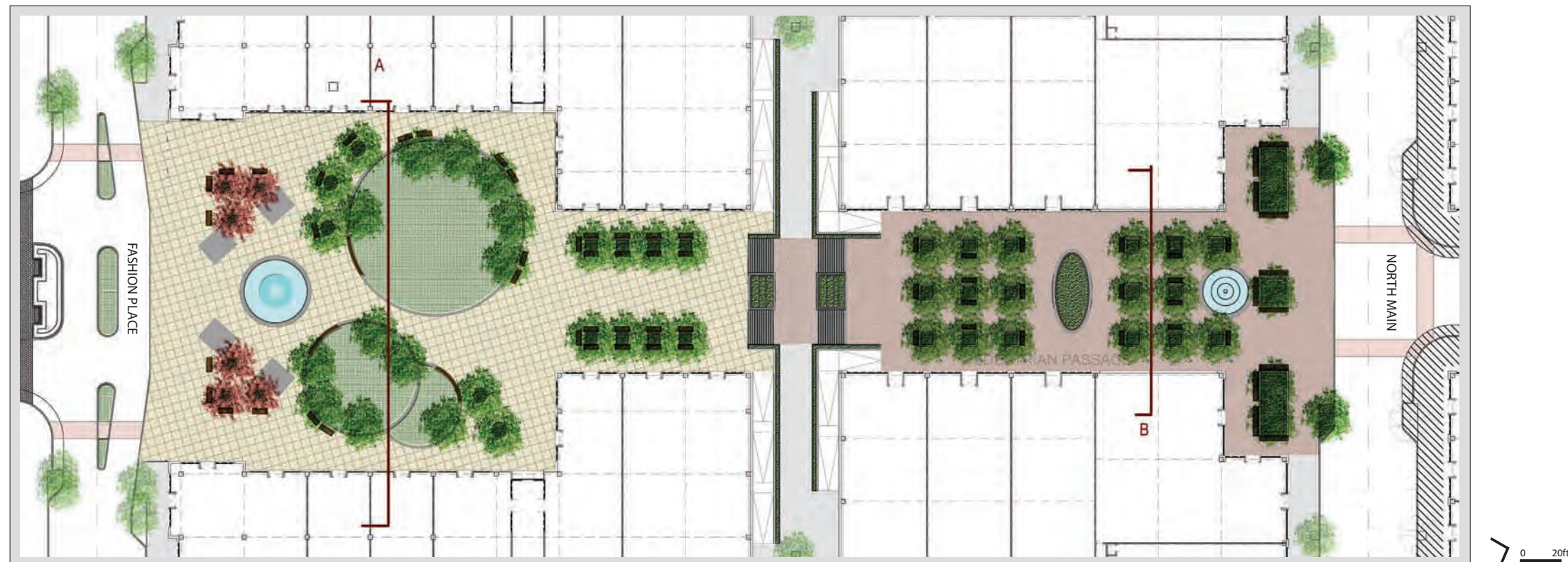
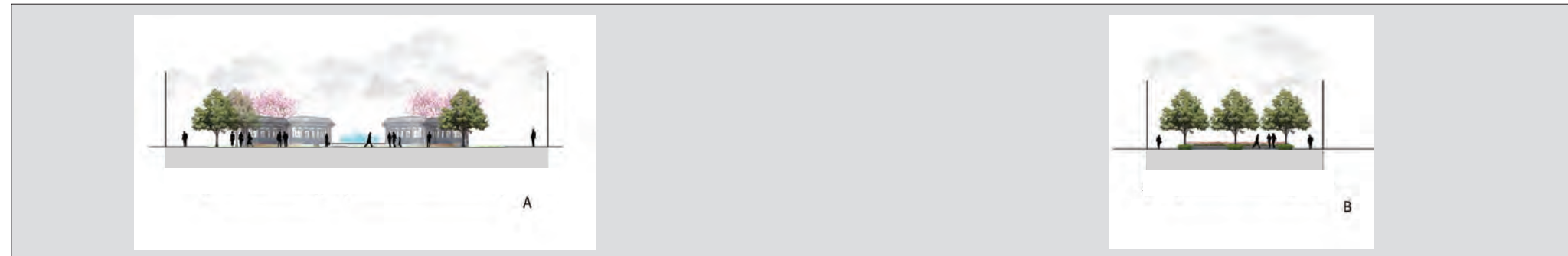


## 5.8.4 Plaza Landscape - Town Square and Pedestrian Passage Formal Option Sections





## 5.8.4 Plaza Landscape - Town Square and Pedestrian Passage Informal Option Sections

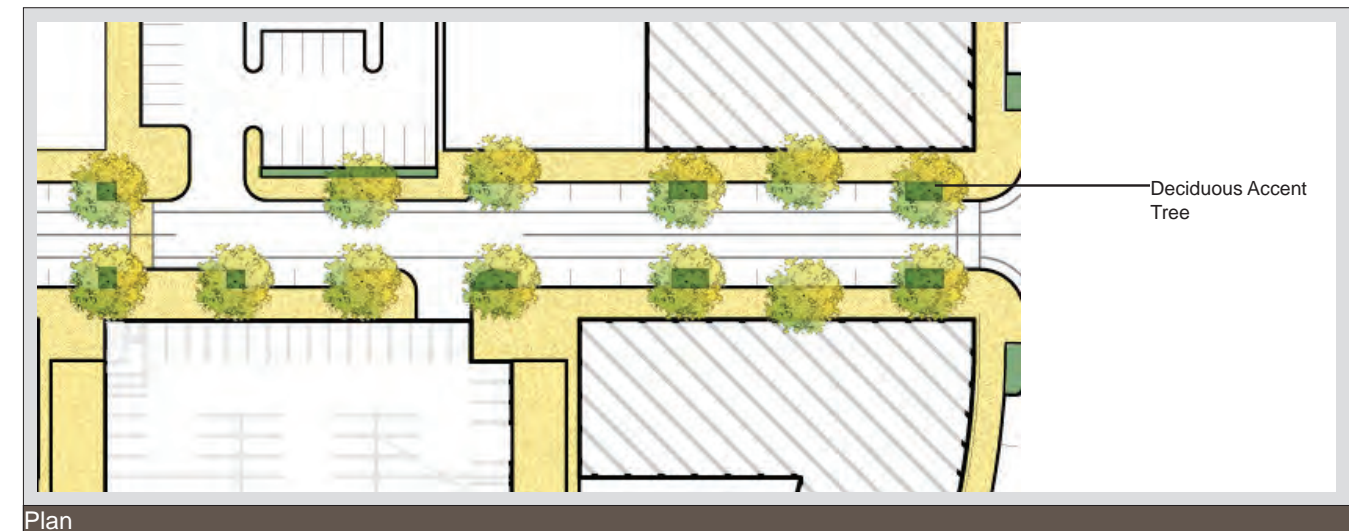




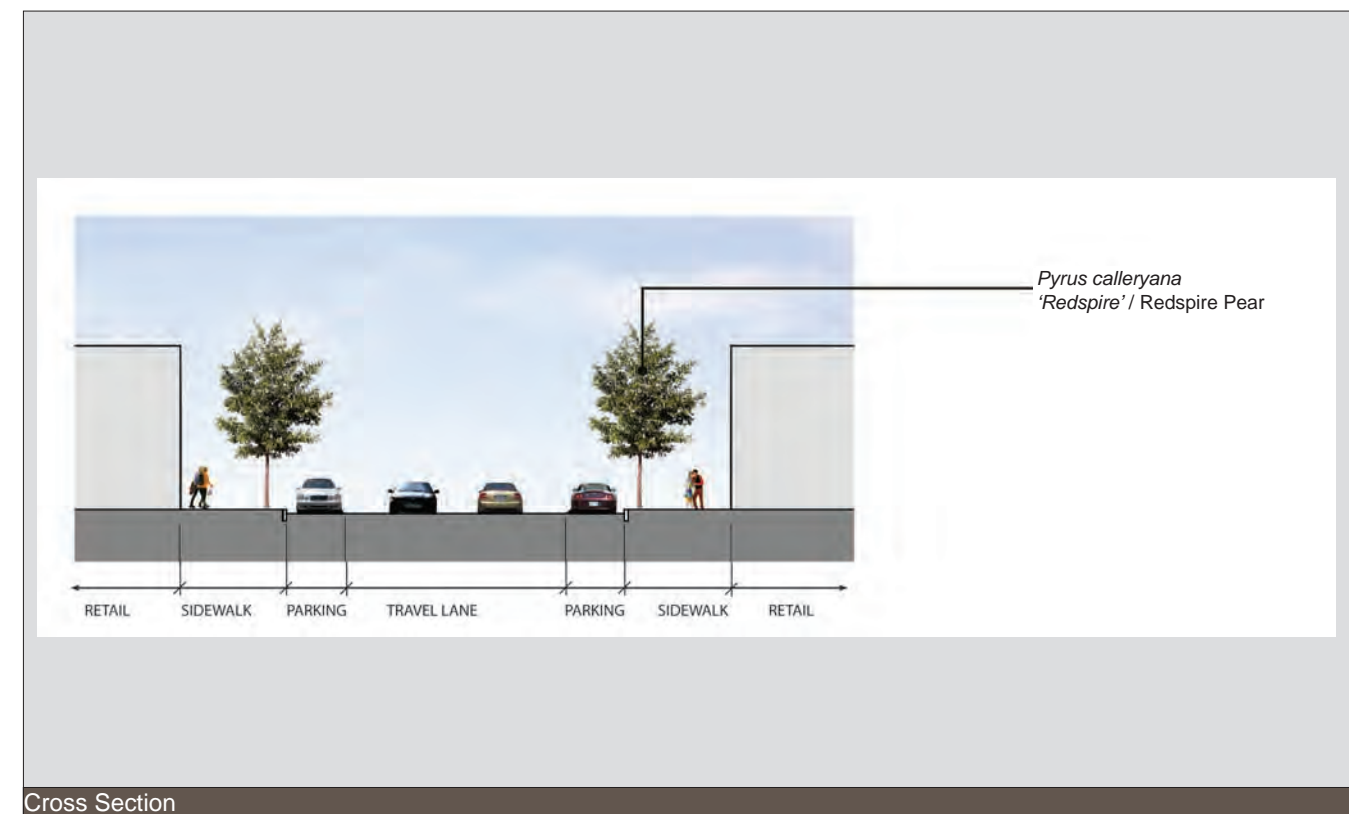
## 5.9 Street Landscape - East / West Street



Segment Location



Plan



Cross Section



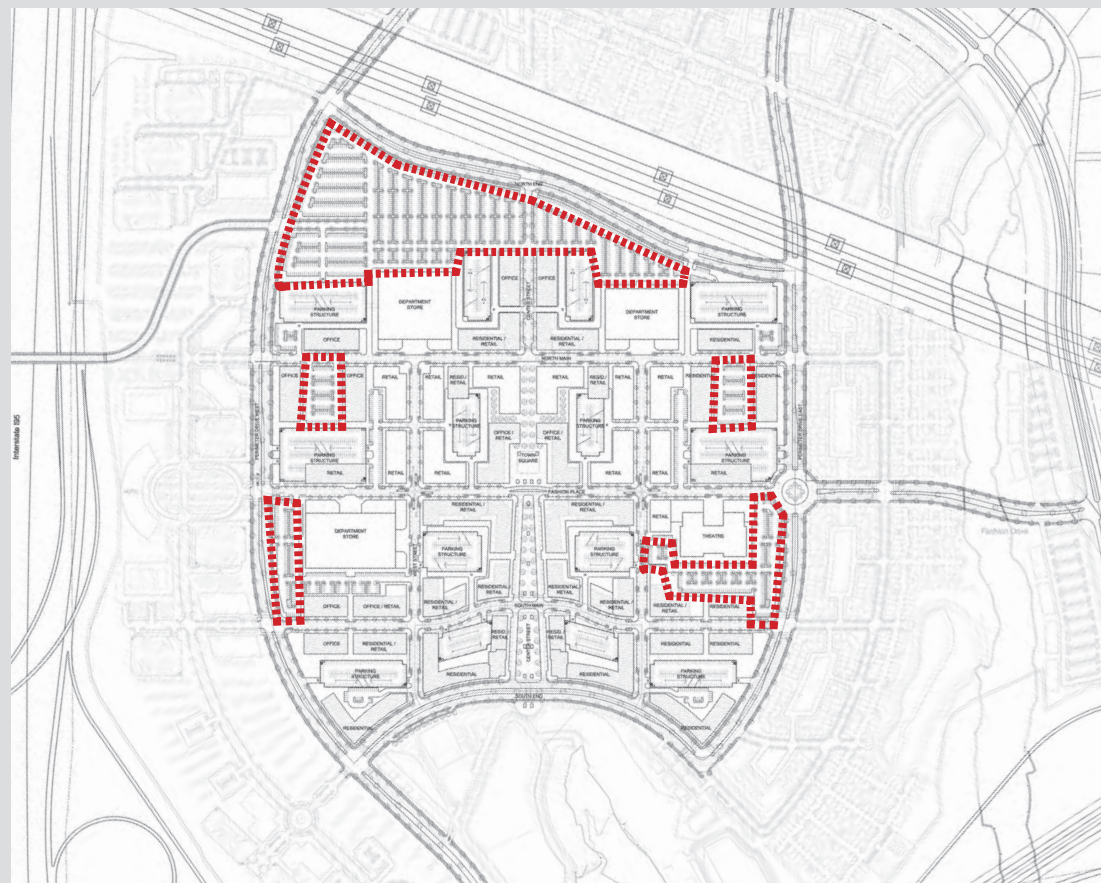
## 5.10 Street Landscape - Parking Lot



*Prunus x incamp* 'Okame' /  
Okame Cherry

*Taxodium distichum* /  
Bald Cypress

*Tilia tomentosa* / Silver Linden



Segment Location



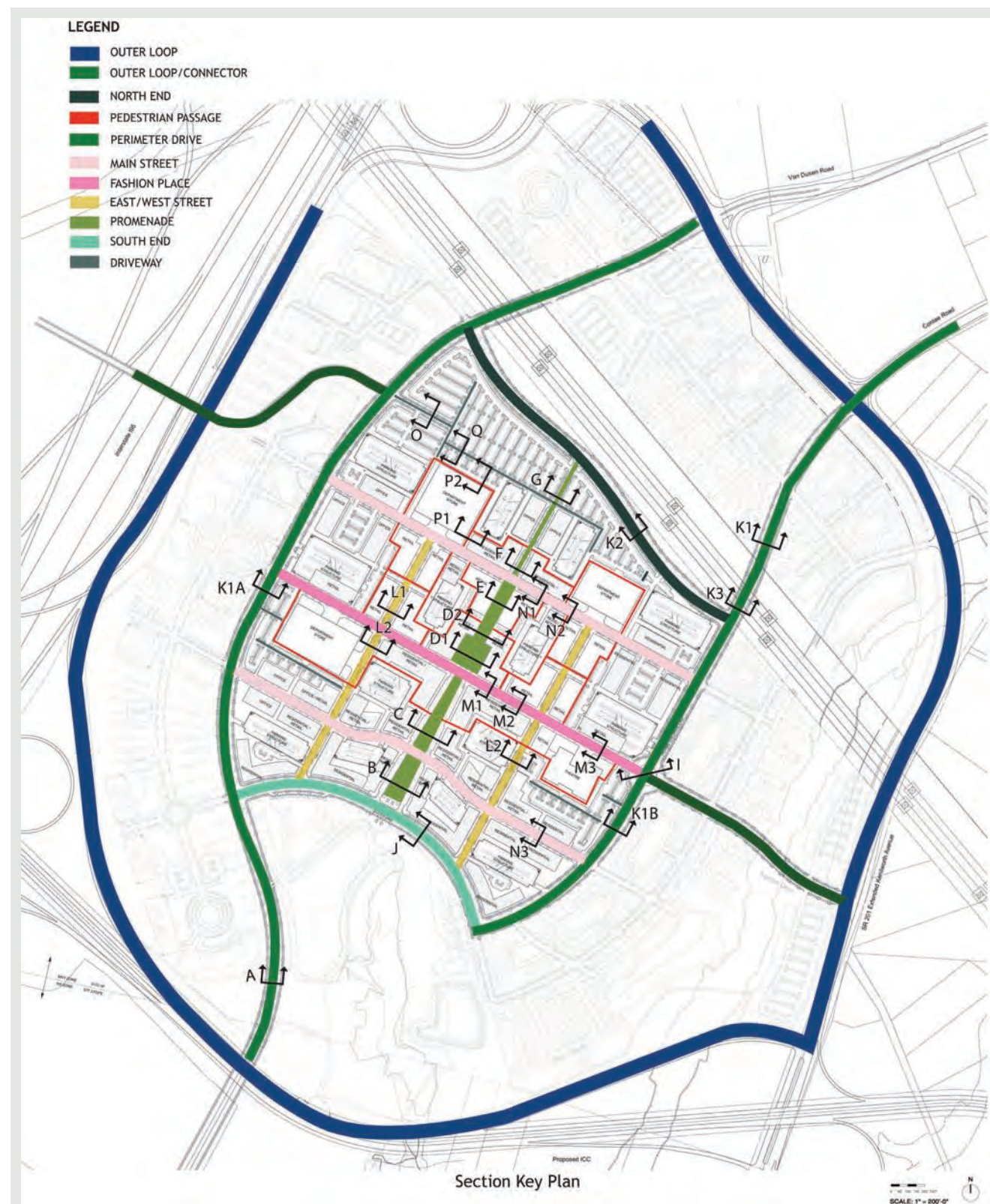
Pedestrian Pathway "Allee" style Character Images



Plan and Section of Pedestrian Pathway "Allee" style

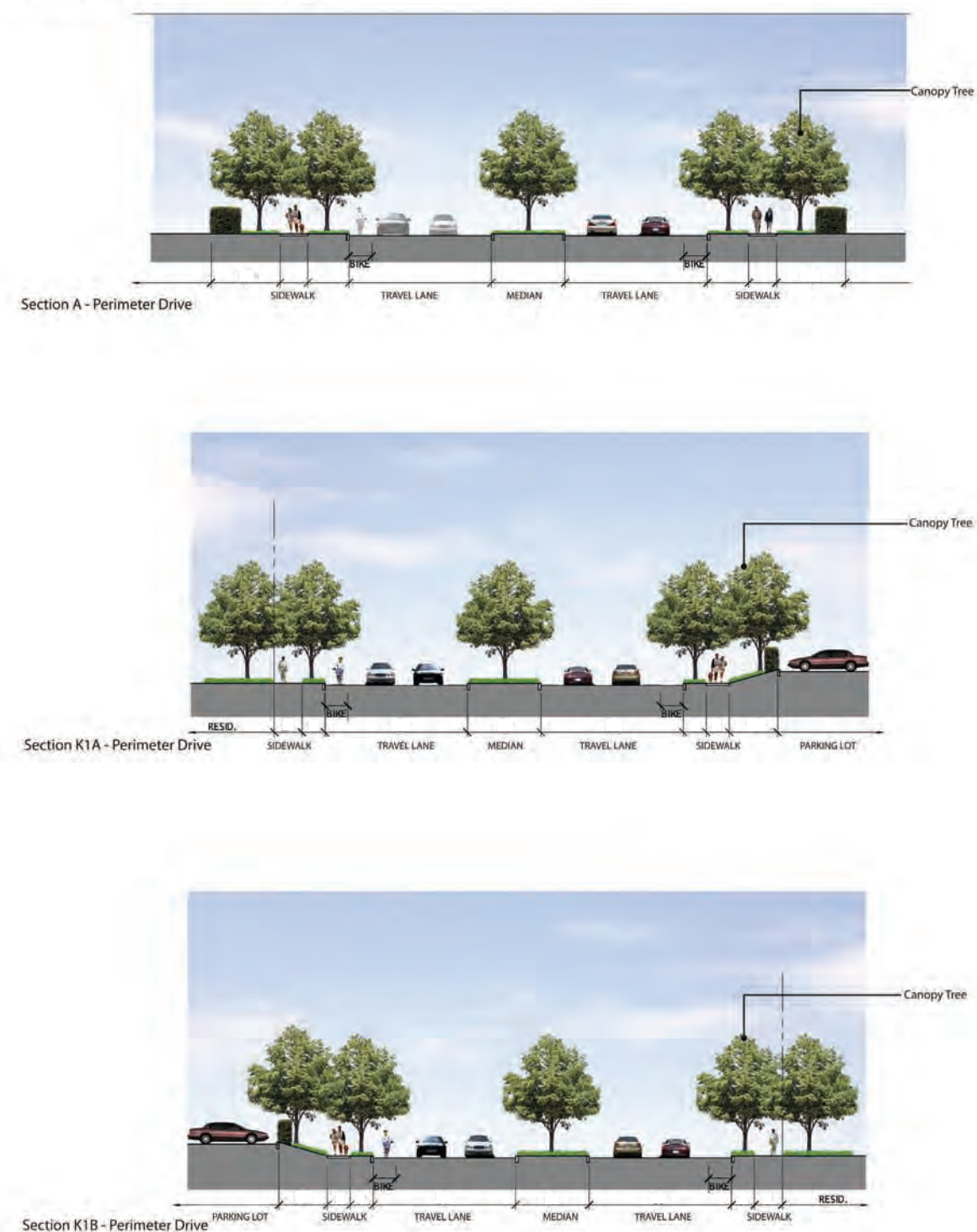


## 5.11 Street Cross Sections



Sections Key Plan

### PERIMETER DRIVE WEST AND PERIMETER DRIVE EAST Sections A, K1A, K1B and K3

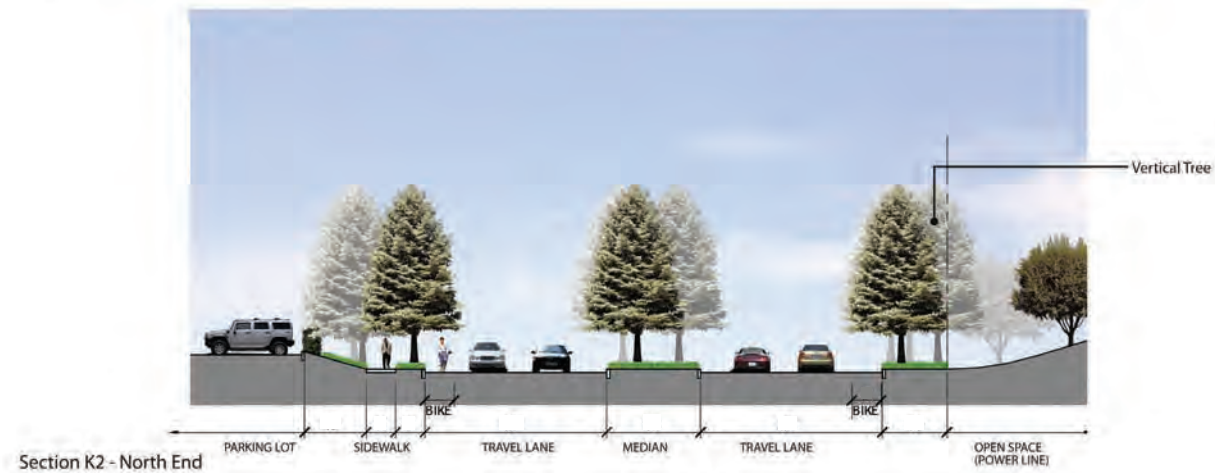


Sections



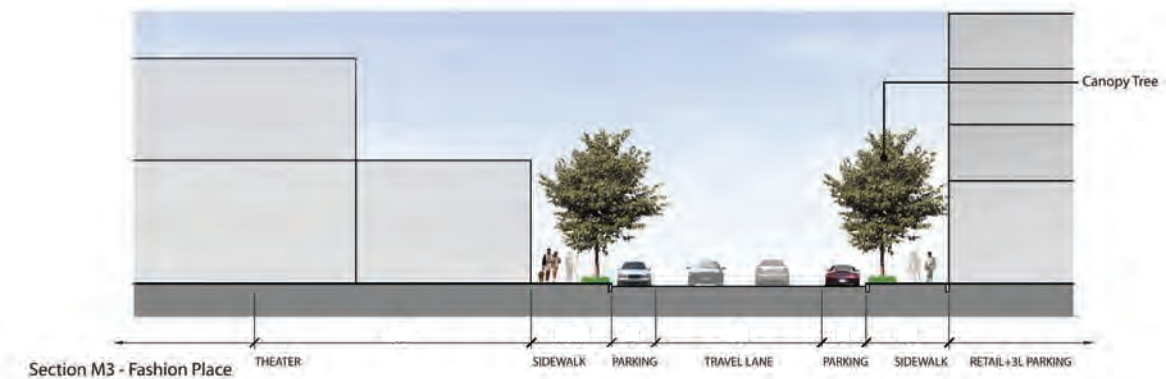
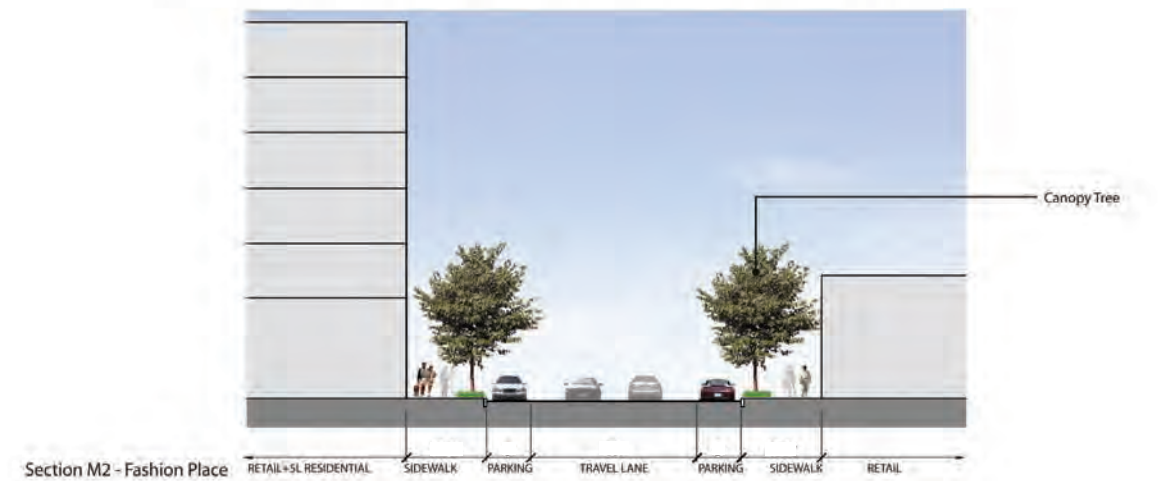
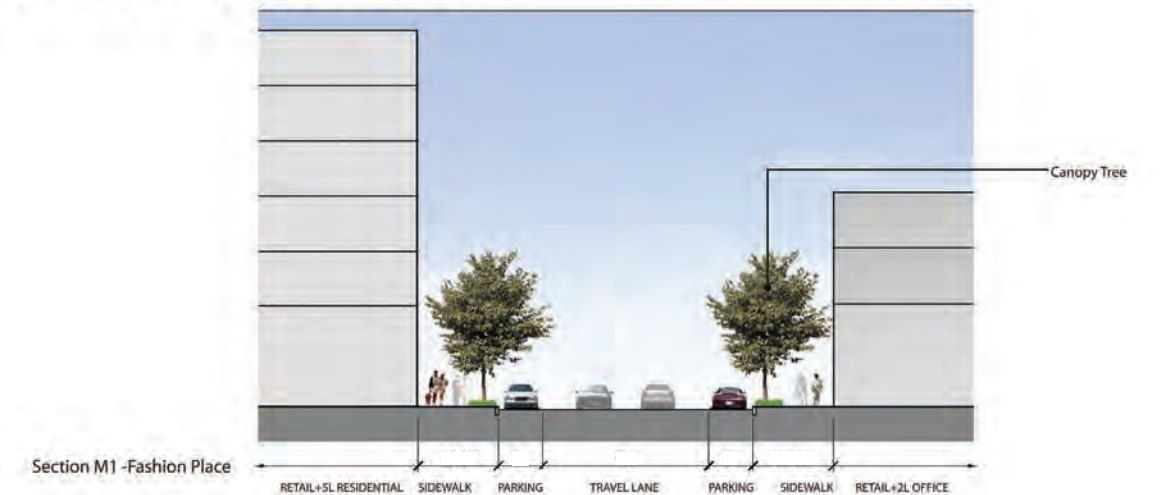
## 5.11 Street Cross Sections

**NORTH END  
Section K2**



Sections

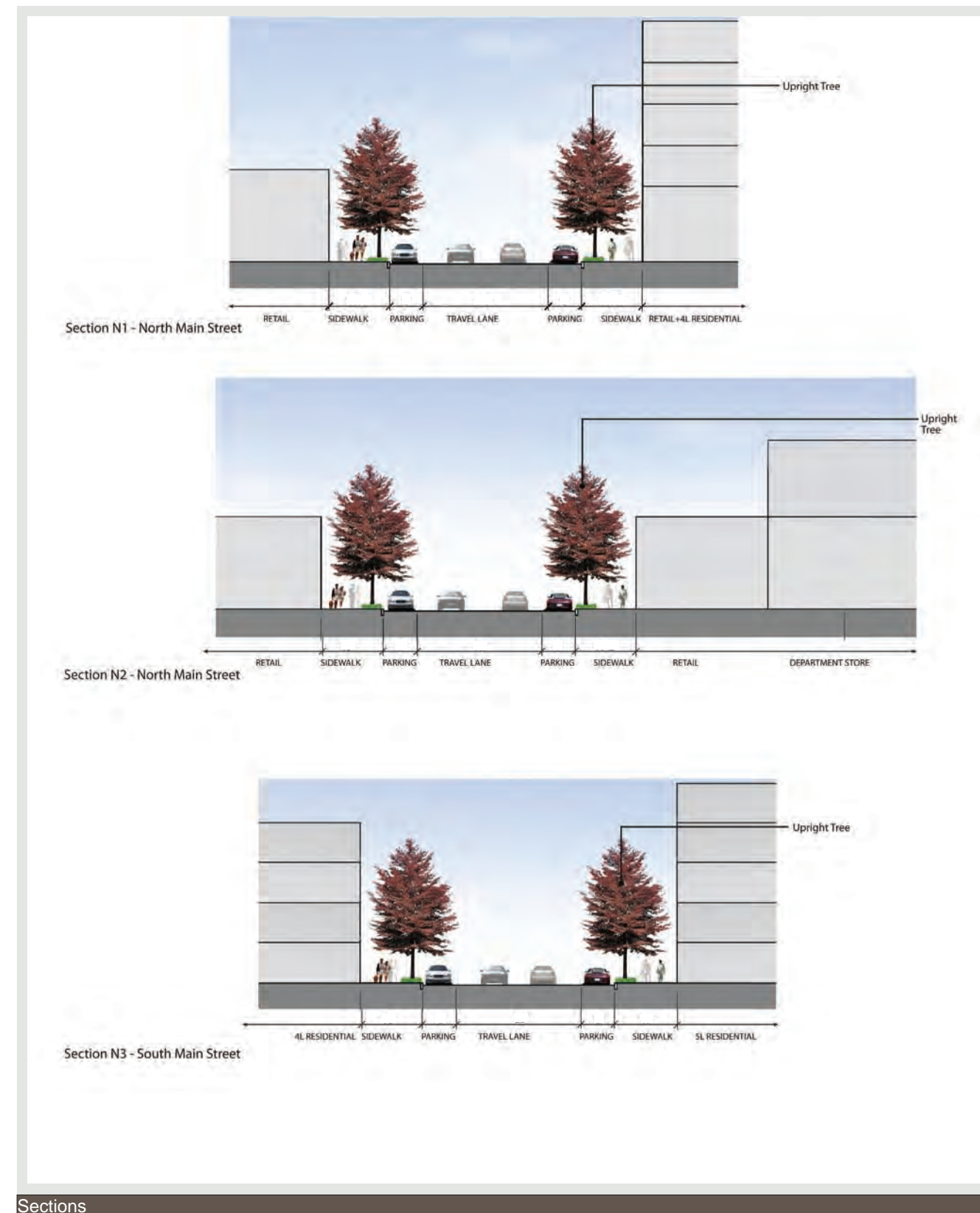
**FASHION PLACE, NORTH MAIN STREET and  
SOUTH MAIN STREET  
Sections M1, M2, M3, N1, N2, and N3**



Sections

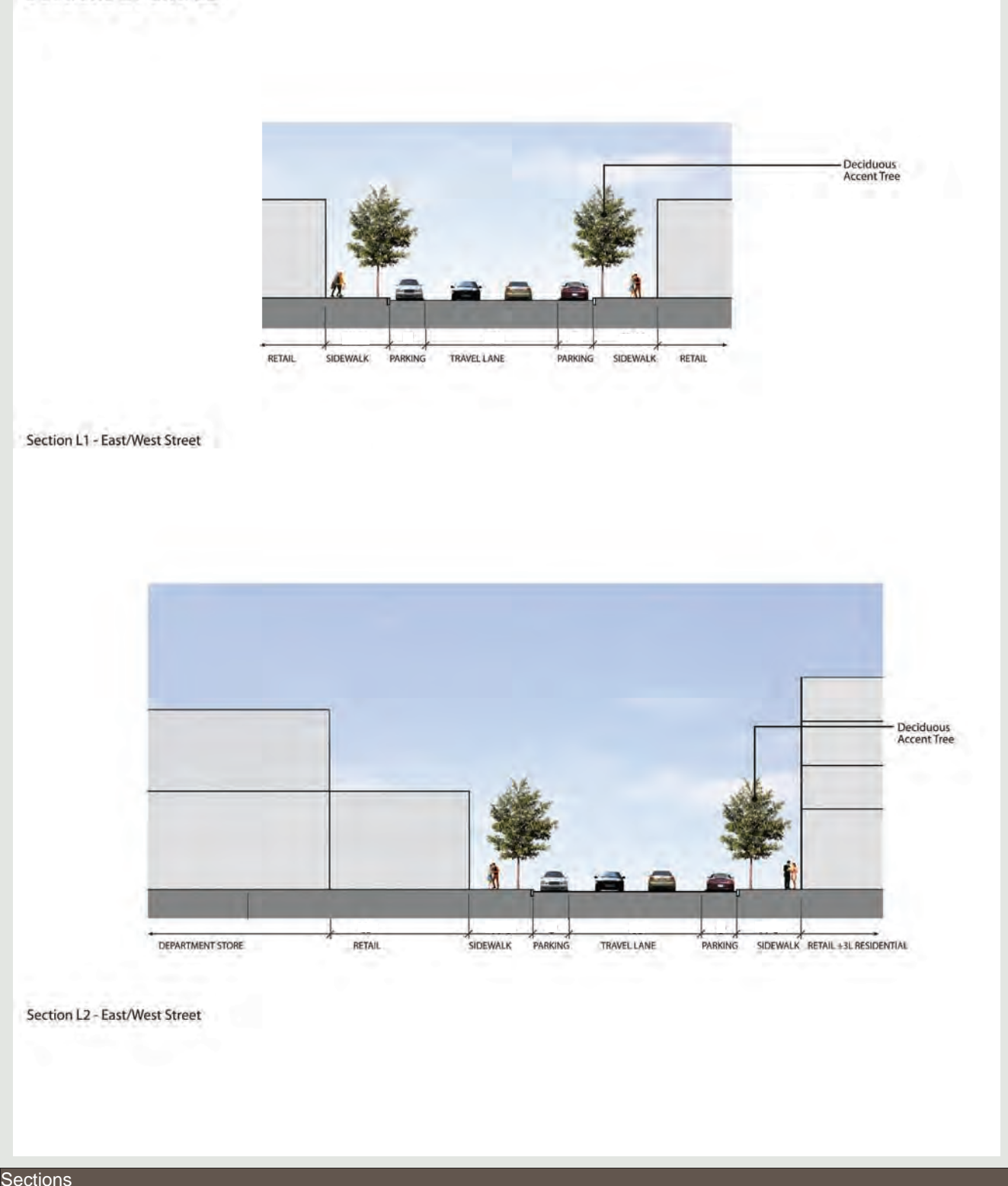


## 5.11 Street Cross Sections



Sections

### EAST/WEST STREET Sections L1 and L2



Sections

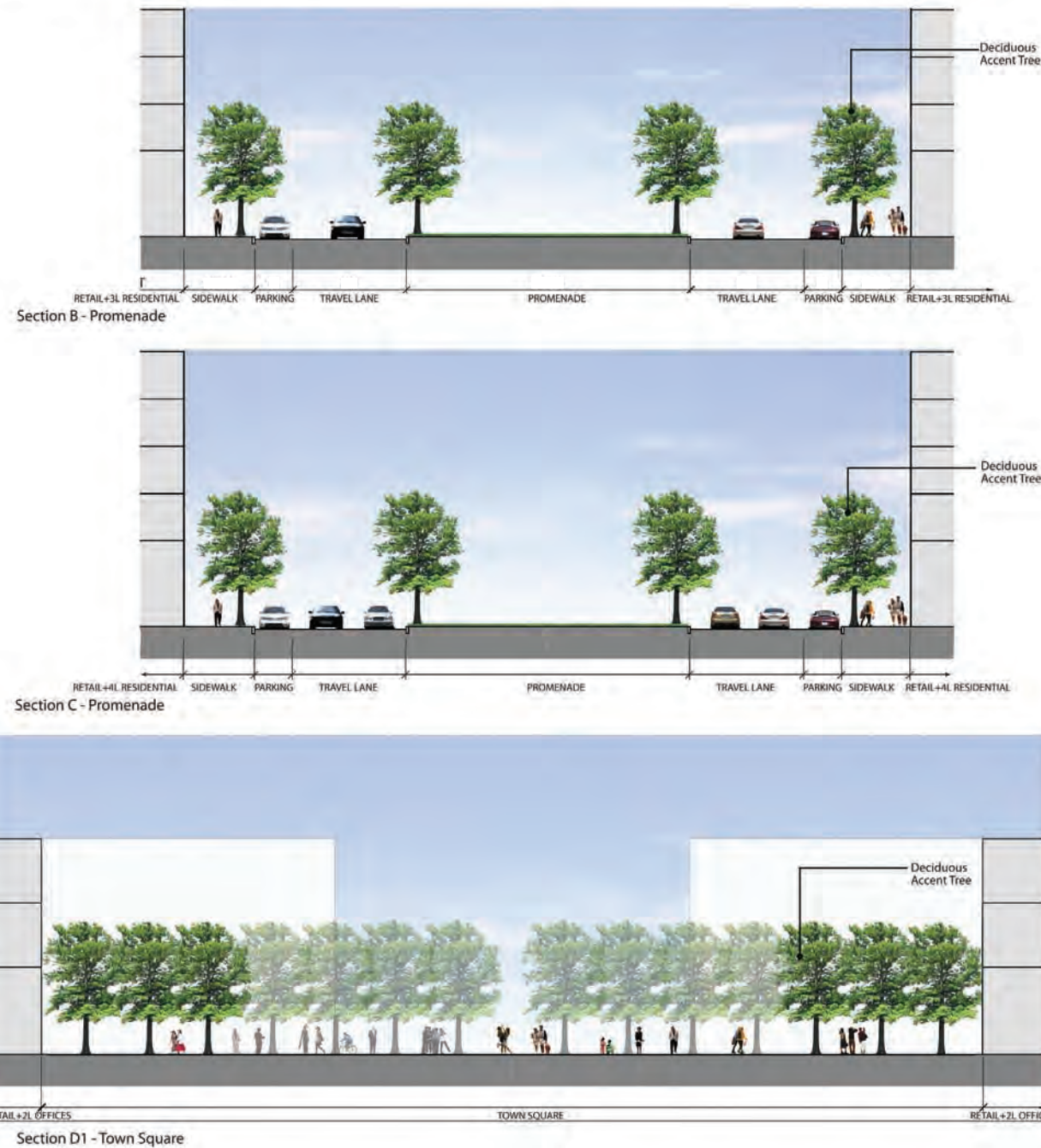


## 5.11 Street Cross Sections

### THE PROMENADE AND TOWN SQUARE Sections B, C, D1, D2, E, F and G

The pedestrian Promenade serves as the actual and symbolic heart of the Town Center. The pedestrian Promenade forms the signature visual axis running through the project north to the core's edge and south to the civic enclave and natural

open space preserve. It represents the Center's lifeline and central landscaped street and courtyard connector. The Promenade bisects the Fashion Place and two Main Streets and parking fields at the Center's perimeter. Formal deciduous trees identify this street's character from the other components of the project.



Sections

Section D2 - Promenade

Section E - Promenade

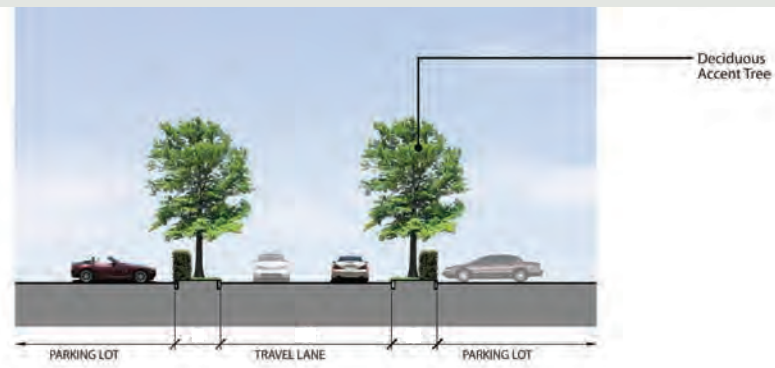
Section F - Promenade

Sections



## 5.11 Street Cross Sections

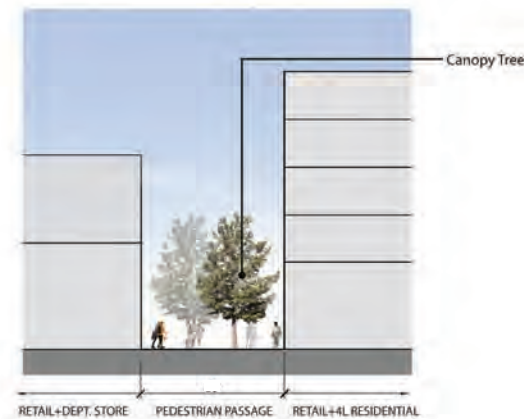
Section G - Promenade



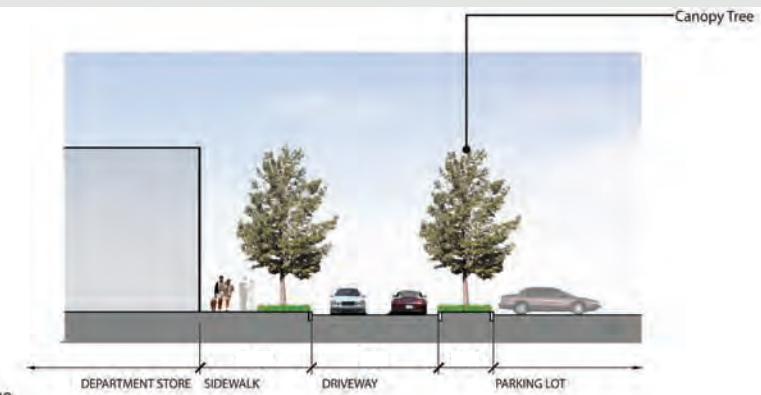
### PEDESTRIAN PASSAGES Sections P1, P2, O and Q

Sidewalks connect the surface parking areas to the Core and adjacent to the commercial retail buildings. Small deciduous canopy trees will define these passages.

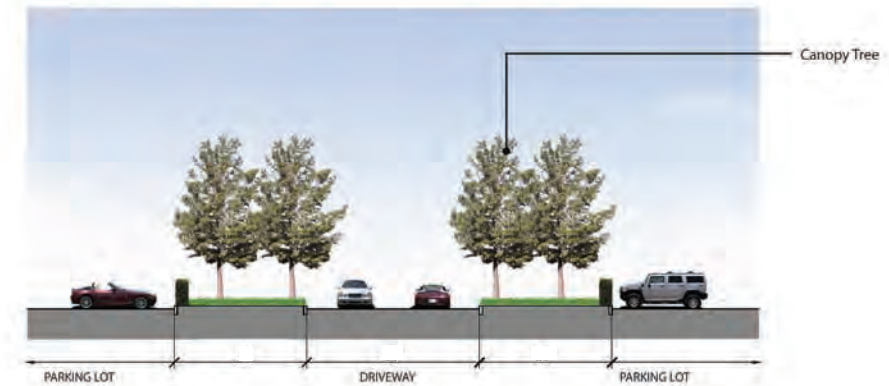
Section P1 - Pedestrian Passage



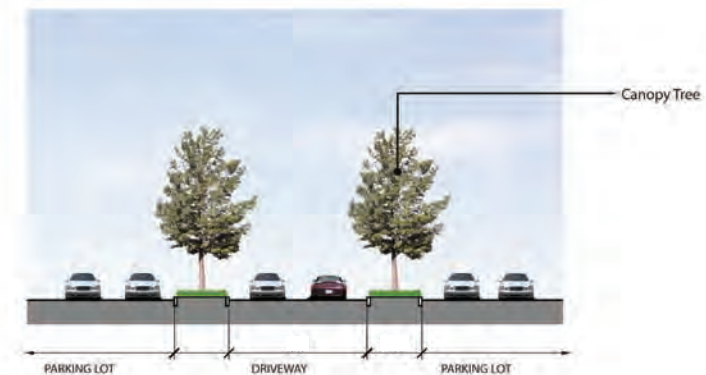
Section P2 - Pedestrian Passage



Section O - Driveway



Section Q - Driveway



Sections

Sections

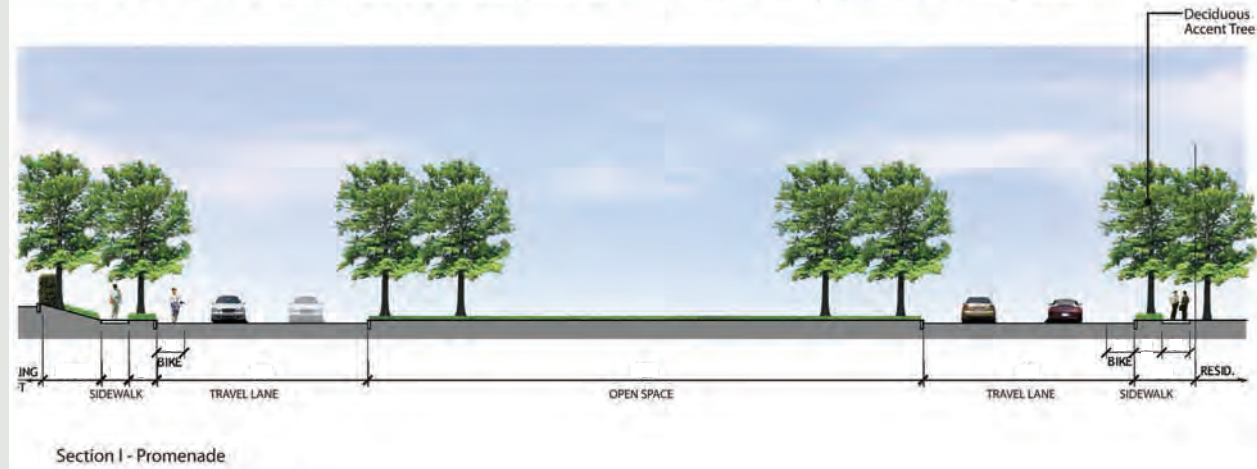


## 5.11 Street Cross Sections

### ROUNDAABOUT GARDEN Section I

The Roundabout Garden will mark the entry into the Core at the intersection of Fashion Place and the East Perimeter

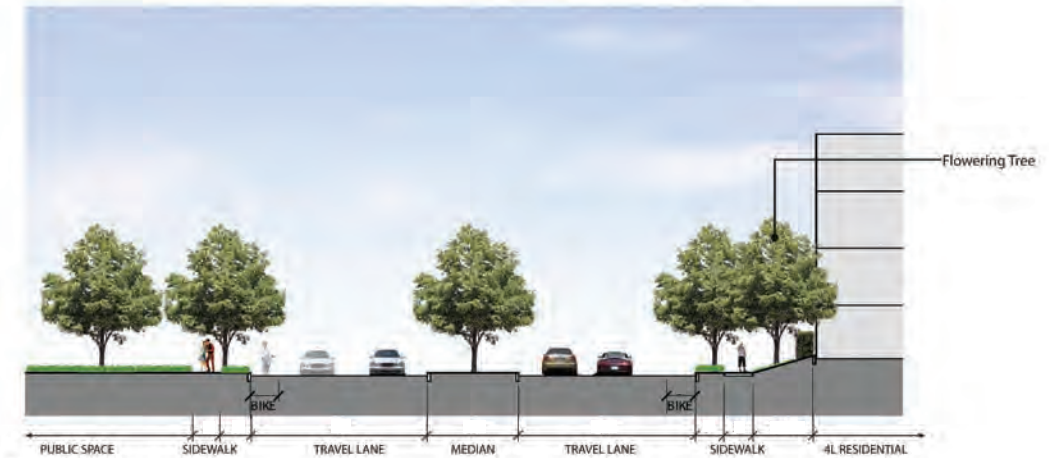
Drive. The central island will consist of perennial grasses and shrubs framed by accents of flowering trees.



### SOUTH END Section J

South End joins the east and west perimeter drives and bisects the Promenade as it continues south towards the proposed Inter County Connector. South End serves as the southern bor-

der of the Core and the civic enclave that includes open space preserves, formal gardens and informal landscape and trial systems. South End will consist of double rows of flowering trees on both sides of the road.

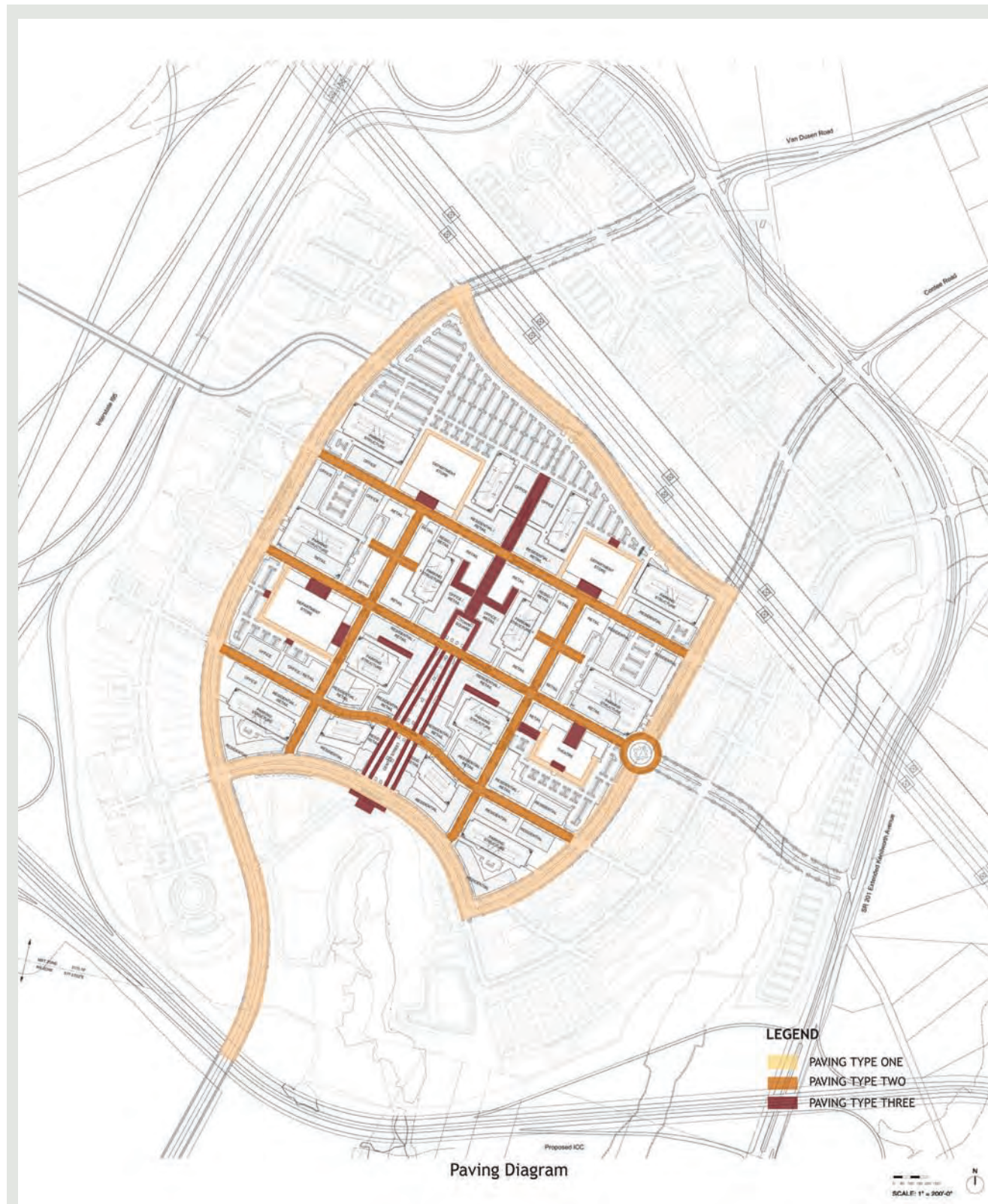


Sections

Sections



## 5.12 Paving



Paving Concept Diagram



Stone Paving Character



Brick Paving Character

Brick Accent Paving in Crosswalk



Precast Unit Paver Character

Specialty Accent Paving

The paving throughout shall consist of a combination of bricks, stone pavers, precast unit pavers and some plain and enhanced concrete. Specialty and accent paving shall highlight distinct areas and entries.

Paving Type One links the outer pedestrian paths and walks around buildings with simple clean paving styles and patterns.

Paving Type Two is located at the primary pedestrian paths formed by the grid of streets in the Core. A mix of distinctive features such as pavers shall be used at these locations to define and characterize zones, crosswalks, and entries into the project from the environs.

Paving Type Three occurs at the plazas, courts, and Promenade areas and serves to accent these open space areas and entries into major commercial and residential buildings. A variety of the unit pavers including stone and specialty paving shall be used in these areas.

Paving Concept Diagram



# 5.13 Plazas and Courts

The plazas and courts for Konterra Town Center East are unique in their design, places to introduce water as a central element of thier composition. Coupled with the Landscape Plan plant palette, the plazas and courts are designed to emulate the heritage planting of the region. The Promenade, Town Square and Main Streets are the largest and most important central spaces in the project. They are the symbolic and actual heart of the Core. The Promenade serves as the physical link and terminus to the civic and public open space preserves to the south.

The Promenade may also include gardens where various plants are designed and labeled to provide an educational as well as an emotional connection to the cultural and historic landscape of the region. The entry courts of larger tenants shall be emphasized as smaller gathering areas with a continuation of the landscape themes and accent planting to create their own identity.

There is a range in size, character type, and programing of the plazas and courts. Some shall have an emphasis on paving with areas for impromptu games and dining, while others are defined by softscape and open grass areas for gathering or play.



Plaza and Courts Character

Plaza and Courts Character



# 6.0 Signage/Graphics Standards



## Introduction

Konterra Town Center East's role as the gateway to Prince George County will be supported through signage and graphic standards that ensure a high level of quality throughout the project.

Signage integrates vehicular and pedestrian circulation patterns to decrease demands on infrastructure. Graphics will help the visitor to navigate the urban environment, orienting first time visitors and imparting a sense of familiarity and comfort for those that return often. The sign program presented here includes a rich variety of sign types in accordance with the needs of a mixed-use environment that will continue to grow over time and integrates with a multi-modal transportation network. Cars are directed into parking and pedestrians are guided to their destinations. The scale and location of sign types prioritizes pedestrian traffic and fosters a strong sense of comfort and safety throughout the environment.

Signage also clearly identifies Konterra Town Center East as an active 24 hour environment that reinvents the location as a new center of growth in the region.

## Goals

*Our goal is to regulate unsightly or detrimental signs, to prevent the proliferation of signs, and to control the size and location of signs.*

This document outlines standards of size, quantity, materials, location of freestanding signs, placement of building-mounted signs, materials and illumination. Photographic references show examples of signage and graphics that meet the standards. Signage and graphics will play an important role in maintaining Konterra Town Center East's standards of quality over time and includes room for the project to grow while making sense at each specific phase.

These standards represent the project's goals. Sign quantities will be driven by the needs of the site and not limited by estimates within this chapter.

## The Organization of this Chapter

*Standards are categorized by use, location and function.*

Sequentially, the use types include: Project signs, parking signs, multi-modal transport signs and other site signs. Other site signs include retail, residential, office and retail anchor and are grouped by building typologies.

## Role of Sign Program

The purpose of the Sign Program is to establish a coordinated exterior sign program that contributes to the quality and theme of the Konterra Town Center East. Graphics and signage will compliment the architecture, theme and streetscape as part of its distinctive sense of place. This document begins to establish a continuous design character and set parameters for all exterior sign types.

Beyond sustaining the quality appearance of Konterra Town Center East, the sign program facilitates a safe and healthy

**Note:** The first permit plans for sites located within the adjoining blocks that front on the Main Streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of trees, street furniture and lighting fixtures for that street on which the project site is fronting.

circulation throughout the site. A variety of project signage activates the sightlines along major arterials. Additional project signs are layered throughout the space. Materials and forms are inspired by an urban tradition. Material standards emphasize durability. This attention to longevity and quality defines Konterra Town Center East as an urban environment that is invested in long-term sustainable growth.

Finally, the signage fosters compliments between uses. Illumination standards consider adjacent uses and will reinforce the cooperative nature of the retail/entertainment, commercial and residential uses. Wayfinding signage allows for a diverse range of configurations and scales. In this way, each use type can respond to the nature of its zone and the needs of its users.

Graphics and signage help to build an experience of the space. This goes well beyond functional identification and directional requirements. Choices of color, shape, ornamental motif, materials, scale, type, orientation, et cetera create a unified sense of place. Specialty graphics such as paving patterns, handrails, banners, sculpture, fountains and public arts add to the sense of quality and excitement within the project.

These elements combine with architecture and landscape to express the personality of the project and its many moods. Graphics help to create a place that brims with energy and interest.

## Project Wide Standards

In addition the goals mentioned above and to follow, all signage and graphics must meet the following design standards:

- *Maintain the quality and theme of Konterra Town Center East and its environment.*
- *Compatible with architecture*
- *Compatible with urban levels of density*
- *Compatible with project location*
- *Visible according to the needs of their function through contrast and the use of type sizes that meet ADA requirements*
- *Durable in construction and materials*
- *Compatible with their use type*
- *As part of a mixed use environment, signs must be sensitive to neighboring uses.*
- *Illumination will be kept to a minimum, with the majority of signs using ambient or concealed internal illumination*

These standards are described in more detail within the following chapter by function, use type and building typology.

The designs of project signs will continue to be refined parallel to site development. Other site signage will be described through a criteria that controls height, sqft, locations, placement and illumination. These sign must meet both landlord and county reviews.

Additional detailed standards are found on page 6.16.

## Project Sign/Graphic Standards

The majority of project signs are freestanding. Height and square feet (sqft) standards shall be finalized by individual sign type upon site plan development. Location and quantity are described conceptually on page 6.1. The lighting of project signs shall not exceed the needs of the site for visibility within each functional category. Signs oriented to adjacent vehicular traffic will require more illumination. We recommend that most of the internal project signs receive ambient or concealed internal illumination.

Materials within the project sign category will use a coordinated palette across all sign types. All street level signs will incorporate materials with integral color in line with the project's architectural standards.

## Site Sign/Graphic Standards

With few exceptions, site signage will be building-mounted. Final development of site signage standards may tighten the overall sqft and illumination allowance for certain classes within the site signage group by use or building typology.

Additionally, all fascia signs will use individually mounted dimensional letters. Square footage for such signs will be measured in the same format as letters integrated into an architectural canopy. No allowance will be granted to account for the space between letters, such as that described in section 27.592.



# 6.1 Project Sign Locations

## Project Signs Introduction

Project signs address the following circulation types:

1. Adjacent vehicular refers to traffic off-site, particularly along the I-95 corridor.
2. Vehicular traffic refers to on-site travel in cars and buses. It may also include alternative forms of transportation, such as bikes which travel at comparable speeds to cars.
3. Pedestrian circulation is on foot. It is vital to the success of this project. The majority of project signs work to move people out of their cars and into a pedestrian mode of circulation. These signs mark pedestrian entry corridors at the edge of the project's massing. Memorable pedestrian landmarks and directional signage assure comfort within the guest experience.

Additional project signs may be added to address mass transit circulation where those needs are not met by the current roster of signs.

In addition to wayfinding graphics such as arrows and maps, specialty graphics contribute to a quality pedestrian environment and will help to foster the long-term sustainability of the project.

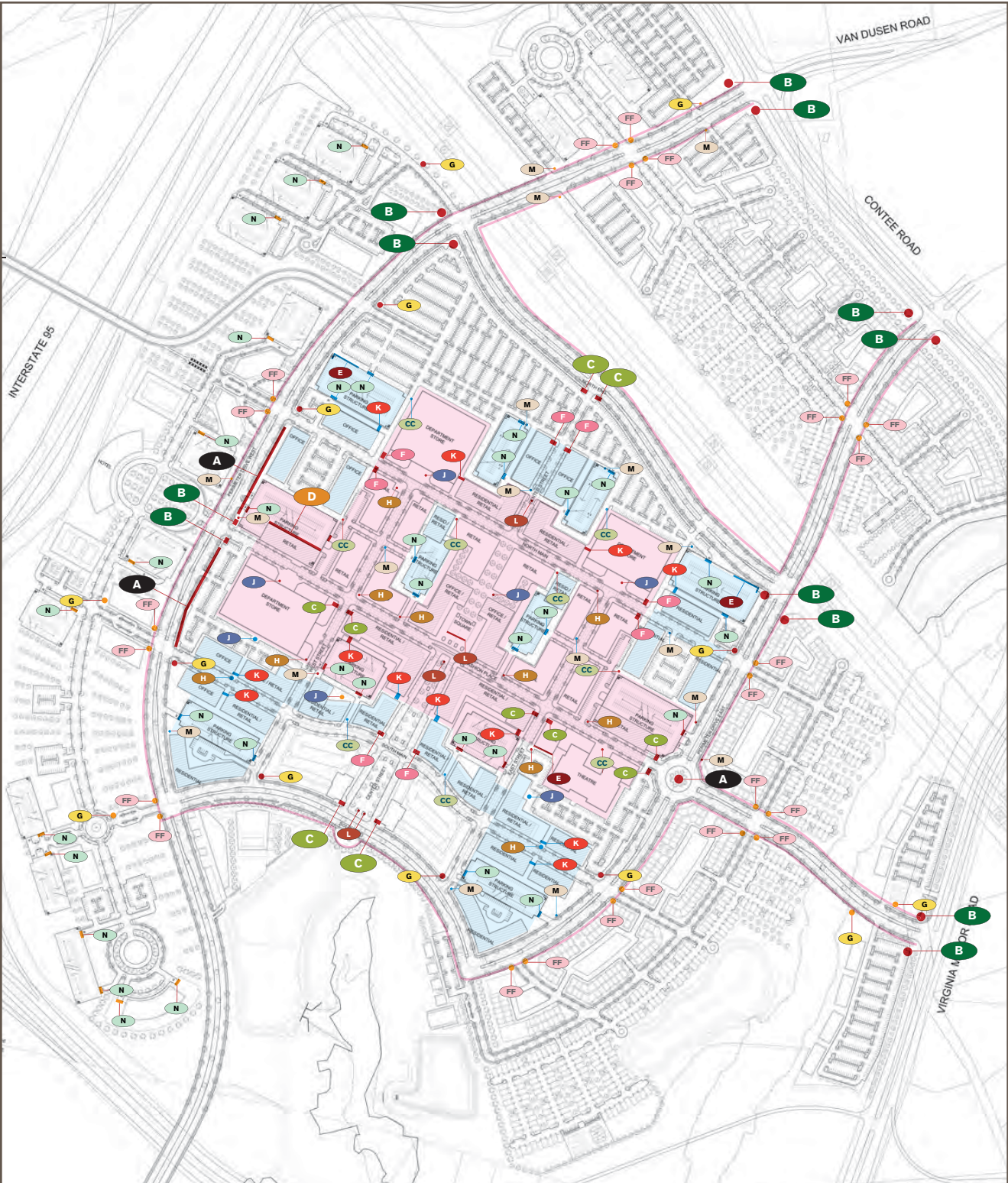
As we proceed through this portion of the chapter, signs are organized according to the circulation patterns outlined in the project introduction (chapter 1.0). This rubric also coincides with a gradual decrease in a.) the speed at which users are traveling at and b.) scale or size of signage. It also is inversely proportional to the level of detail included on each sign, which increases as the speed of travel slows and signs gets smaller. Each sign type has a letter designation shown at right. This shorthand applies throughout the chapter. Signs are not included in alphabetical order, but by the criteria described earlier.

Signs and graphics with the highest level of detail, designed as placemaking, storytelling, and public art elements are reserved for passageways, parks and open space, which are described in the final portions of the project signage sequence.

The location plan at right shows all major project signs for all three phases of the Konterra Town Center East project. The plan is highlighted by color to show the building masses by phase. Sign locations are indicated with a leader line and are not to scale. All leader lines are color coded by the phase of the project in which they will be implemented.

Additional location plans are located within each subsection of the chapter for all major project signs.

Address sign standards are not included here. All address signs will meet county codes and be compatible with site and project architecture.



**Project Sign Conceptual Location Plan.**  
All locations shown here are representative of the project goals. Locations will not be limited by the concept illustrated above. Final locations to be determined based on site plan development. Signs may be added, moved or deleted based on the needs of the site.

## BUILDING MOUNTED PROJECT SIGNAGE/GRAPHICS

- D** Primary Project Identity, Rooftop
- E** Building Mounted Project Identity
- K** Paseo/Alley Identity
- N** Parking Entry Identity

## FREESTANDING PROJECT SIGNAGE/GRAPHICS

- A** Project Landmark
- B** Project Identity Monument
- C** Secondary Project Identity, Freestanding
- CC** Secondary Project Identity, Pole-Mounted
- F** Pedestrian Project Gateway
- FF** Pedestrian Project/Site Gateway
- G** Vehicular Directional
- H** Pedestrian Directional
- J** Project Directory
- K** Paseo/Alley Identity
- L** Park/Open Space Identity
- M** Parking Directional

## LOCATION AND PLAN LEGEND

- Phase One Implementation
- Phase Two Implementation
- Phase Three Implementation
- Phase One Massing
- Phase Two Massing
- Phase Three Massing

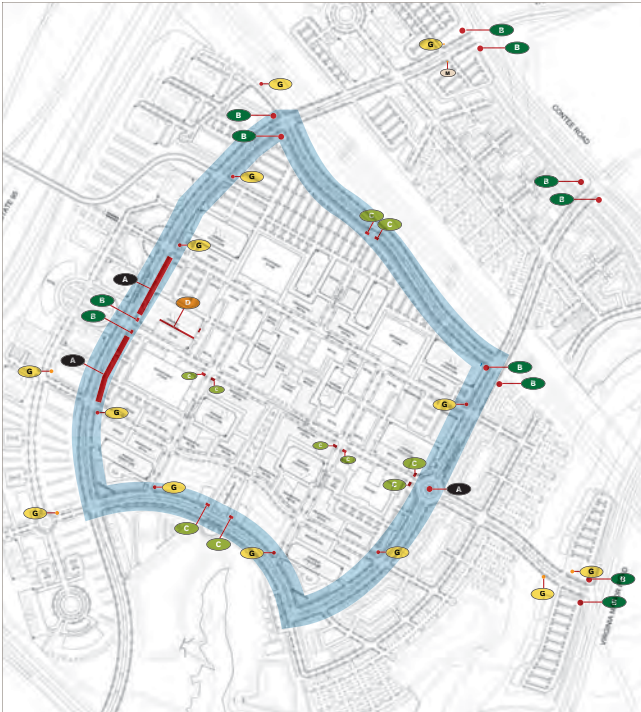
## Additional Project Signs

Additional signs or changes to the locations indicated here may be required based on site plan development. Within this range, sqft of signs shall not exceed by more than 15% of these standards. Other amenities signage may be needed, to include but not limited to bus signs, mass transit, valet parking, taxi drop-off, and concierge services. These shall be considered separate from the 15% variance.

Smaller elements may be added as part of the overall placemaking plan within the site, including but not limited to paving elements, all plaques, project banners and temporary seasonal pageantry.



## 6.2 Perimeter Project Signs



Overall plan showing perimeter sign locations

### Perimeter Project Signs

Project perimeter signs include (A) Project Landmark, (B) Project Identity Monument, (C) Secondary Project Identity, (D) Primary Project Identity, Rooftop and (G) Vehicular Directional.

### Intent

Signs in this category are oriented to either adjacent or site vehicular traffic. They are intended to promote a safe transition of vehicular traffic into the project interior.

All type and graphics will be scaled for clear legibility by users traveling at high and medium speeds along the project perimeter. For a select few signs, the goal is visibility from the I-95 corridor.

### Recommendations

Signs will be composed of materials with a strong relationship to the site architecture. Formal design elements of the sign style and details will be drawn from the formal vocabulary of key buildings within the site and in the landscape design.

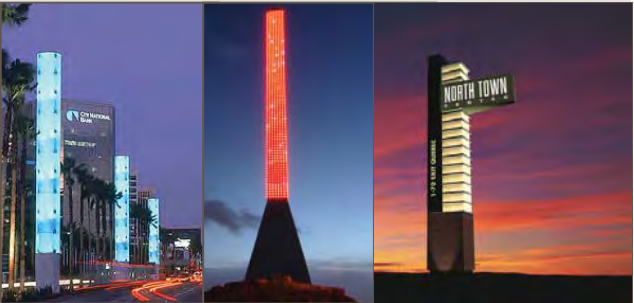
Within this category, signs will also have a strong formal relationship to each other, with a consistent use of materials and illumination standards.

### Design Standards by Signtype

**A. Project Landmark:** An icon for Konterra Town Center East as the gateway to Prince George County that identifies the project as an active regional destination.

location: see p6.1  
max height: 100ft      max sqft: TBD  
quantity: 3  
material: durable modern materials  
illumination: internal  
required: no tenant names  
suggested: repeating element along I-95  
no type on landmark  
optional: lights may be animated  
project name

\* Project Landmark Signs in excess of 50 ft shall require approval of a separate detailed site plan.



A. Examples of graphics that meet the project's standards

**B. Project Identity Monument:** A horizontal sign that marks the project boundaries. The project name will be dimensional and strongly integrated into the construction of the gateway.

location: see p6.1  
max height: 15ft      max sqft: 600  
max letter height: 8ft  
quantity: 5 symmetrical pairs  
material: durable modern materials  
illumination: external and halo  
required: project name  
no tenant logotypes  
suggested: masonry base  
optional: vertical beacon up to 50 ft high  
upto 3 tenant names  
lantern element

\* Vertical Beacon Signs in excess of 30 ft shall require approval of a separate detailed site plan.



B. Examples of monuments that meet the project's standards

**C. Secondary Project Identity:** Pedestrian scaled vertical gateway that marks key entry locations for vehicular traffic. The secondary identity may take two forms: 1. Similar to the project monuments as over scaled gate posts with masonry construction. 2. Freestanding pole-mounted sign constructed of painted metal.

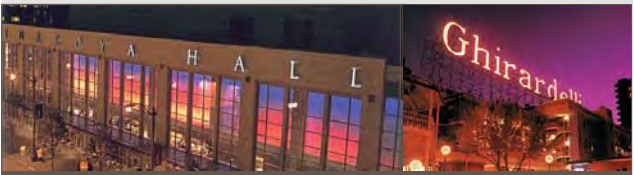
location: see p6.1  
max height: 30ft      max sqft: 90  
max letter height: 18 in  
quantity: 5 pairs  
material: durable modern materials  
illumination: external and halo  
required: project names  
no tenant names  
suggested: street name  
bench/seating element  
optional: lantern element



C. Examples of secondary identity signs that meet the project's standards

**D. Primary Project Identity:** A horizontal sign building-mounted sign visible from 1-95. Max height refers to height above roof line. This sign will have large-scaled individual letters mounted to an exposed metal framework attached to the parapet or mounted to the site architecture. The lighting of the sign will not have any movement or blinking, but will be tastefully done to reflect the precedents in other urban applications.

location: see p6.1  
max height: 20ft      max sqft: 800  
max letter height: 15ft  
quantity: 1  
material: exposed frame, metal letters  
illumination: external internal  
required: project name  
no tenant logotypes  
suggested: limited color palette  
optional: exposed lighting fixture  
limited hours of illumination



D. Examples of building-mounted/rooftop sign that meet the project's standards

**G. Vehicular Scaled Directional:** Located at major vehicular intersections, these directionals guide the visitor in the car to major areas within the site, such as theatres, offices, anchor tenants, plazas and other amenities. These directionals typically have no more than six listings with arrows. Sign may include both vertical or horizontal designs, according to the needs of the site.

location: see p6.1  
max height: 8ft      max sqft: 90  
max letter height: 8 in  
quantity: 12  
material: durable modern materials  
illumination: external and internal  
required: no tenant logotypes  
uniform directional font  
suggested: masonry base  
metal sign panels  
push-through letters  
optional: two-sided



G. Examples of vehicular directionals that meet the project's standards



## 6.3 Internal Project Signs



Overall plan showing internal sign locations

### Internal Project Signs

Along the interior and active streets with Konterra Town Center East, internal project signs will focus on pedestrian traffic. Pedestrian directionals concentrated along East/West streets include: (E) Building-mounted Project Identity, (F) Pedestrian Project Gateway, (FF) Pedestrian Project/Site Gateway and (H) Pedestrian Directional.

### Intent

East/West streets create strong axes for pedestrian traffic and form the basis for tenant districting. North/South streets support connections with intersecting pedestrian passageways.

### Recommendations

Signs will be composed of materials with some relationship to the site architecture.

Formal design elements of the sign style and details will be drawn from the project perimeter signs. Within this category, signs will incorporate an additional level of detail, appropriate for pedestrian interaction.

### Design Standards by Signtype

**E. Building-mounted Project Identity:** Additional project identity signs will be located on perimeter site architecture as project gains density in later phases. Signs must compliment architecture.

location: see p6.1  
max height: NA      max sqft: 200  
max letter height: 8ft  
quantity: 3  
material: durable modern materials  
illumination: external and/or halo  
required: project name and/or logo  
no tenant names  
individual letters on site wall  
suggested: dimensional metal letters  
optional: projected element



E. Examples of building-mounted project identity signs that meet the project's standards

**F. Pedestrian Project Gateway:** A vertical pole-mounted sign that marks key transition areas within the project.

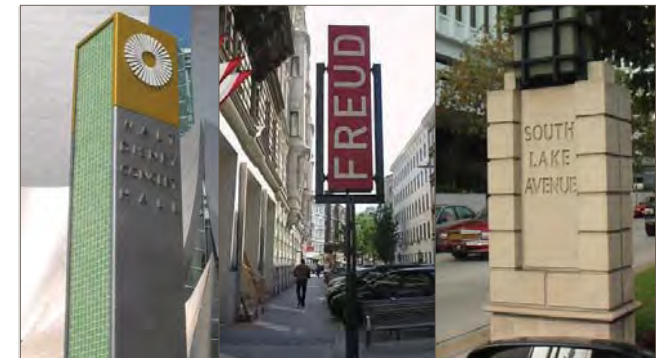
location: see p6.1  
max height: 15ft      max sqft: 12  
max letter height: 8in  
quantity: 8  
material: durable modern materials  
illumination: external and/or internal  
required: project name  
no tenant logotypes  
suggested: push-through letters  
optional: internal district name



F. Examples of pedestrian project identity signs that meet the project's standards

**FF. Pedestrian Project/Site Gateway:** A sign that identifies both the Konterra Town Center East project and select site development outside the project perimeter. Sign blends elements of the project signage with elements unique to the site location

location: see p6.1  
max height: 15ft      max sqft: 12  
max letter height: 8in  
quantity: 24  
material: durable modern materials  
illumination: external and/or internal  
required: site name  
suggested: internally illuminated letters  
vertical orientation  
optional: project name  
street name or project address



FF. Examples of pedestrian project/site gateways that meet the project's standards

**H. Pedestrian Directional:** Freestanding or building-mounted projecting blade signs that point to key site amenities, anchor tenants or other important destinations within the project. Bottom of sign panel must be a minimum of 8ft from the ground plane.

location: see p6.1  
additional locations TBD  
max height: 15ft      max sqft: 12  
max letter height: 3.5in  
min letter height: 3 in  
quantity: 18  
material: durable modern materials  
illumination: external and/or internal  
required: uniform directional font  
no tenant logotypes  
suggested: internally illuminated letters  
optional: project name and/or logo



H. Examples of pedestrian directionals that meet the project's standards



## 6.4 Alley and Passageway Signs



Overall plan showing passageway sign locations

### Alley & Passageway Signs

Signs and graphics in this category include: (K) Passageway/Alley Identity, Restroom Plaques and additional specialty graphics to be determined with site plan development.

Because these signs have a wide range of uses, they are divided into a few options based on the size and location of the application.

### Intent

Passageways and alleys are located at transition points between uses, such as parking and back of house, retail and residential, public and private.

A range of signs that marks these locations will create memorable landmarks for wayfinding. In some areas Passageway Identity signs define the boundary between public and project space. In these cases, the signs should relate more strongly to adjacent site architecture than any project design palette.

### Design Standards by Signtype

**K1. Passageway/Alley Identity, Option 1:** Building-mounted signs that spans the entire width of the passageway. Passageway or alley name is strongly integrated into the spanning structure. Height measured from lowest part of built element. Distance from ground plane must be 8ft.

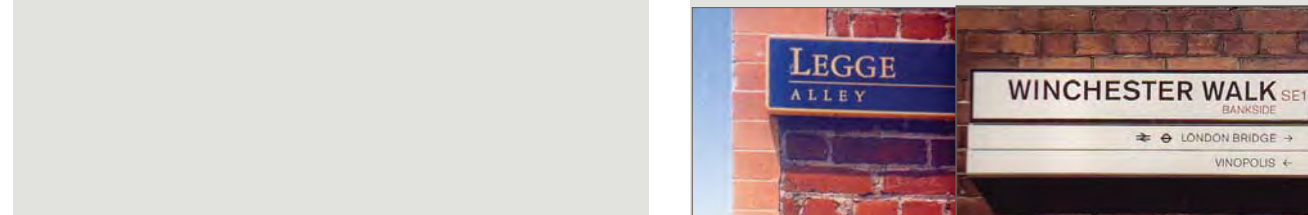
location: locations TBD  
max height: 8ft max sqft: 60  
max letter height: 18in  
min letter height: 3 in  
quantity: TBD  
material: durable modern materials  
illumination: external  
required: painted metal construction  
suggested: project logo  
optional: integrated planting



K1. Examples of passageway/alley signs that meet the project's standards

**K2. Passageway/Alley Identity, Option 2:** Freestanding architectural portal. Sqft measured using maximum height and width, not to exclude the opening within.

location: locations TBD  
max height: 15 ft max sqft: 100  
max letter height: 18 in  
min letter height: 3 in  
quantity: TBD  
material: durable modern materials  
illumination: internal  
required: identifying type or icon  
suggested: project logo  
optional: internally illuminated body



K3. Examples of wall mounted passageway/alley signs that meet the project's standards



K2. Examples of freestanding passageway/alley signs that meet the project's standards

**K3. Passageway/Alley Identity, Option 3:** Plaque mounted onto site architecture.

location: locations TBD  
max height: 2 ft max sqft: 10  
max letter height: 18 in  
min letter height: 3 in  
quantity: TBD  
material: durable modern materials  
illumination: external or ambient  
required: identifying type  
suggested: project logo  
optional: additional directional info



K3. Examples of wall mounted passageway/alley signs that meet the project's standards

**Restroom Directionals:** Directional blades mounted onto site architecture.

location: above restroom corridors  
max height: 2 ft max sqft: 10  
min letter height: 3 in  
quantity: TBD  
material: durable modern materials  
illumination: internal or external  
required: identifying type  
suggested: push-through letters  
optional: arrow

**Restroom Plaques:** Wall plaques mounted onto site architecture.

location: on wall adjacent to entry  
max height: 2 ft max sqft: 10  
max letter height: 2 in  
min letter height: 5/8 in  
quantity: TBD  
material: durable modern materials  
illumination: external or ambient  
required: identifying type and icon  
dimensional type and icon  
braille  
suggested: project directional font  
optional: NA



Examples of restroom signs that meet the project's standards



## 6.5.0 Open Space and Plaza Signage



Overall plan showing open space sign locations

### Open Space and Plaza Signs

Signs and graphics in this category include: (J) Project Directory, (L) Park/Open Space Identity, (CC) Pole-Mounted Secondary Project Identity, (PA) Public Art, and any additional specialty graphics to be determined at site plan development.

### Intent

Unlike most of the signtypes reviewed in the first part of this chapter which are about wayfinding, the signs within this category function largely as placemaking. The intent of signage and graphics within plazas and open spaces is to foster the role of human comfort in creating sustainable communities.

### Design Standards by Signtype

**J. Project Directory:** Freestanding case with changeable space for project map with retail tenant locations. Alternate configurations allow for dimensional or flat map on table top style directory or wall-mounted directory sign.

location: locations TBD  
 max height: 8 ft      max sqft: 50  
 max letter height: 8 in  
 min letter height: 3 in  
 quantity: 8  
 material: durable modern materials  
 illumination: internal or external  
 required: changeable map case  
 suggested: two-sided  
                     back reserved for ads  
 optional: internally illuminated map  
                     pocket for paper maps  
                     directional arrows



J. Examples of directory signs that meet the project's standards

**L. Park/Open Space Identity:** Freestanding sign element that may take many forms, from a horizontal monument integrated into site furnishings to a pole-mounted painted metal sign. These signs are primarily pedestrian in nature. Specific design elements should reflect the sign location and the uses contained within the open space.

Along the North-South promenade, the park should be viewed as a continuous park with a unified sign family.

location: locations TBD  
 max height: 12 ft      max sqft: 36  
 max letter height: 12 in  
 min letter height: 6 in  
 quantity: TBD  
 material: durable modern materials  
 illumination: external or ambient  
 required: park/open space name  
 suggested: park rules and hours  
 optional: directionals to off-site trails  
                     sponsorship information



L. Examples of open space identity signs that meet the project's standards



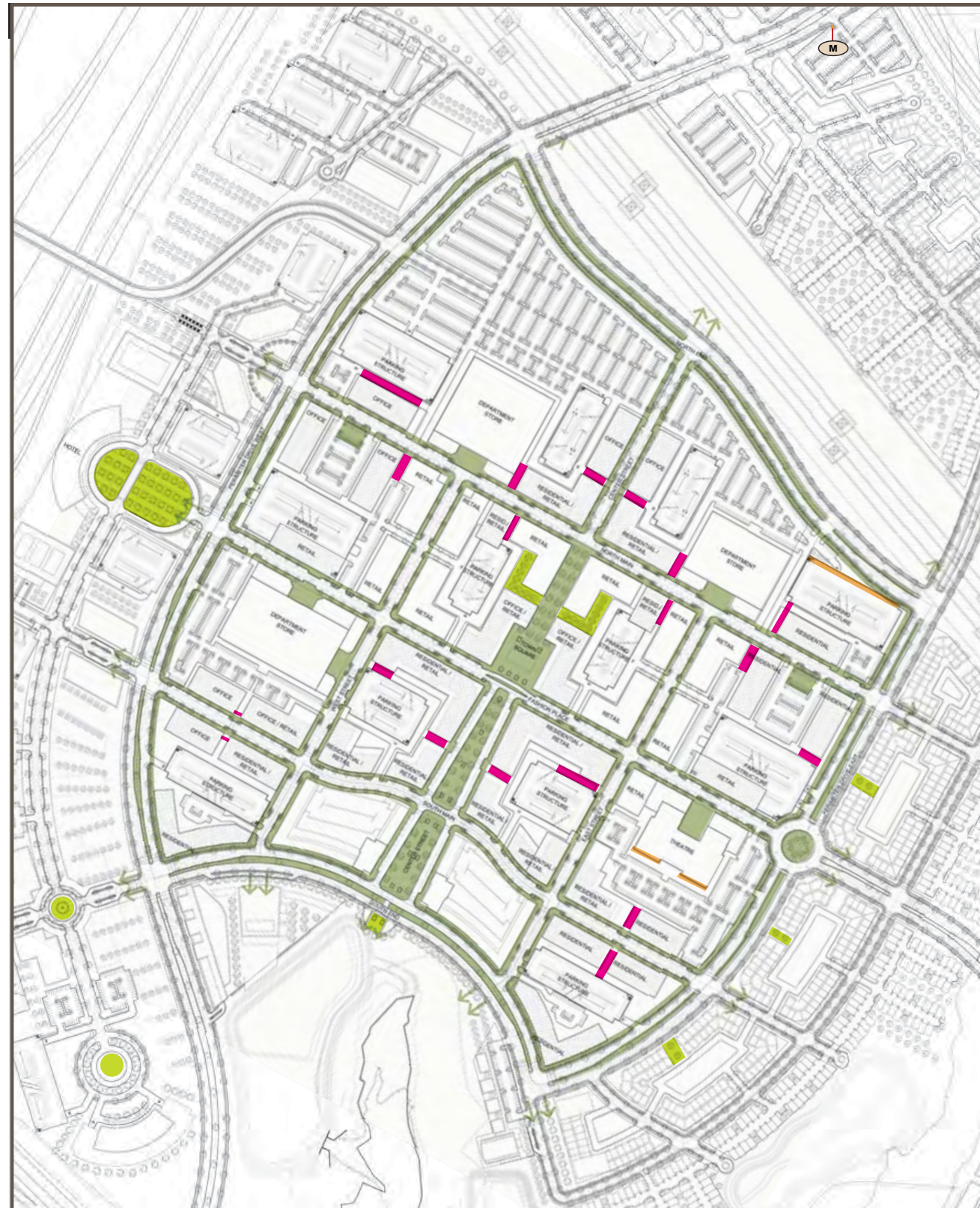
CC: Examples of pole mounted secondary project identity signs that meet the project's standards

**CC. Pole-Mounted Secondary Project Identity:** A pedestrian scale sign located at transition areas between structured or surface parking and the project's interior. These simple signs serve as a directional landmarks for project wayfinding and add to the texture of the street.

location: locations TBD  
 max height: 12 ft      max sqft: 12  
 max letter height: 8 in  
 quantity: 8  
 material: painted metal  
 illumination: internal or external  
 required: painted pole  
                     district or area name  
 suggested: push-through letters  
 optional: directional arrows



## 6.5.1 Open Space and Plaza Graphics and Public Art



Overall plan showing potential areas for public art locations, including plazas, paseos and outdoor rooms

### Design Standards by Signtype

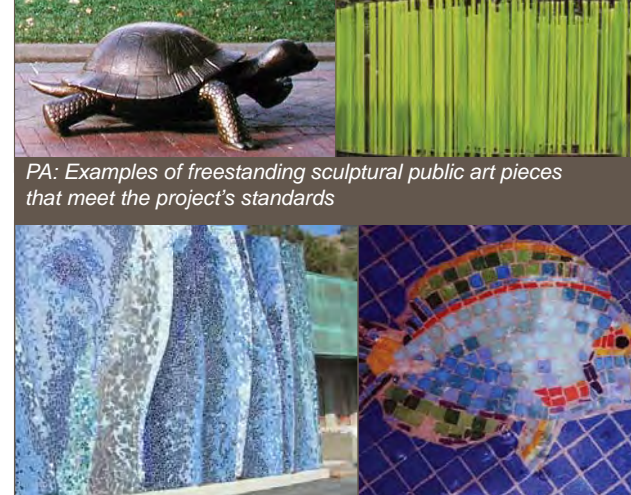
**PA. Public Art:** Public art adds to the texture of the streetscape, expressing the qualities of community found at Konterra Town Center East.

Locations indicated on the plan are appropriate for freestanding sculptural elements, murals, custom bike racks, enhanced walls or benches, tile features and paving graphics. Additional locations may be added based on site plan development. Further criteria for public art selection to be determined with site plan development.

locations: locations TBD  
 max height: NA      max sqft: NA  
 quantity: min. 2  
 material: durable exterior grade  
 illumination: TBD  
 required: must meet community standards of decency  
 suggested: regional artists  
 optional: sustainable art  
 temporary installations



PA: Examples of freestanding sculptural public art pieces that meet the project's standards



PA: Examples of wall mounted mosaic murals that meet the project's standards for public art



PA: Examples of paving graphics that meet the project's standards for public art



PA: Examples of additional graphics that meet the project's standards for public art



## 6.6 Multi-modal Transport Signs

### Custom Regulatory Signs

Within the boundaries of Konterra Town Center East the project may choose to use custom regulatory signs, including traffic signs, mid-block crossings and public transit signs.

### Intent

Konterra Town Center East will incorporate the best concepts in multi-modal transportation including bicycle, pedestrian, vehicular and mass transit. The use, location and messaging of regulatory signs will foster safe and healthy circulation within the project. Signs will be located by a traffic engineer on site plan development.

Konterra Town Center East is a large development within the county and as such may choose to create a custom street sign or street sign pole and additional custom regulatory signs. These signs will be code compliant while adding another layer of quality to the project.



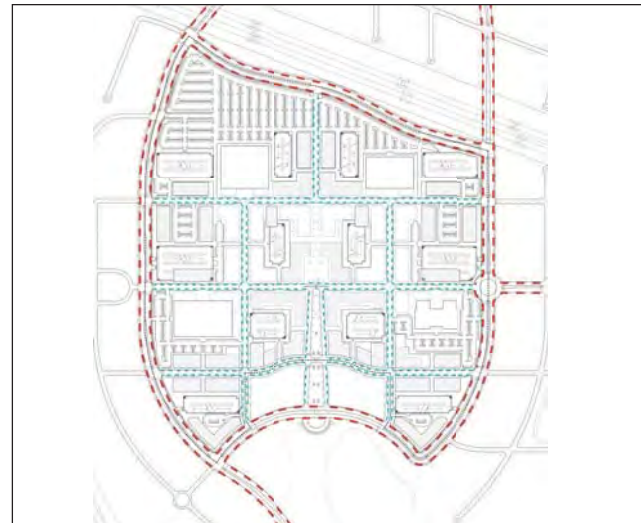
Examples of custom street signs



Examples of custom regulatory signs



Examples of custom crosswalk signs



Overall plan showing bike lane locations



### Bike Signs

Bike signs may be added to the palette of project signs, particularly along the perimeter streets. Directional signs to bike parking may also be added along primary bike routes to locations within the project interior.

### Intent

The use of bicycles is central to project philosophy of multi-modal transportation. As the project develops over time, with the extension of the Washington D.C. Metro Line, the use of bicycles will increase. Project signage will be used to insure the safety and convenience of bicycles for commuters.



### Trail Signs and Maps

Trail signs round out the suite of multimodal transportation/project signs. Directional signs located at the project boundary facilitate the flow of bike, pedestrians and other alternative transport both in and out of the site. This includes both fitness and commuter uses.

### Intent

Trails sustain the site over the long-term and further the goals of safe and healthy circulation within Prince George County. Signs will include important destinations off-site such as transportation hubs, public parks, and a regional trail system. Mile markers and maps are encouraged.

Signs will meet high standards of visibility, durability and vandal resistance.

Required: Maps and directionals to be coordinated with appropriate local and regional park personnel.

Suggested: Type and graphics may be created using an acid-etched and paint-filled application, such as that employed by the National Park Service.

Optional: Additional educational and interpretive information may be included on directional signs or additional sign types not specifically mentioned here.

### Back of House and Service Signs

The project will require additional emergency, back of house and service signs not specifically mentioned within these standards. Such signs will conform to project wide standards of color, font and materials. Additional ADA and code compliance controls the messaging, size, location and quantity of such signs.

Service and delivery directionals will be building or pole-mounted plaques which compliment project-wide design standards.

Individual sites, such as multi-tenant residential and commercial, may employ a separate sign family for these back of house signs. For individual sites, the interior sign family, identifying door plaques, elevators, et cetera, will follow uniform design standards.



Examples of service directional



Examples of emergency signs



## 6.7 Parking Signs



Overall plan showing entries into structured parking and exterior parking sign locations

### Parking Signs

Parking signs in the public realm are (M) Parking Directional, and (N) Parking Entry Identity. Additional interior parking signs will be required to facilitate the flow of traffic into parking structures and the smooth transition from cars to foot traffic.

### Intent

Parking signage should facilitate a smooth transition from vehicular to pedestrian circulation. A wide range of users and use types creates a wide range of parking sign designs.

The design standards leave room for each site and use to craft parking signage that meets the unique needs and user group.

**M. Parking Directional:** Freestanding pole mounted sign or building-mounted projecting blade sign with the universal parking "P" as the primary message in a high contrast design.

location: see plan  
max height: NA      max sqft: 12  
max letter height: 12 in  
quantity: min. 15  
material: durable modern materials  
illumination: internal or external  
required: two-sided messaging  
suggested: parking use designation, such as public, residential or addresses of commercial offices  
optional: parking lot name or number



M. Examples of freestanding parking directional sign that meet the project's standards.



M. Examples of freestanding parking directional sign that meet the project's standards.

**N. Parking Entry Sign:** A parking entry identity will be required for all structured parking lot entries. Within the project, office and residential parking structures may choose to use an understated approach to define their parking entries. Retail structured parking should use internally illuminated letters that are highly visible along the major entry axes.

Parking signs must relate to the streetscape and the adjacent building typology. Likely configurations include: entry along a side street or alleyway, retail at grade entry, office entry, residential parking entry, shared parking entries and may incorporate tenant identity signage where parking feeds an anchor department store location.

location: see plan  
max height: NA      max sqft: 30  
max letter height: 30 in  
min letter height: 12 in  
quantity: min 30 by phase three  
material: durable modern materials  
illumination: TBD by use  
required: parking  
suggested: address or site name  
optional: NA



M. Examples of parking entry signs that meet the project's standards.



## 6.8 Tenant Site Signage Introduction

### Intent: a varied experience

Tenant signs within Konterra Town Center East will be required to meet strict standards of quality and meet landlord approval prior to construction. Standards will include quantity, quality, illumination, location, and dimensions based on lineal foot of frontage along public streets.

Building typologies will be the primary characteristic used to define the standards. In the following pages, each building type's signage and graphics standards are described in greater detail.

Konterra Town Center East will also employ an additional organizing principle of tenant districts to add texture within the pedestrian experience. The concepts for these districts are outlined in the following descriptions within this page.

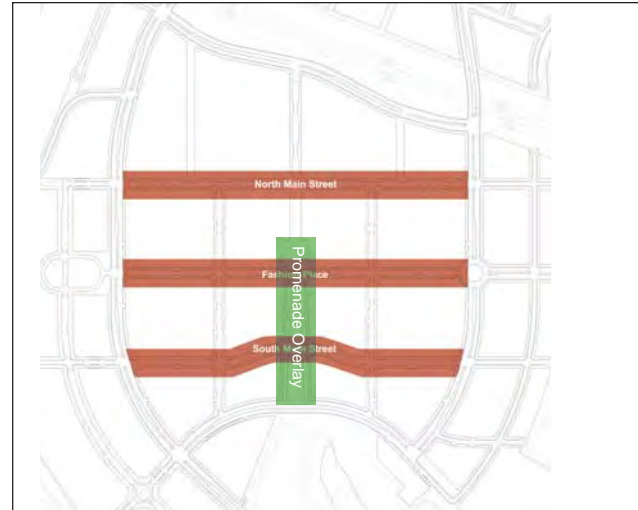
### Goals

1. To generate varied and creative site signage through distinctive logos, type styles and design elements.
2. To establish signage as a design element that contributes to the experience of a mixed use environment unique to the region.
3. To provide standards of acceptability for signs in order to facilitate the review and approval process.

### Overall Site Signage Standards

All tenant signs should include only the tenant name and logotype, no tag lines allowed. Within the following sign types, each tenant shall have a maximum of one (1) sign per sign type per entry frontage. Additional standards on page 6.16.

1. **Fascia/Canopy Sign.** Fascia sign must use individually mounted dimensional letters with a uniform color palette. No backplates allowed. Canopy signs may integrate the tenant name into a raceway.
2. **Building-mounted Projecting Blade Sign.** Dimensional blade signs are encouraged for all retail and commercial tenants with ground level entries.
3. **Window Graphics:** Vinyl to appear as etched may not exceed 10% of the total window area for street level retail tenants only.
4. **Awning signage:** Canvas awnings with tenant name in silk-screened or embroidered type on the vertical surface of awning may be permitted where compatible with site architecture. Type not to exceed 60% of the vertical sign band. Type max height: 8 in. No illumination.
5. **Additional specialty tenant signage:** Paving graphics, entry plaques and other applications to be permitted based on landlord approval. Multiple signs allowed within this sign type.



Overall plan showing East-West streets and the Promenade overlay for the purposes of tenant signage districts

### North Main Tenant Signs

The northern most district marks the terminus of the central promenade. It expresses the energy of a mixed use environment. These qualities define North Main as an active center.

### Recommendations:

Signage within the North Main district will encourage unique and active approaches to signage, such as neon. Tenants may propose non-traditional signs, subject to landlord approval, such as vertical signs or sculptural elements. Within the three districts, tenants here will be given the most sqft per lineal frontage.



### Promenade Overlay

In order to enhance the environment and experience of the Central Promenade, tenant signage surrounding this space will be subject to an additional overlay criteria.

### Recommendations:

Signage along the central core will be inspired by a traditional commons, where residents, patrons and local workers may gather. Signage standards will be developed in accordance with architectural framework of the building. In general, the formal qualities of the promenade park will continue into the signage standards. These standards may include a uniform letter height, required plaques, operation signage to appear as acid-etched type on bottom half of glass doors, uniform blade sign brackets and/or uniform max sqft for fascia signs regardless of frontage.

### Fashion Place Tenant Signs

Fashion Place forms the more traditional core of the project, a sheltered pedestrian space with strong built elements along the axials to the perimeter streets.

### Recommendations:

Signage along Fashion Place will emphasize the pedestrian experience with required blade signs. Fascia signs should employ external or halo illumination for individually mounted metal letters. Based on site plan development a uniform sign band height may also enforce the continuity of this East-West axis.

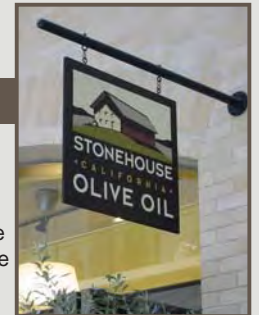


### South Main Tenant Signs

South Main is the closest to the park and residential elements of the plan. It also includes a soft curve where the street intersects with the central promenade.

### Recommendations:

Signage along South Main will have the tightest sqft per lineal frontage and illumination criteria with the goal of creating signage that is compatible with the neighboring uses. To balance these considerations, tenants are encouraged to use specialty signage including window graphics, cast plaques and paving elements,



### Illumination

#### Illumination terms used within this document:

**Ambient illumination:** No lighting fixtures.

**Indirect illumination:** Lighting by adjacent or integrated fixture(s). Lighting is partial.

**Halo illuminated:** Also referred to as reverse pan channel. Concealed internal illumination of letters directed to the mounting surface that creates a "halo" effect.

**External illumination.** Illumination by additional fixtures such as an ornamental gooseneck or concealed uplighting from ground sources.

See illustrations at right:



External illuminated dimensional letterforms



Signage on canopy with concealed internal illumination



Ambient (above) and halo (below)



## 6.9 Retail Signs

### Intent

Retail activates the core of Konterra Town Center East. The combination of retail tenants' signs and graphics defines the character of the project as a pedestrian environment. These standards help to control the quality, size and location of signs within Konterra Town Center East.

### Design Standards

Required:

- **Fascia/Canopy Sign.** Concealed internal illumination or external illumination required. No more than three lines of type per tenant, with a strong recommendation of one line of type only. Max letter height 30". Max sqft: 1.5 sqft per linear frontage. Max quantity: 2.  
If peg-mounted, the individual letters are to be peg mounted minimum 1.5" and maximum of 2" from face of wall. All seams are to be welded and ground smooth.  
When the tenant occupies a corner store location and there is a corner entry/architectural feature, tenant must locate primary identity over the corner entry.  
To ensure variety in the project, adjacent tenants may be required to use different sign types, materials, and colors.

Recommended:

- **Building-mounted Projecting Blade Sign.** Blades signs are encouraged as an important element in activating the side-walk experience. In an exception to the overall standards, tenants bordering project passageways may use one blade sign on the passageway frontage even when there is no public entry.  
Illumination required for all retail blade signs. Max sqft: 9sqft. Type shall occupy no more than 80% of the sign area.
- **Paving graphics:** Signage is required to be within the Tenant's lease line and may not extend beyond the storefront. Sign must be fabricated out of durable, non-slip materials. When vacating tenant space, tenant is to replace flooring to appear as new.

Additional requirements may be included within the tenant district standards to be included in site plan development.

Optional:

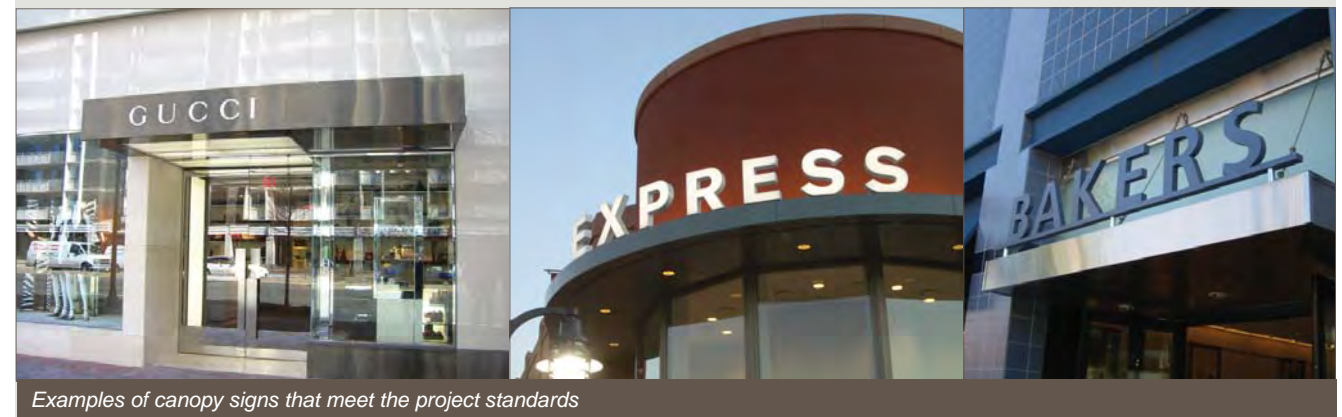
- **Window Graphics:** Vinyl in black, white or frosted hues only. Gold-leaf applications also allowed.
- **Awning signage.** Where compatible with the site architecture canvas awnings may be used for additional tenant signs
- **Menu Boards.** Menu boards to be mounted directly next to main customer entrance doors, enclosed in weather proof, metal enclosure. Menus can be illuminated by a concealed light source integrated into the design of the enclosure. Back-lit menu boards are prohibited. Max sqft: 4sqft.
- **Cast metal wall plaque.** Sign must be mounted next to public entry doors, centered at 50" from the ground plane. Max sqft: 4sqft.



Overall plan showing proposed retail building locations



Examples of fascia signs that meet the project standards



Examples of canopy signs that meet the project standards



Examples of awning signs that meet the project standards



Examples of projecting blade signs that meet the project standards



## 6.10 Residential over Retail Signs

### Intent

Residential over retail activates the retail experience and extends the hours that the project will be used throughout the day. Signage within this typology is smaller in scale and quantity. Illumination restrictions are the key tool with the standards that preserve a quality residential environment.

### Retail Design Standards

Required:

- **Fascia, Canopy or Awning Sign.** Concealed internal illumination or external illumination required. No face lit letters. No more than two lines of type per tenant, with a strong recommendation of one line of type only. Max letter height 24". Max sqft: 1.25 sqft per linear frontage. Max quantity: 2. If peg-mounted, the individual letters are to be peg mounted minimum 1.5" and maximum of 2" from face of wall. All seams are to be welded and ground smooth. To ensure variety in the project, adjacent tenants may be required to use different sign types, materials, and colors.
- **Building-mounted Projecting Blade Sign.** Tenants bordering project passageways may use one blade sign on the passageway frontage even when there is no public entry. Illumination required for all retail blade signs. Max sqft: 9sqft. Type shall occupy no more than 80% of the sign area.

Recommended:

- **Paving graphics:** Signage is required to be within the Tenant's lease line and may not extend beyond the storefront. Sign must be fabricated out of durable, non-slip materials. When vacating tenant space, tenant is to replace flooring to appear as new.

Additional requirements may be included within the tenant district standards to be included in site plan development.

Optional:

- **Window Graphics:** Vinyl in black, white or frosted hues only. Gold-leaf applications also allowed.
- **Menu Boards.** Menu boards to be mounted directly next to main customer entrance doors, enclosed in weather proof, metal enclosure. Menus can be illuminated by a concealed light source integrated into the design of the enclosure. Back-lit menu boards are prohibited. Max sqft: 4sqft.
- **Cornerstone graphics**

See page 6.9 for additional photographic references.

### Residential Design Standards

Required:

- **FasciaSign.** External illumination or ambient. One line of type only. Max letter height 48". Max sqft: 2 sqft per linear frontage. Max quantity: 2. All letters must appear as cast stone. Fascia signs will be integrated in the architectural facade. Preferred locations are within the top 10% of the vertical surface and within corner treatments.

Required for any multi-tenant residential with a shared lobby on street level, one of the following signs:

- **Building-mounted Projecting Blade Sign** No illumination required. Max sqft: 9sqft.



Overall plan showing proposed residential over retail building locations

- **Canopy Sign.** Site name and/or street address integrated into an architectural canopy with dimensional letters. Ambient illumination recommended.

Recommended:

- **Paving graphics:** Signage is required to be within the 6 feet of the site entry and may not extend beyond the entry bay. Sign must be fabricated out of durable, non-slip materials.

Optional:

- **Window Graphics:** Vinyl in white or frosted hues only. Located at 60" from ground plane. Max type height: 2 in.
- **Wall plaque:** Cast metal construction mounted to site architecture next to doorway, centered on 60" from ground plane. No visible fasteners.
- **Call box**
- **Leasing/Management Office Signs**



Examples of residential fascia signs that meet the project standards



Residential blade sign



Examples of residential canopy signs that meet the project standards



Examples of residential paving graphics that meet the project standards



## 6.11 Office over Retail Signs

### Intent

Office and retail are complimentary uses whose prime hours benefit the other by creating a dynamic core with activity throughout the day. Signage standards help to further foster this mutually beneficial relationship by establishing clear boundaries for the two while ensuring good visibility for both.

### Retail Design Standards

Required:

- **Fascia or Canopy Sign.** Concealed internal illumination or external illumination required. No face lit letters. No more than two lines of type per tenant, with a strong recommendation of one line of type only. Max letter height 24". Max sqft: 1.25 sqft per linear frontage. Max quantity: 2. If peg-mounted, the individual letters are to be peg mounted minimum 1.5" and maximum of 2" from face of wall. All seams are to be welded and ground smooth. To ensure variety in the project, adjacent tenants may be required to use different sign types, materials, and colors.
- **Building-mounted Projecting Blade Sign.** Tenants bordering project passageways may use one blade sign on the passageway frontage even when there is no public entry. Illumination required for all retail blade signs. Max sqft: 9sqft. Type shall occupy no more than 80% of the sign area.

Recommended:

- **Paving graphics:** Signage is required to be within the Tenant's lease line and may not extend beyond the storefront. Sign must be fabricated out of durable, non-slip materials. When vacating tenant space, tenant is to replace flooring to appear as new.

Additional requirements may be included within the tenant district standards to be included in site plan development.

Optional:

- **Window Graphics:** Vinyl in black, white or frosted hues only.
- **Menu Boards.** Menu boards to be mounted directly next to main customer entrance doors, enclosed in weather proof, metal enclosure. Menus can be illuminated by a concealed light source integrated into the design of the enclosure. Back-lit menu boards are prohibited. Max sqft: 4sqft.
- **Cornerstone graphics**

See page 6.9 for additional photographic references.

### Office Design Standards

Required:

- **FasciaSign.** External illumination or ambient. One line of type only. Max letter height 48". Max sqft: 2 sqft per linear frontage. Max quantity: 2. All letters must appear as cast stone. Fascia signs will be integrated in the architectural facade. Preferred locations are within the top 10% of the vertical surface and with corner treatments.

Required for any multi-tenant office with a shared lobby on street level, one of the following signs:

- **Canopy Sign or Enhanced Entry.** Located directly above entry doors, this sign street address. Sign may be integrated into an architectural canopy with dimensional letters or use alternative design elements to create a singular entry statement. Illumination recommended.



Overall plan showing proposed office over retail building locations

Optional:

- **Tenant Listings.** Max letter height: 5 in. All tenant names must be listing in a the same color in a uniform listing style.
- **Projected Vertical Blade Sign. Alternate for Canopy/Entry Sign.** Location coordinated with tenant below. Max sqft 20sqft. Bottom of sign must be 4ft above vertical terminus of ground floor tenant. No face lit letters. Ambient lighting recommended.
- **Window Graphics:** Translucent vinyl only.
- **Wall plaque:** Cast metal construction mounted to site architecture next to doorway, centered on 60" from ground plane. No visible fasteners.
- **Call box**
- **Leasing/Management Office Signs**



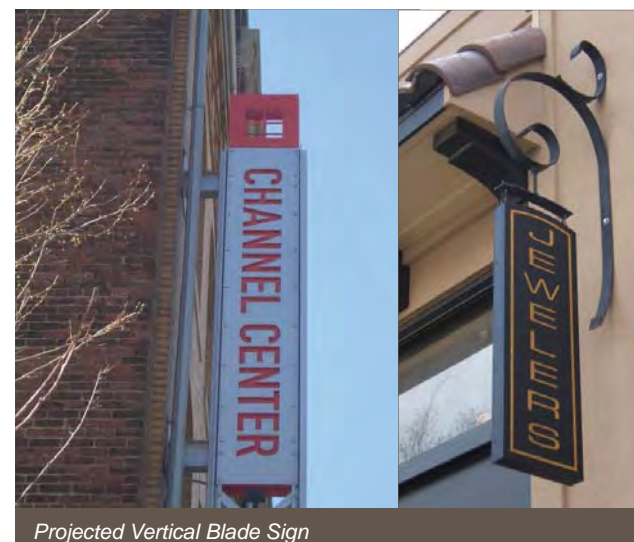
Office fascia signs sign



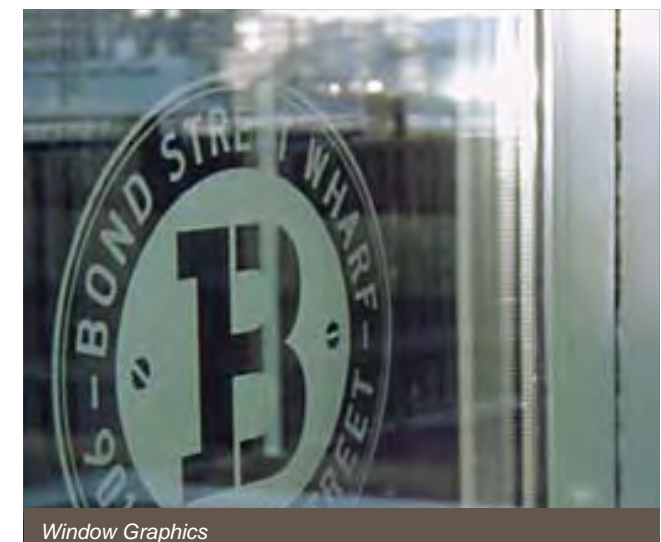
Wall plaque with and without tenant listings



Examples of enhanced office entry and canopy signs signs that meet the project standards



Projected Vertical Blade Sign



Window Graphics



## 6.12 Department Store Signs

### Intent

Anchor tenants are to be classified so by the Landlord and based on a uniform standard of measurable square feet. They will have special consideration for signage.

### Department Store Standards

Required:

- **Perimeter Facing Fascia Sign.**
  - Option 1: Dimensional letters with concealed illumination integrated into the architectural facade, such as cove lighting.
  - Option 2: Reverse pan channel halo lit dimensional letters.
  - Option 3: Day/Night Plexface letter to appear black in daylight and illuminate white at night. All letter located in consideration of entryways and architectural detailing.
  - No more than two lines of type per tenant, with a strong recommendation of one line of type only. Max letter height for one line of copy: 6ft. Max letter height for two lines of copy: 4 ft. Max sqft: None. Max quantity: 2. One per frontage. Signs may not be located on adjacent corners. Recommended sign location: upper 25% of the perceived facade.

- **Internal Fascia or Canopy Sign.** Concealed internal illumination or external illumination required. Face lit letters not recommended. No more than two lines of type per tenant, with a strong recommendation of one line of type only. Max letter height 3 ft. Max sqft: none. Max quantity: 3. One per frontage. Signs must be located above public entry locations.

Recommended:

- **Paving graphics:** Signage is required to be within the Tenant's lease line and may not extend beyond the storefront. Sign must be fabricated out of durable, non-slip materials. When vacating tenant space, tenant is to replace flooring to appear as new.
- **Building-mounted Projecting Blade Sign.** Along internal roads, anchor tenants may use one blade sign per frontage where anchor facade is less than 20 feet from the curbline. Anchor tenants bordering project passageways may use one blade sign per passageway frontage even when there is no public entry. This may be encouraged in later phases, as density requires in the transition areas between parking structures and the project interior. Illumination required for all retail blade signs. Max sqft: 20sqft. Only height dimension may exceed 3 feet, allowing for a vertical blade option within this sighttype. Type shall occupy no more than 80% of the sign area.

Additional requirements may be included within the tenant district standards to be included in site plan development.

Optional:

- **Perimeter Facing Canopy or Awning Sign.** When anchor tenant have a public entry facing the project perimeter, an additional sign may be located directly above each entry. This allowance is in addition to the fascia sign on the same frontage. Concealed internal illumination or external illumination required. No face lit letters. One line of type only. Max letter height 18in. Max sqft: 12sqft. Max quantity: 1



Overall plan showing proposed anchor retail building locations

per entry with a maximum of 2 per frontage. Signs must be located above public entry locations.

- **Internal Awning Signage :** Canvas awnings with tenant name silk-screened or embroidered type on the vertical surface of awning may be permitted where compatible with site architecture. Type not to exceed 60% of the vertical sign band. Type max height: 8 in. No illumination. In special cases, tenant may apply to use a strong awning treatment in lieu of the internal fascia/canopy sign requirement.
- **Window Graphics:** Vinyl in black, white or frosted hues only. Max sqft: not to exceed 10% of total window area. Spandrel glass cannot be used in the calculation of window area.
- **Cast metal wall plaque.** Sign must be mounted next to public entry doors, centered at 50" from the ground plane. Max sqft: 4sqft.
- **Cornerstone graphics.** Cast stone type may be built into a cornerstone element. Additional type may include the year built and any dedication information approved by landlord.
- **Parking Entry Identity.** Based on project development, anchor tenant identity signage may be incorporated into parking entry signage when the majority of retail parking is located with structured parking. Max letter height: 2 ft. Internal illumination recommended.

See page 6.9 for additional photographic references.



Examples of perimeter facing fascia signs that meet the project's standards



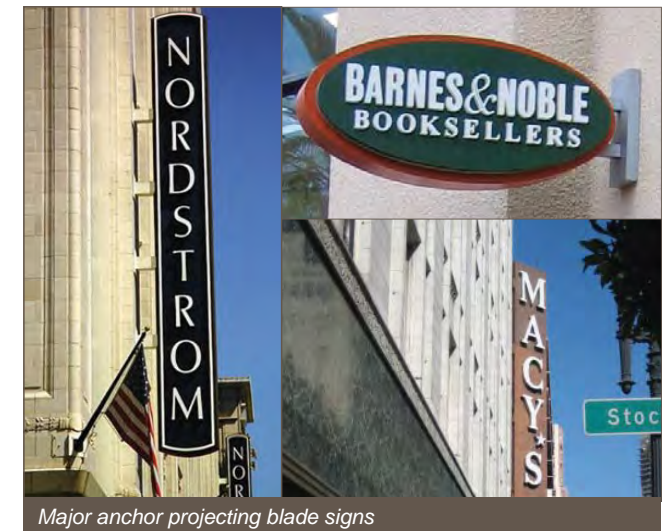
Canopy Signs



Internal Fascia Sign



Cast metal wall plaque



Major anchor projecting blade signs



## 6.13 Office Signs

### Intent

Office buildings located along the project perimeter are an important element in defining the mixed use character of the site. Their signage standards bring a commercial voice to the mix that enriches the whole environment.

### Office Design Standards

Required:

- **Fascia Sign.** External illumination or ambient. One line of type only. Max letter height 48". Max sqft: 2 sqft per linear frontage. Max quantity: 2. Fascia signs will be integrated in the architectural facade. Fascia sign should be located to maximize visibility of the site. Type may include the site name, site address or one major tenant. Preferred locations are within the top 10% of the vertical surface and with corner treatments.
- **Canopy Sign or Enhanced Entry Sign.** Located directly above entry doors or integrated into architectural design of the entry, this sign addresses the pedestrian streets. Sign may be integrated into an architectural canopy with dimensional letters or use alternative design elements to create a singular entry statement. Illumination recommended.

Optional:

- **Tenant Listings.** Max letter height: 5 in. All tenant names must be listing in a the same color in a uniform listing style. Building-mounted tenant listing may be use on the street frontage. A freestanding tenant listing sign is permitted on the site exterior along the parking frontage. Max quantity: 1 sign per public entry.
- **Projected Vertical Blade Sign. Alternate for Canopy/Entry Sign.** Location coordinated with tenant below. Max sqft 20sqft. Only height dimension may exceed 3 feet, allowing for a vertical blade option within this signtype. Bottom of sign must be 8ft above ground plane. Intenal illumination recommended.
- **Entry Monument.** May include site name and address. No tenant names or logos. Exceptions may be granted for one major anchor. Max height: 6ft. Max letter height: 10in. Max sqft: 50sqft. Maybe be located along parking entry sequence or entry plaza. Sculptural address may exceed dimensions included here.
- **Window Graphics:** Translucent vinyl only.
- **Wall plaque:** Cast metal construction mounted to site architecture next to doorway, centered on 60" from ground plane. No visible fasteners.
- **Call box**
- **Leasing/Management Office Signs**



Overall plan showing proposed office building locations



Examples of fascia signs signs that meet the project standards



Wall plaques with and without tenant listings



Examples of enhanced office entry and canopy signs signs that meet the project standards



Window graphics



Entry monuments that meet the project standards



## 6.14 Residential Signs

### Intent

Office buildings located along the project perimeter are an important element in defining the mixed use character of the site. Their signage standards bring a commercial voice to the mix that enriches the whole environment.

### Residential Design Standards

Required for any multi-tenant residential with a shared lobby on street level:

- **Canopy Sign.** Site name and/or street address integrated into an architectural canopy with dimensional letters. Ambient illumination recommended. Type is not required on the canopy, if a unique architectural canopy element is used. If type is not used on the canopy, a window graphics or wall plaque must identify the site address.

Recommended for residential not included above:

- **Entry Monument.** May include site name and address. Max height: 8ft. Max letter height: 10in. Max sqft: 50sqft. Should be located along parking entry sequence. Ambient or external illumination only. A lantern element may be incorporated as an additional lighting element.

Optional for any multi-tenant residential with a shared lobby on street level::

- **Fascia Sign.** External illumination or ambient. One line of type only. Max letter height 48". Max sqft: 2 sqft per linear frontage. Max quantity: 1. All letters must appear as cast stone. Fascia signs will be integrated in the architectural facade. Preferred locations are within the top 10% of the vertical surface and with corner treatments.
- **Window Graphics:** Vinyl in white or frosted hues only. Located at 60" from ground plane. Max type height: 2 in.
- **Paving graphics:** Signage is required to be within the 6 feet of the site entry and may not extend beyond the entry bay. Sign must be fabricated out of durable, non-slip materials.
- **Wall plaque:** Cast metal construction mounted to site architecture next to doorway, centered on 60" from ground plane. No visible fasteners.
- **Call box**

Optional for all residential:

- **Cornerstone graphics.** Cast stone type may be built into a cornerstone element. Additional ype may include the year built and any dedication information approved by landlord.
- **Leasing/Management Office Signs**

See page 6.10 for additional photographic reference



Overall plan showing proposed residential building locations



Examples of entry monuments that meet the project standards



Examples of canopy signs that meet the project standards



Examples of cornerstone sign



Examples of wall plaques



6.15 Prohibited Signs and Additional Standards

**Intent**

The most important goals of the sign standards are a.) controlling the overall quality of signage within the project and b.) to give the project and sites within good visibility. Both goals can be met within the set of prohibitions listed here.

These standards apply to all signs within Konterra Town Center East.

**Project and Sign Standards**

**Prohibited Sign Types:**  
A full set of standards will be included in the forthcoming tenant signage criteria. The following types will be prohibited in further evolutions of the project:

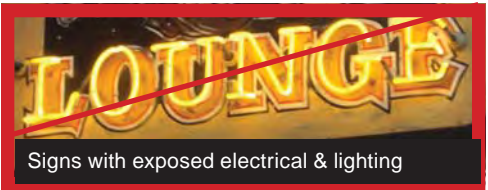
- a. Illuminated sign boxes, also known as cabinet signs.
- b. Interior signage meant to be viewed from the exterior of the building.
- c. Signs with tag lines, slogans, phone numbers, service description, or advertising.
- d. Illuminated canvas awnings. Domed, waterfall, concave or convex awnings. Vinyl or plastic awnings.
- e. Signs with exposed raceways, conduit, junction boxes, transformers visible lamps, tubing, or neon crossovers of any type.
- f. Rotating and flashing signs.
- g. Pole signs and other signs with exposed structural supports not intended as a design element except for code-required signs.
- h.. Signs attached, painted on, or otherwise affixed to trees, other living vegetation, landscaping or natural materials.
- i. Signs attached, painted or otherwise affixed to tents or umbrellas, however, such signs may be permitted in conjunction with special design review by the Landlord.
- j. Balloons and inflatable signs.
- k. Any signs including freestanding signs advertising the availability of employment opportunities.
- l. Signs which emit sound, odor or visible matter.
- m. Signs which bear or contain statements, words or pictures of an obscene, pornographic or immoral character, as deemed so by the Landlord.
- n. Fluorescent or reflective sign colors.
- o. Simulated materials, i.e., wall covering, paper, cardboard or foam, or Sentra.
- p. Signage with back plates behind letters is prohibited.



Surface mounted, box cabinet signage



Plexi-face channel letters



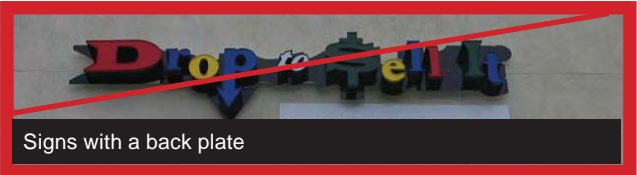
Signs with exposed electrical & lighting



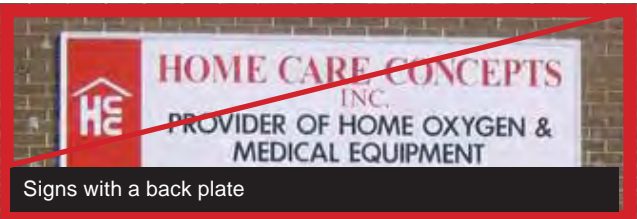
Signs that use LED messaging



Formed Plastic



Signs with a back plate



Signs with a back plate



Multiple tenant logos on a monument

q. Signs with LED type or other animated and/or digital projection of type.

**Additional standards:**  
Awnings: Awnings should reflect the overall facade organization of a building. Awnings should be located within the building elements which frame storefronts. Important architectural details should not be concealed by awnings, canopies or marquees. Awnings should be designed to project over individual window and door openings.

Awnings on a multiple-storefront building should be consistent in character, scale, and location, but need not be identical.

Open sided awning with a proportional valance are recommended, also known as shed awnings.

Scalloped or decorative valences are strongly discouraged.

Mixed-use: When a building contains multiple tenants vertically and horizontally, the signs should relate well to each other in terms of height and proportion. Maintaining a degree of uniformity among these characteristics reinforces the building's facade composition while still retaining each business's identity.

External Illumination: If the sign can be illuminated by a projected light (e.g., gooseneck), this is usually preferred. Ex-

ternal illumination allows more room for the sign to integrate with the building's architecture. External lighting emphasizes the continuity of the structure's surface with signs becoming an integral part of the facade. Light fixtures are preferably small and must meet the same standards of compatibility with the site architecture as all other signage elements

Fascia signs, also known as wall mounted signs: Fascia signs should be sized to fit within existing friezes, lintels, span-drels, and other such features and not extend above, below, or beyond them. Typically, wall-mounted signs should be centered on horizontal surfaces (e.g., over a storefront opening).



## 7.0 Lighting Standards

The Lighting Standards Report summarizes the lighting design objectives, quality, and solutions recommended by Francis Krahe & Associates Inc. as a part of the design team for this project. This report identifies the scope of work, defines design guidelines & objectives, summarizes design criteria applied in the development of community lighting standards and illustrates the roadway light levels proposed under the DSP submittal. These lighting standards define the recommended limits of lighting design solutions for future implementation. This document summarizes our approach to the project, and serves as the standards for future detailed design of individual project components for Konterra -Town Center East.



### 7.0.1 Project Scope

The Lighting scope of work encompasses the exterior lighting for public spaces of the project including:

#### Streetscape Lighting along Perimeter, Collector and Local Streets.

- Roundabout
- Promenade
- Town Square
- On Grade Parking

#### Pedestrian Lighting

- Community Parks
- Neighborhood Parks
- Passages

#### Landscape Lighting Features

- Trees

#### Architectural Lighting Features

- Parking Structures

### 7.0.2 Design Criteria

The following design criteria will apply to the analysis of proposed design solutions for each of the project scope components:

Architectural design character and style

Illuminance (footcandels)

Color Quality

Energy Efficiency and Efficacy

Maintenance Standards

Initial and Operating Costs

Availability

Durability

Safety

Codes and Guidelines, (Appropriate and safe illumination in accordance with the following codes and standards):

ADA

State of Maryland Codes

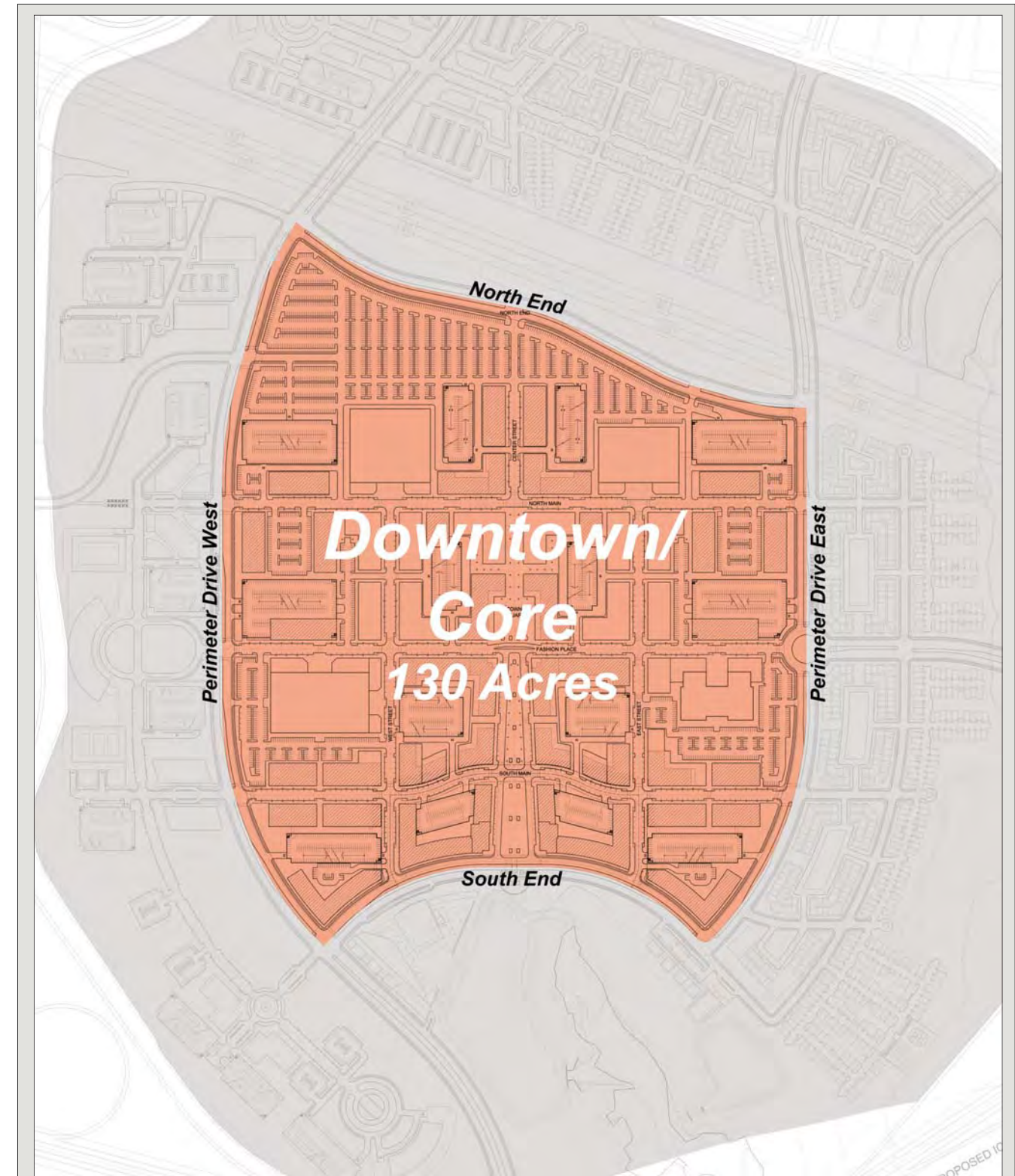
National Electric Code (NEC)

Illuminating Engineering Society (IES)

Maryland Department of Transportation Standards

Local Codes

**Note:** The first permit plans for sites located within the adjoining blocks that front on the Main Streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of trees, street furniture and lighting fixtures for that street on which the project site is fronting.



Konterra Town Center East site area. Downtown/core highlighted in red, environs highlighted in beige.



## 7.1 Design Intent

### Identity

Light defines the image at night, and will create a distinct community and town center identity through the illumination of streets, plazas, monuments, parks & landscape, building exteriors & signage.

### Cohesion

Light will unite the various commercial districts and residential components by creating unique light qualities to define subdistricts for the various activity areas.

### Architecture

Light will enhance, feature and complement the distinct architectural components within each district, street, park and plaza to create landmarks.

### Wayfinding

Light will define the entrances to the Community, the major streets and intersections, retail, residential, and pedestrian pathways.

### Durability

Light fixtures will be attractive and durable to create a sense of quality, dignity and integrity to the community.

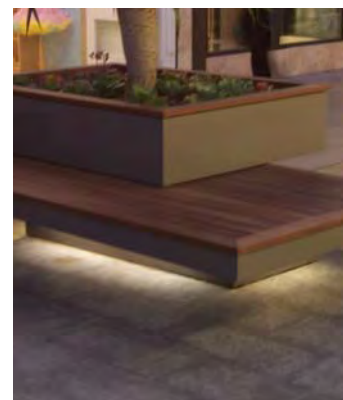
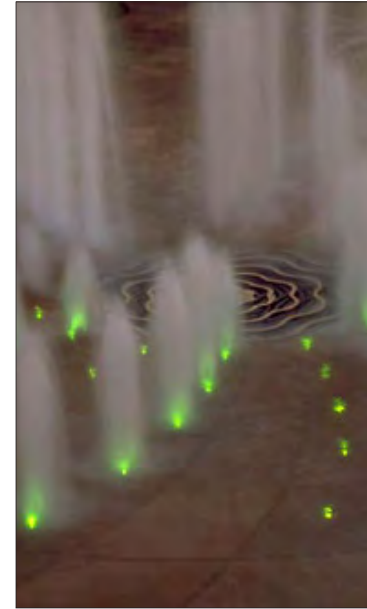
### Commercial

Light fixtures and pattern will create sophisticated quality.

### Efficiency

Light will be generated by efficient light sources to save energy and minimize operating costs.

**Note:** Pole mounted perimeter street and interior parking area fixtures, excepting light fixtures provided by and subject to BG&E requirements, are to be light fixtures with full cut-off optics. All light fixtures located within service court areas are also to be fixtures with full cut-off optics to the extent practicable. Decorative internal street and plaza poles, decorative building-mounted fixtures, and other fixtures used for aesthetic effect or security, including those that may be submerged in water features, will be light fixtures with cut-off optics, to the extent practicable and consistent with the District Council finding.



Images of Lighting Fixtures.

### Environment

Light will be generated from environmentally friendly solutions which limit light pollution, or the disposal of harmful waste products.

### Security

Light will create a sense of safety and security throughout the community with clear identification of circulation, gathering spaces, and parking facilities.

### Visibility

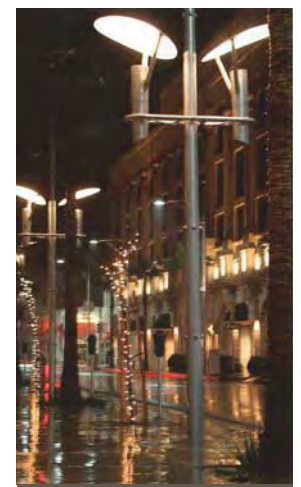
The community entrances and Town Square should be prominent at night to attract and welcome the public. We believe this environment should be designed from the user's perspective, with a dedicated effort to maximizing the experience. Light quality affects our impression of a place and should add to the sense of quality.

### Celebration

The Town Square, the Promenade Center Street and the Pedestrian Passage should become the focus of the community. The adjacent buildings should express this opportunity with excitement and interest and the lighting should support and enhance the opportunity to host public events.

### Domestic

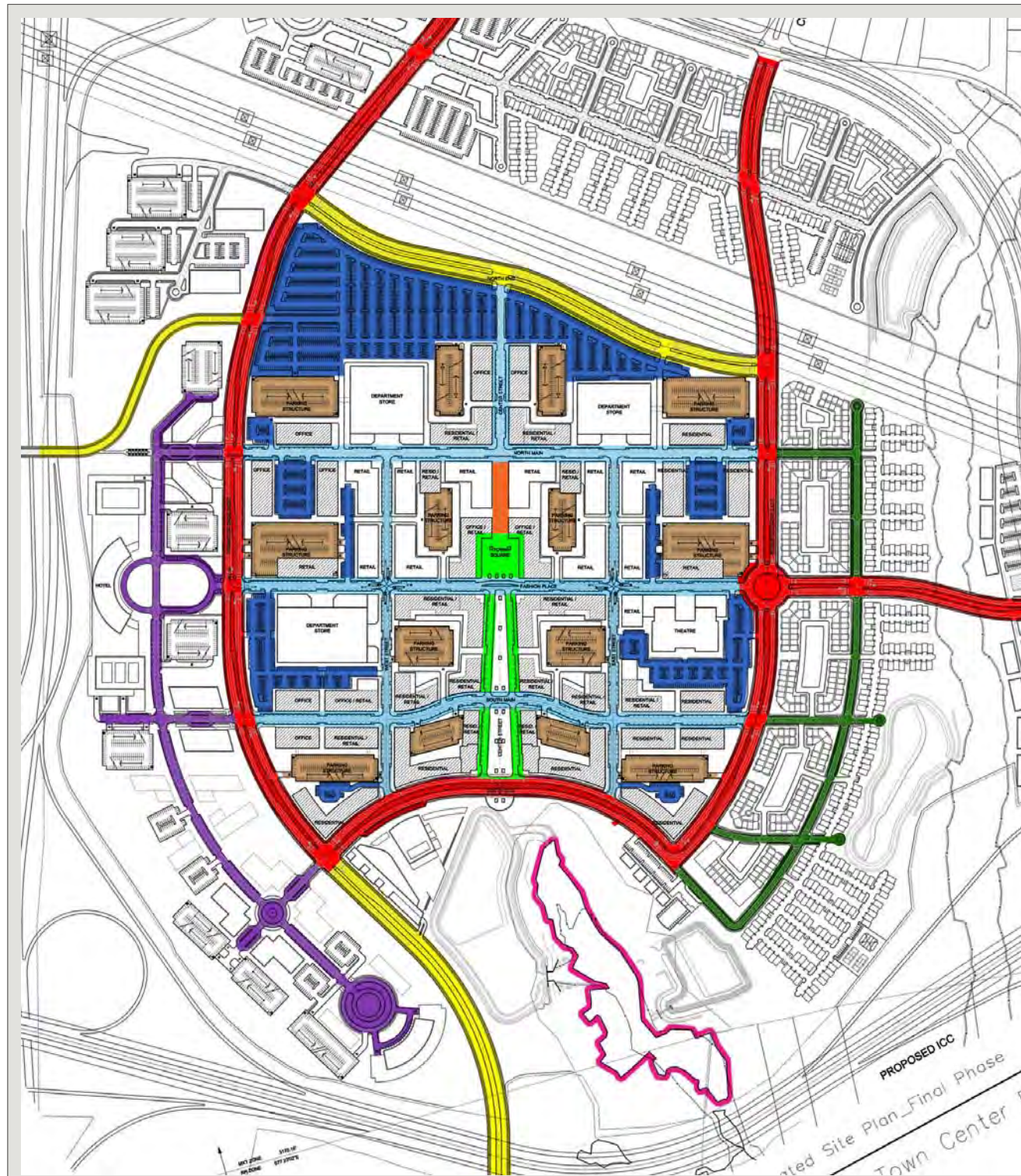
The light intensity, character, and quality should express the residential nature of the community. Light within streets, parks, exterior of residential, and neighborhoods should be extremely comfortable and convey the special character of Konterra Town Center East.



Images of Lighting Fixtures.



## 7.2 Design Concept



Streetscape Diagram: showing locations for various design approaches relative to street typology.

### Perimeter Roads (Major Site Access/Circulation)

Establish the sense of arrival to the project with a distinct design character currently proposed under the contemporary style with a traditional option. Lighting will include canopy tree uplifts in the median and power for seasonal lights in trees along pedestrian walkways. All intersections and roundabouts will use double head poles for both pedestrian and traffic with the exception that poles immediately adjacent to residential buildings will be single headed.

### Collector Roads (Secondary Site Access/Circulation)

Continued use of the Perimeter Road poles selected to reflect the character of this unique community with the exception that poles will be single headed as pedestrian traffic in these areas will be lighter. There will be no canopy tree uplifts in the medians along these roads.

### Main Streets

Color and intensity of the lighting fixtures used along Main Street areas should define the community and continue the use of contemporary style streetscape poles with the option to hold seasonal color baskets. All intersections and roundabouts will use double head poles for both pedestrian and vehicular traffic. Retail and residential areas will continuously use contemporary double and single head poles with banner option.

### Promenade & Town Square

Light quality and design character will define and reinforce the architectural character and pedestrian experience within the retail and residential areas, plazas, and parks. Light fixtures, color and intensity should define the plazas for public entertainment activities and support the project's opportunity to host public events. Lights for special events has been provided at four corners with 30' mask which will hold seasonal and performing light.

### Pedestrian Passage

Lighting will define the public passage ways and create light patterns at night through a palette of decorative fixtures, concealed sources, and cable lights with hung pendants.

### Office & Hotel (Future Environ Development)

The identity of this district is supported with double and single head contemporary poles.

### Park Pedestrian Path

Provide minimum IES recommended light levels along pedestrian pathways to enhance 24 hour environment for community activities within this mixed-use urban development.

### Parking Structure

Use indirect pendants at open areas around the structure perimeter and surface mounted fixtures within interior areas.

### Surface Parking

Provide minimum IES recommended light levels for surface parking to create a safe parking environment.

### Residential Street

Provide minimum IES recommended light levels along Residential Street to enhance 24 hour environment for community activities within the neighborhood.



# 7.2 Design Concept

## 7.2.1 Calculation Location Diagram

**Perimeter Roads**

A-1

**Perimeter Drive West**  
Type SB, Double Head 30'/14' Pole, 260' o.c.  
Lamp: 250W HPS 120V Mastercolor Metal Halide.

A-2

**Perimeter Drive East, Entry Roundabout**  
Type SB, Double Head 30'/14' Pole, 260' o.c.  
Lamp: 250W HPS 120V Mastercolor Metal Halide.

A-3

**Perimeter Road and Residential Road Intersection**  
Type SB-1, Single Head 30' Pole, 260' o.c.  
Lamp: 250W HPS 120V Mastercolor Metal Halide.

**Collector Roads**

B-1

**Collector Streets - North End**  
Type SB-1, Single Head 30' Poles at 240' o.c.  
Lamp: 250W HPS 120V

B-2

**Collector Streets - Perimeter Drive West**  
Type SB-1, Single Head 30' Poles at 240' o.c.  
Lamp: 250W HPS 120V

**Main Streets**

C

**Main Street - Fashion Place & East Street**  
Type SC-1, Double Head 22'/12' Pole, 160' o.c.  
Lamp: 100W white sun/250W/120V

C-1

**Main Street - North Main & Pedestrian Promenade**  
Type SC-1, Double Head 12'/22' Pole, 160' o.c., Selux Hi/ Low Ritorno.  
Lamp: 100W/250W(120V) Mastercolor Metal Halide

C-2

**Main Street - South Main & Center Street Couplet - Residential**  
Type SA-1, Single Head 22' Pole, 160' o.c.  
Lamp: 250W(120V) Mastercolor Metal Halide  
  
Type SF Custom Pole

**Promenade & Town Square**

D

**Promenade & Town Square - Vehicular**  
Type SC-1, Double Head 22'/12' Pole, 120' o.c.  
Lamp: white sun/250W/120V  
  
Type SC-3, Double Head, 14' Pole, 45' o.c.  
Lamp: 100W/120V White Son HID

D-1

**Promenade & Town Square - Pedestrian**  
Type SF-1, Special Event Pole  
  
Type SC-3, Double Head, 14' Pole, 45' o.c.  
Lamp: 100W/120V White Son HID

**Pedestrian Passage**

E

**Pedestrian Passage**  
Type SC-1, Double Head 12'/22' Pole, 120' o.c.  
Lamp: 100W/250W/120V

**Office & Hotel (Future Environ Development)**

F

**Office & Hotel (Phase 2/3)**  
Type SA-1, Single Head 22' Poles, 160' o.c.  
Lamp: 250W/120V

**Park Pedestrian Path (Future Environ Development)**

G

**Park Pedestrian Path**  
Type SD-2, 14' Pole, 60' o.c. Poulsen/Selux.

**Parking Structure**

H

**Parking Structure**  
Type SE-3, Indirect Pedestrian.  
Type SE-4, Direct Surface Mount

**Surface Parking**

I

**Surface Parking**  
Type SE, 20' Pole  
Lamp: 250W HPS

**Residential Road Intersection (Future Environ Dev.)**

J

**Residential Road Intersection**  
Type SB-1, 14' Poles at intersections. Poulsen / Selux  
Lamp: 250W, HPS, 120V



Photometric location diagram.



## 7.3 Perimeter Roads

### 7.3.1 Perimeter Drive West

#### Light Level Summary

#### **A-1** Perimeter Drive West

Roadway Cross Section at Perimeter Drive West ( 98' wide x 500' section).

Average (fc) values across Roadway at Main Entry and Perimeter Drive (1.38fc).

Canopy Tree uplights located in median for all trees and extend to include all canopy trees at intersections. Contribution from supplemental landscape lighting fixture, will represent an additional 15-20 % of the total in this area.

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

The light levels are based on the recommended IES minimum.

#### Reference Design Criteria:

Type SB, 30'/14' Doublehead pole, 260' o.c.

Lamp: 250w. HPS/100w Mastercolor Metal Halide

Type SG, Canopy Tree Uplight.

1.3-1.5 fc Average.



Uplight: Erco Tesis



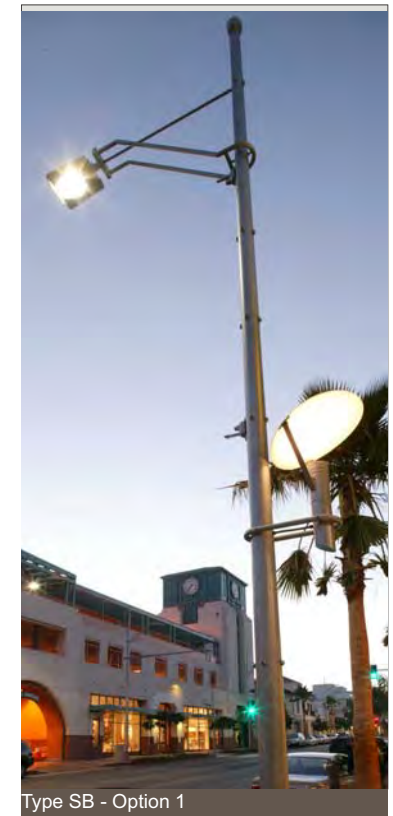
Location map for reference



Plan showing typical locations and light levels for Perimeter Drive West.



Section A-1: Showing typical Perimeter Drive West lighting approach.



Type SB - Option 1



Type SB - Option 2



# 7.3 Perimeter Roads

### 7.3.2 Perimeter Drive East Entry Roundabout

#### Light Level Summary

A-2

Perimeter Drive East - Entry Roundabout

Roadway Cross Section at Entry Roundabout ( 108' radius).

Average (fc) values across Roadway at Main Entry and Perimeter Drive (1.44fc).

Canopy Tree uplights located in median for all trees and extend to include all canopy trees at intersections. Contribution from supplemental landscape lighting fixture, will represent an additional 15-20 % of the total in this area.

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.


The light levels are based on the recommended IES minimum.

#### Reference Design Criteria:


Type SB, 30'/14' Doublehead pole, x(4) poles  
Lamp: 250w. HPS/100w Mastercolor Metal Halide

Type SG, Canopy Tree Uplight.

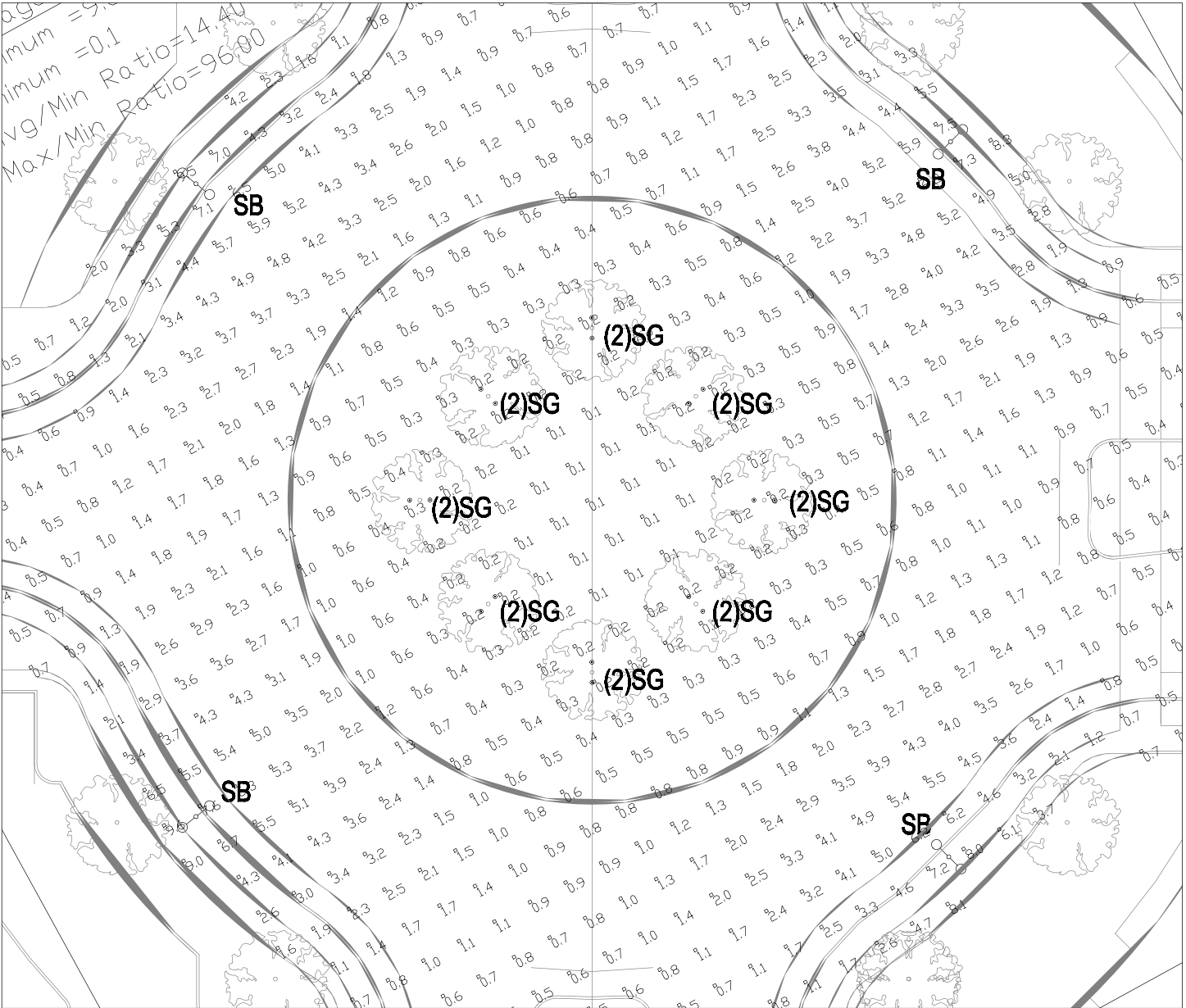
1.3-1.5 fc Average.




Uplight: Erco Tesis




Location map for reference




Plan showing typical locations and light levels for Perimeter Drive East - Entry Roundabout.



Section A-2: Showing Perimeter Drive East - Entry Roundabout lighting approach.



Type SB - Option 1



Type SB - Option 2



# 7.3 Perimeter Roads

### 7.3.3 Perimeter Road & Future Residential Road Intersection

**Light Level Summary**

A-3

**Perimeter Road & Future Residential Road Intersection**

Roadway Cross Section at North Collector Street ( 98' wide x 300' section).


Average (fc) values across Across Collector Street intersection (1.91 f.c)

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

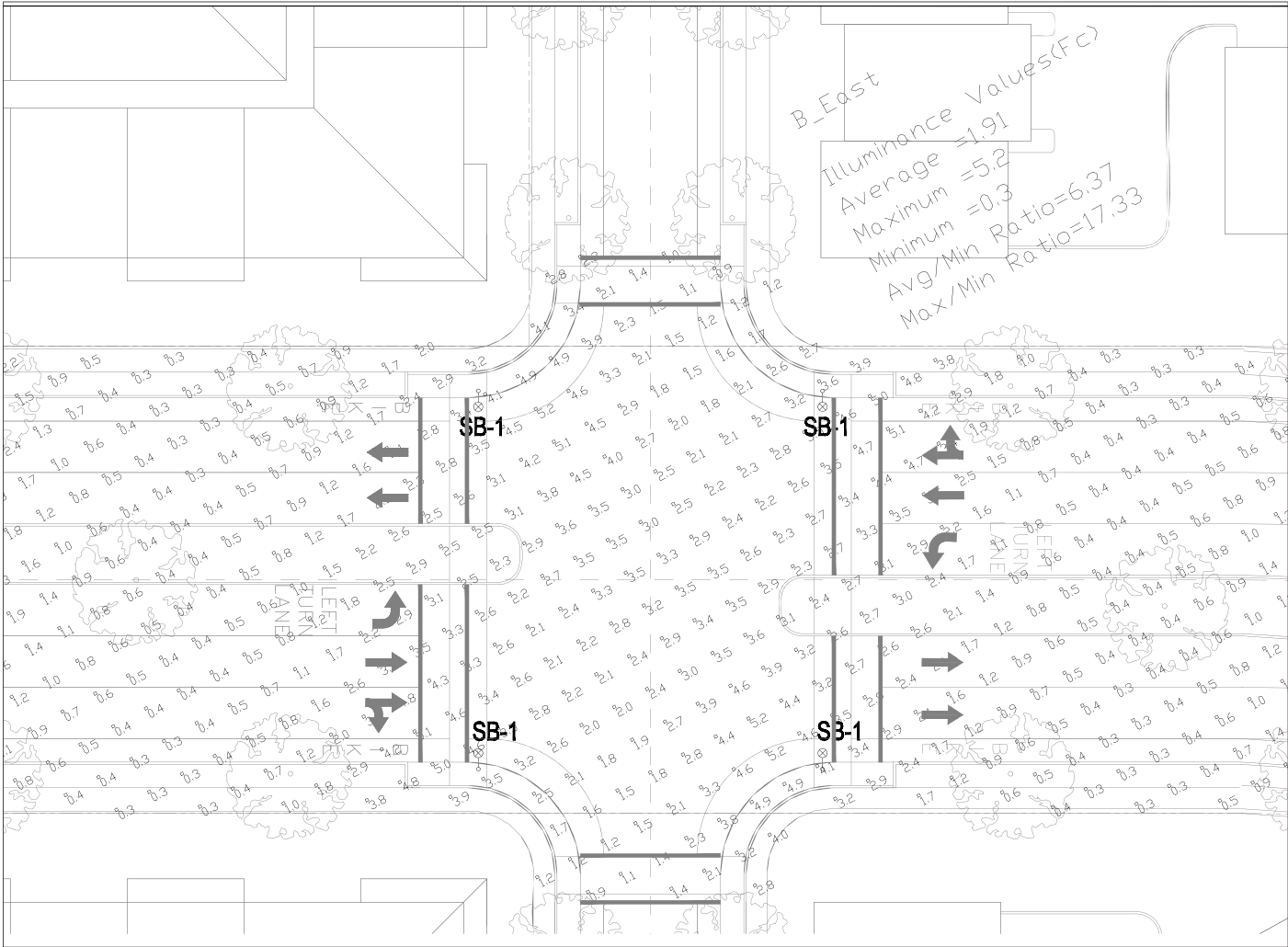
The light levels are based on the recommended IES minimum.

**Reference Design Criteria:**

Type SB-1, 30' Single head pole, 4x @ intersection.  
Lamp: 250w. HPS, 120V  
1.0-1.3 fc Average / Street  
1.8 fc Average / Intersection



Location map for reference



Plan showing typical locations and light levels for Perimeter Road and future residential road intersections.



Section A3: Showing typical design approach for Perimeter Road and future residential road intersections.





# 7.4 Collector Roads

### 7.4.1 Collector Road - North End

**Light Level Summary**

**B-1 Collector Street - North End**

Roadway Cross Section at East Collector Street ( 100' wide x 500' section).

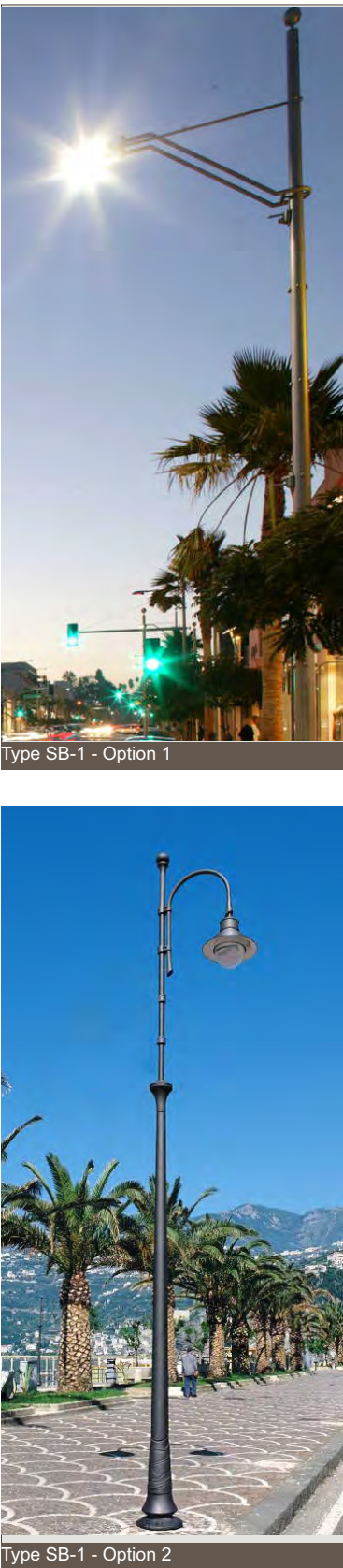
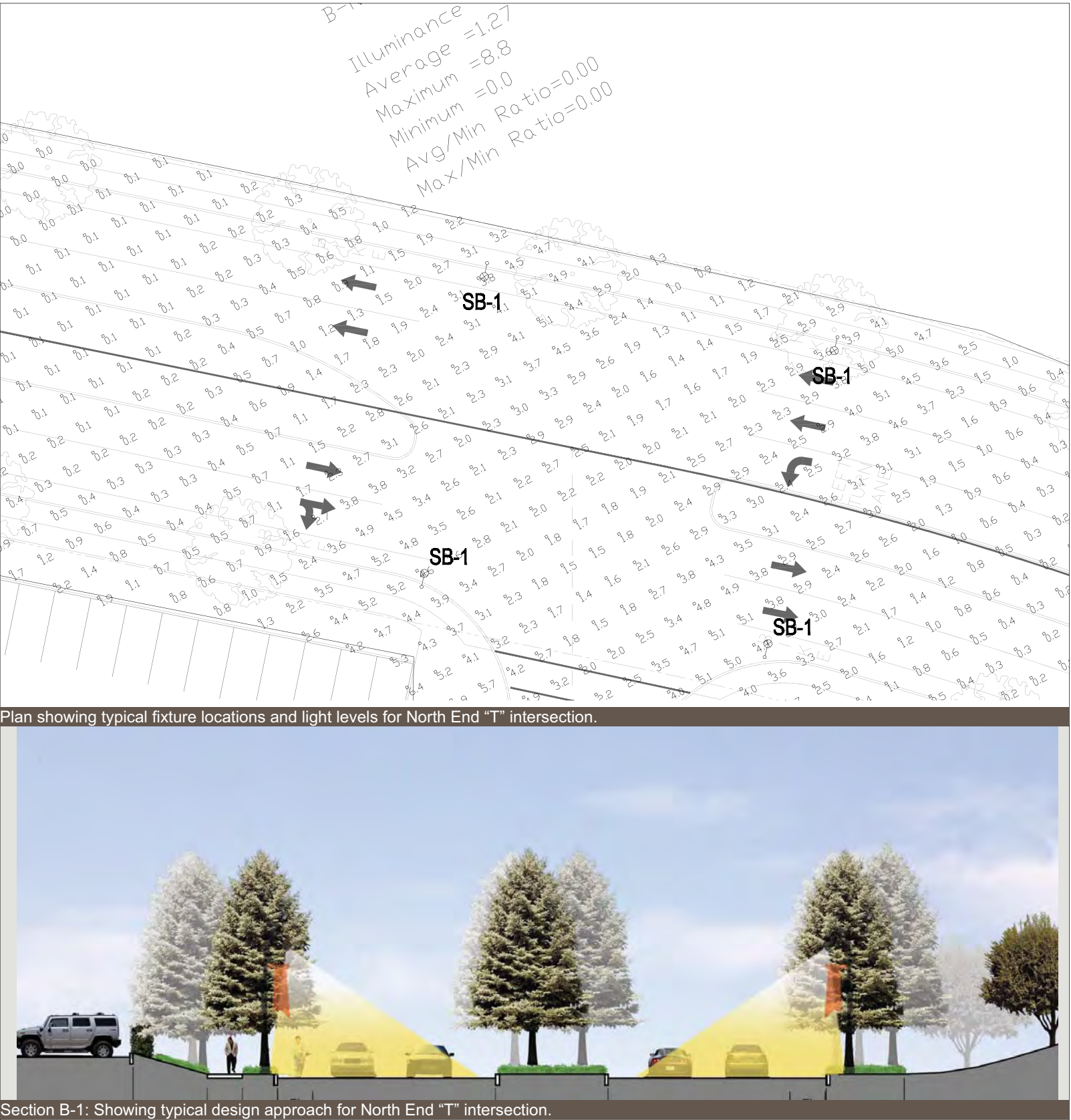
Average (fc) values across Across Collector Street (1.27 fc).

**Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.**

**The light levels are based on the recommended IES minimum.**

**Reference Design Criteria:**

Type SB-1, 30' Single head pole, 240' o.c.  
Lamp: 250w. HPS, 120V  
0.9-1.0 fc Average.





## 7.4 Collector Roads

### 7.4.2 Perimeter Drive West-Collector Road

#### Light Level Summary

#### **B-2** Perimeter Drive West - Collector Road

Roadway Cross Section at South Collector Street ( 100' wide x 500' section).

Average (fc) values across Across Collector Street (1.37 fc).

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

The light levels are based on the recommended IES minimum.

#### Reference Design Criteria:

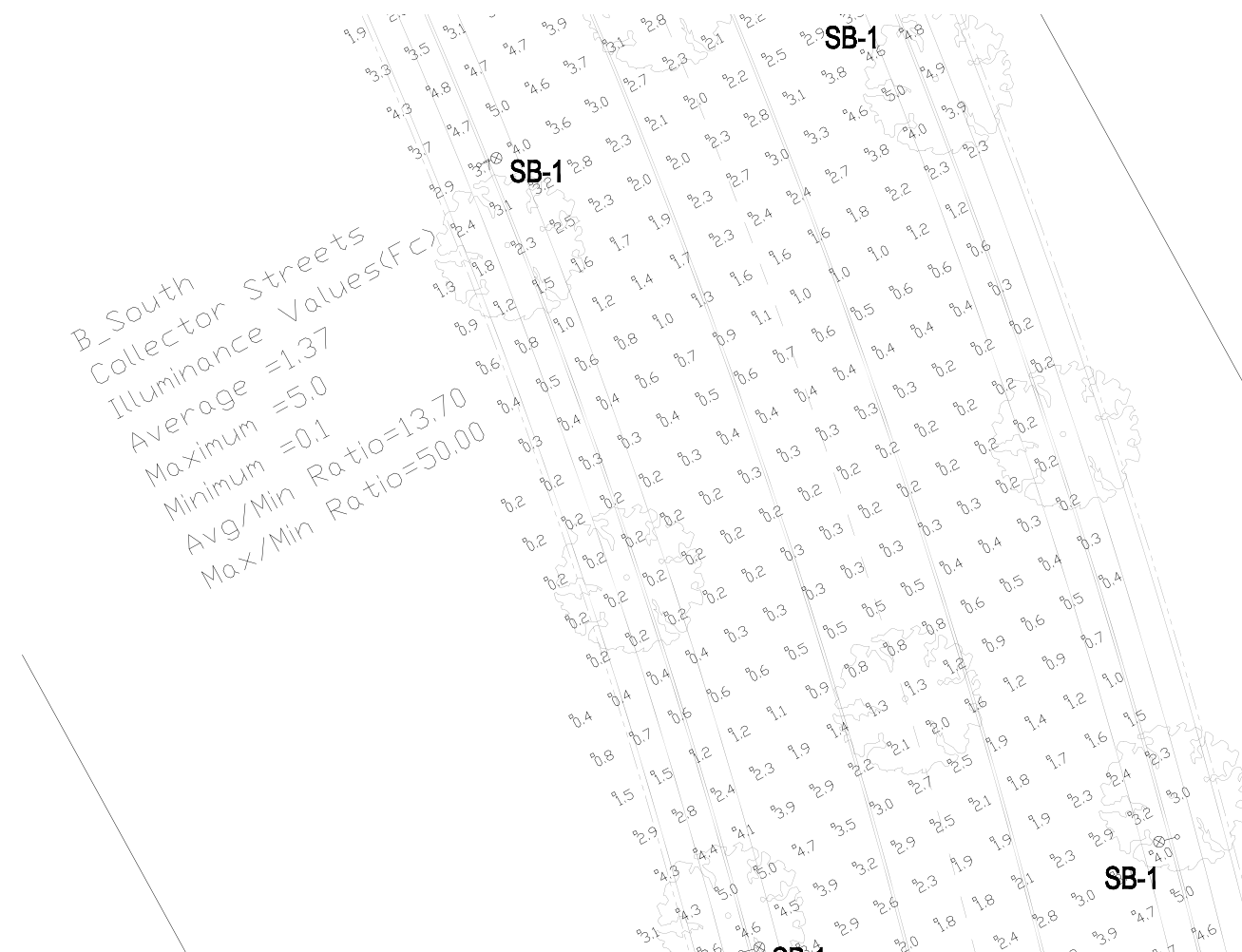
Type SB-1, 30' Single head pole, 240' o.c.

Lamp: 250w. HPS, 120V

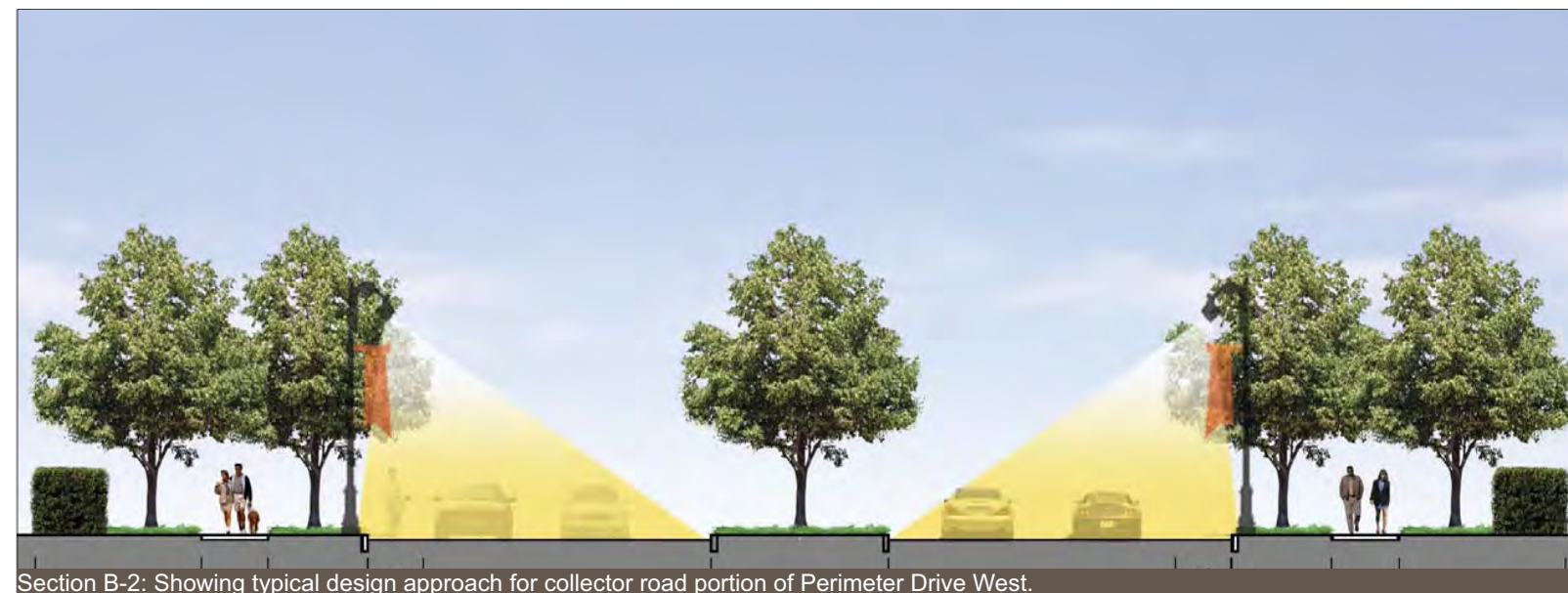
1.0-1.3 fc Average.



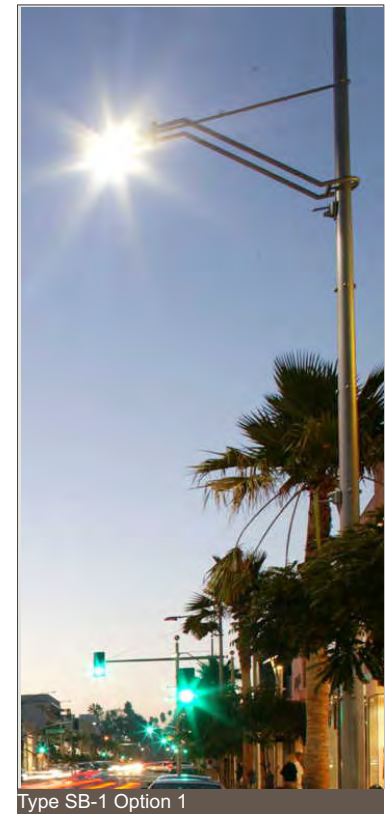
Location map for reference



Plan showing typical fixture locations and light levels for collector road portion of Perimeter Drive West.



Section B-2: Showing typical design approach for collector road portion of Perimeter Drive West.



Type SB-1 Option 1



Type SB-1 - Option 2



# 7.5 Main Streets

### 7.5.1 Fashion Place and East Street

**Light Level Summary**

**C Fashion Place and East Street**

Roadway Cross Section at Promenade Vehicular Street ( 30' wide x 30' section).

Average (fc) values across Promenade Interior (4.15 fc).


Canopy Tree uplights located in median for all trees and extend to include all canopy trees at intersections. Contribution from supplemental landscape lighting fixture, will represent an additional 15-20 % of the total in this area.

**Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.**

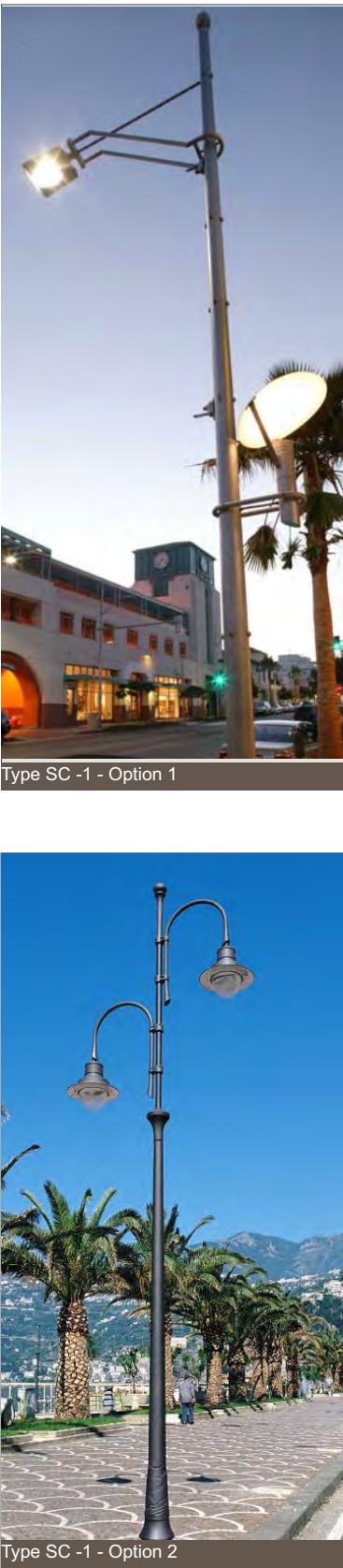
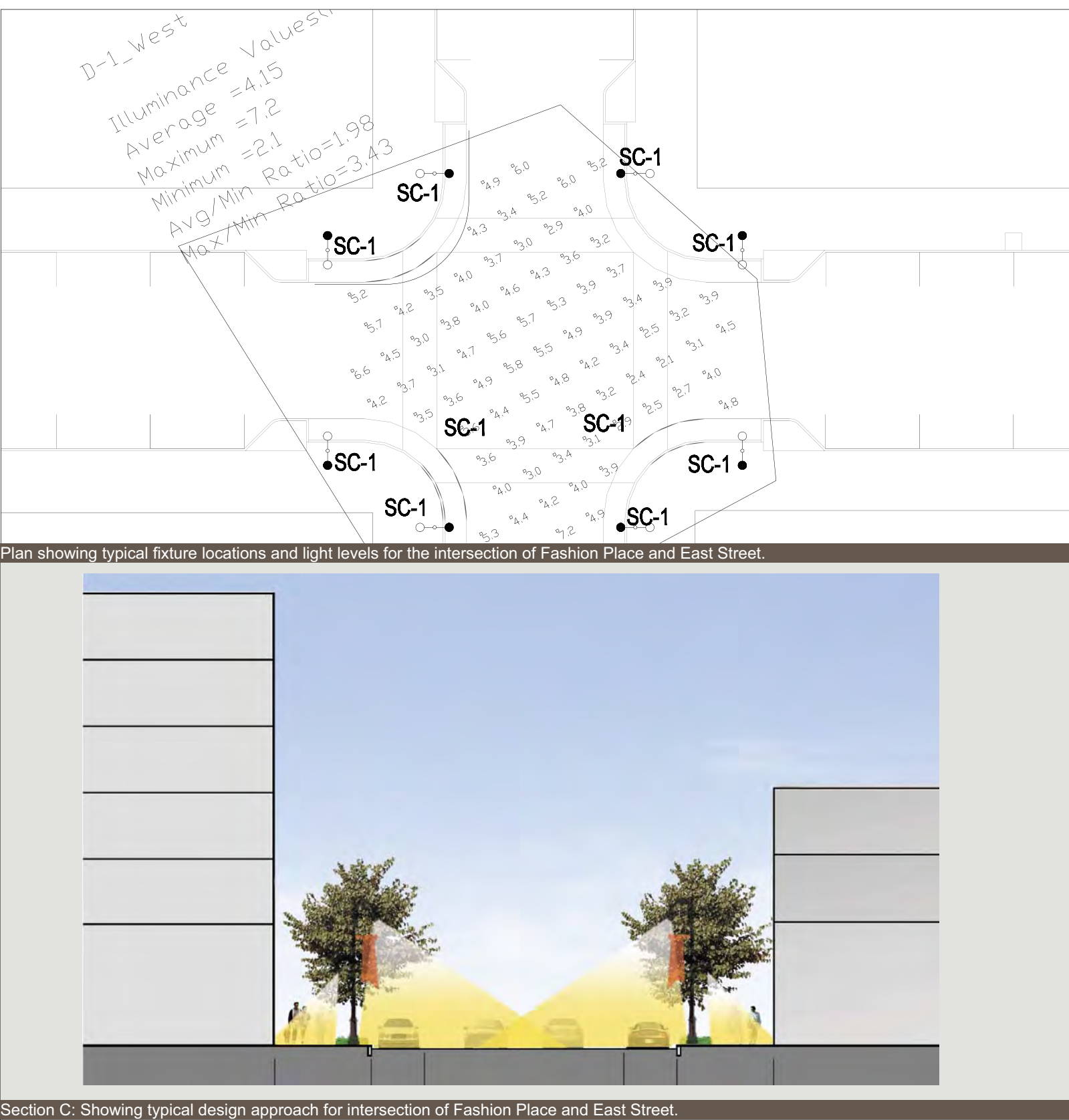
**The light levels are based on the recommended IES minimum.**

**Reference Design Criteria:**

Type SC-1 22'/12' Double Head Pole, 160' o.c.  
Lamp: 100w Mastercolor Metal Halide  
2.0-4.0 fc Average.



Location map for reference





# 7.5 Main Streets

## 7.5.2 North Main & Pedestrian Promenade

### Light Level Summary

#### C-1 North Main & Pedestrian Promenade

Roadway Cross Section at Promenade Vehicular Street ( 75' wide x 300' section).

Average (fc) values across Promenade Interior (1.82 fc).

Canopy Tree uplights located in median for all trees and extend to include all canopy trees at intersections. Contribution from supplemental landscape lighting fixture, will represent an additional 15-20 % of the total in this area.

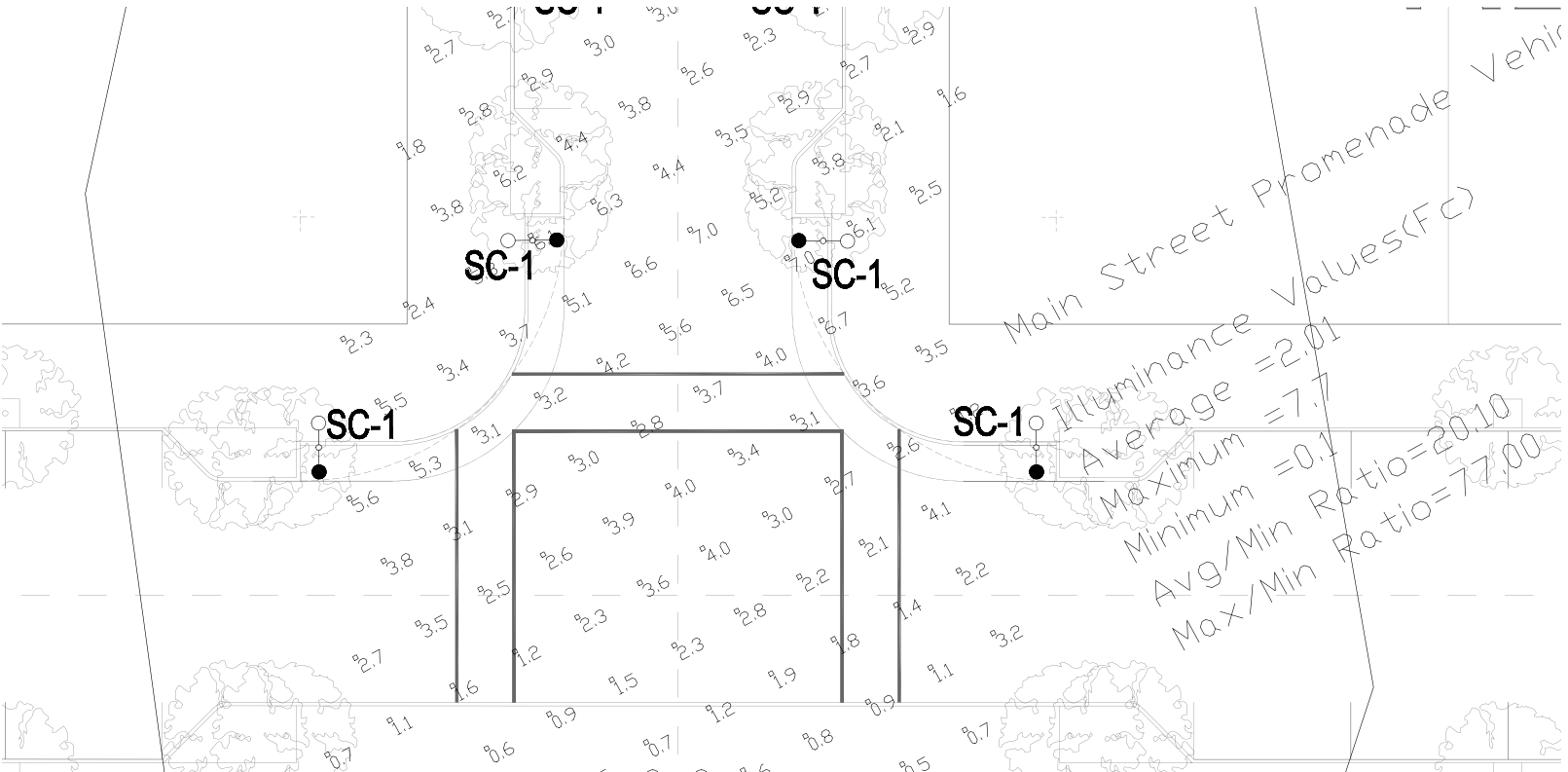
Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

The light levels are based on the recommended IES minimum.

### Reference Design Criteria:

Type SC-1, 12'/22' Double Head Pole at 160' o.c., Selux Hi/Low Ritorno.

Lamp: 100W/250W(120V) Mastercolor Metal Halide  
2.0-4.0 fc Average.



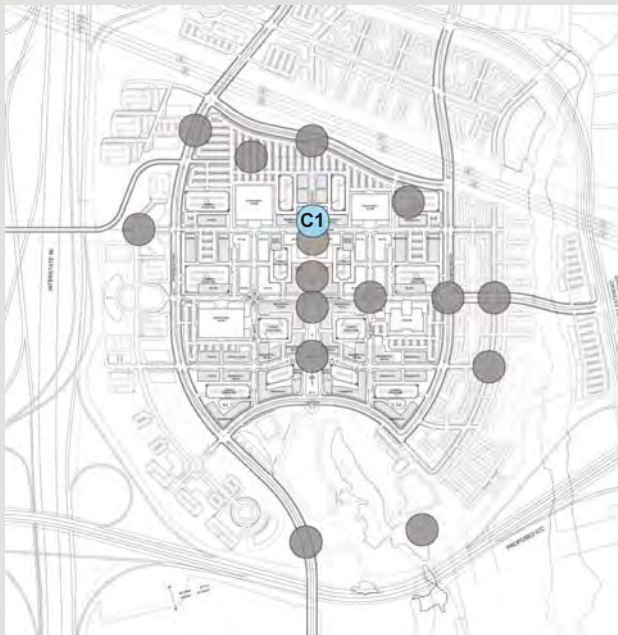
Plan showing typical fixture locations and light levels for intersection of North Main and the Pedestrian Promenade.



Type SC-1 - Option 1



Type SC-1 - Option 1



Location map for reference



Section C-1: Showing typical design approach for intersection of North Main and the Pedestrian Promenade.



# 7.5 Main Streets

## 7.5.3 South Main & Center Street Couplet Residential Area

### Light Level Summary

**C-2 South Main & Center Street Couplet - Residential**

Roadway Cross Section at Main Street Fashion ( 70' wide x 550' section).

Average (fc) values across Main Street (1.32 fc).

Canopy Tree uplights located in median for all trees and extend to include all canopy trees at intersections. Contribution from supplemental landscape lighting fixture, will represent an additional 15-20 % of the total in this area.

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

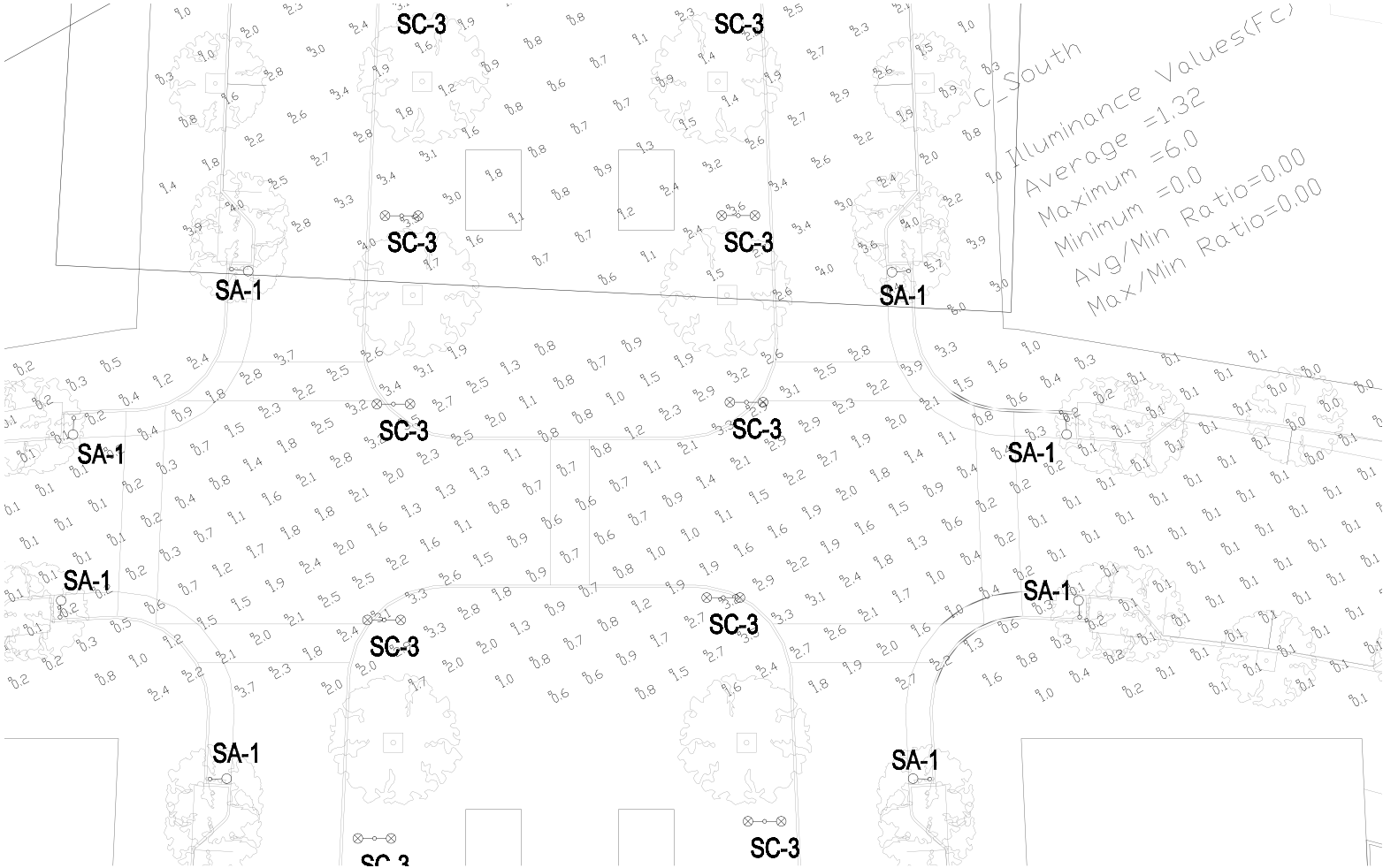
The light levels are based on the recommended IES minimum.

### Reference Design Criteria:

- Type SF Double Head - custom pole
- Type SA-1, 22' Single Head Pole at 160'o.c.
- Lamp: 250W/120V. HPS
- 1.0-1.3 fc Average.



Location map for reference



Plan showing typical fixture locations and light levels for the intersection of South Main and Center Street couplet.



Section C-2: Showing typical design approach for intersection of South Main and Center Street Couplet.



Type SA-1 Option 1



Type SA-1 Option 2



# 7.6 Promenade / Town Square

## 7.6.1 Promenade & Fashion Place Vehicular

### Light Level Summary

#### **D** Promenade & Town Square - Vehicular

Roadway Cross Section at Promenade Vehicular Street ( 168' wide x 300' section).

Average (fc) values across Promenade Interior (2.22 fc).

Canopy Tree uplights located in median for all trees and extend to include all canopy trees at intersections. Contribution from supplemental landscape lighting fixture, will represent an additional 15-20 % of the total in this area.

Canopy Trees on retail pedestrian walkways and center median will allow power provision for seasonal lights.

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

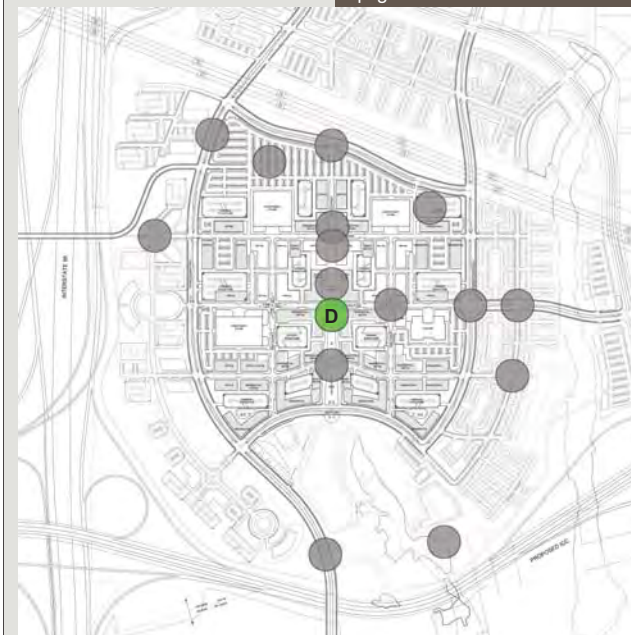
The light levels are based on the recommended IES minimum.

### Reference Design Criteria:

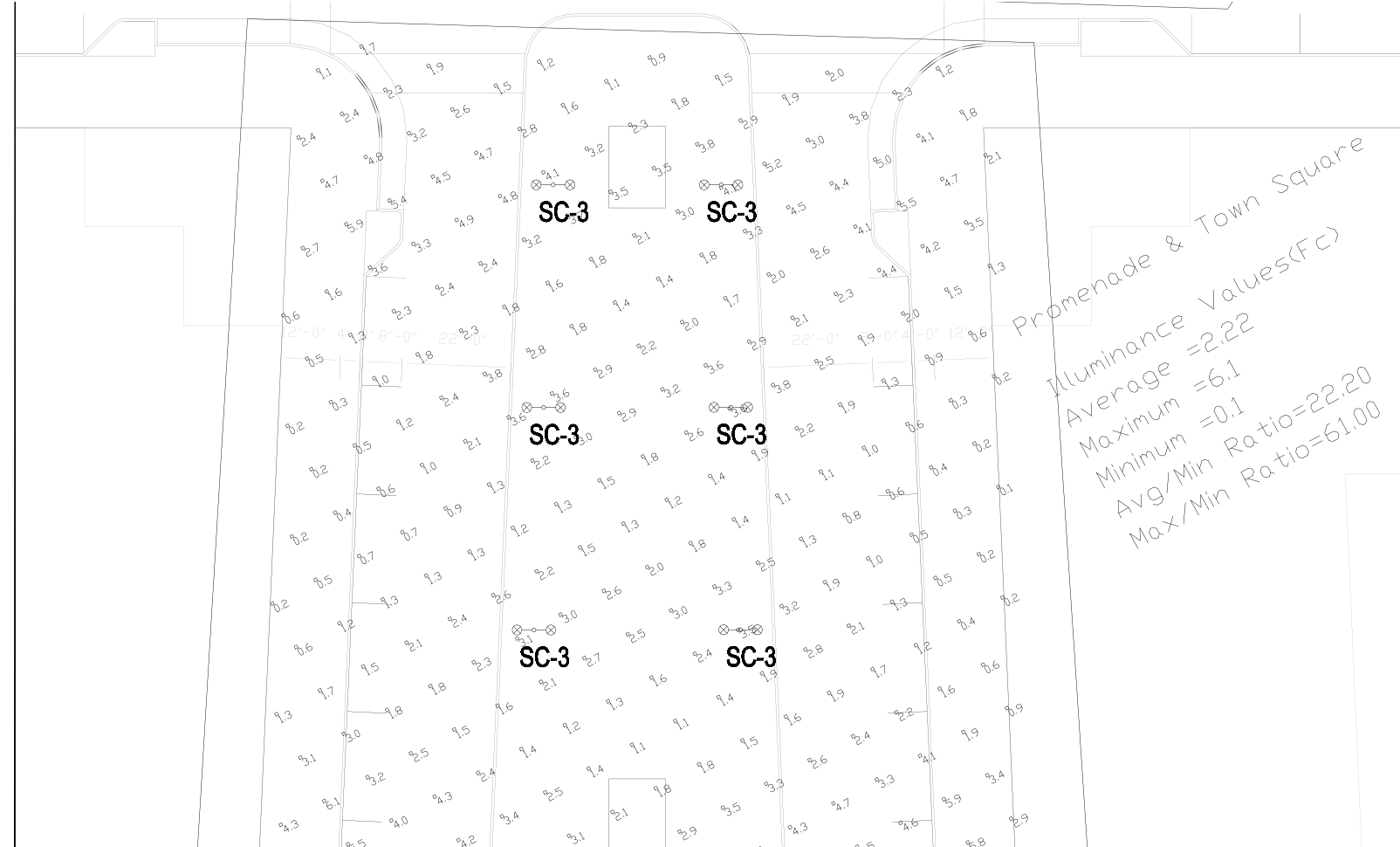
Type SC-3, Double Head, 14' Pole, 45' o.c.  
Lamp: 100W/120V White Son HID



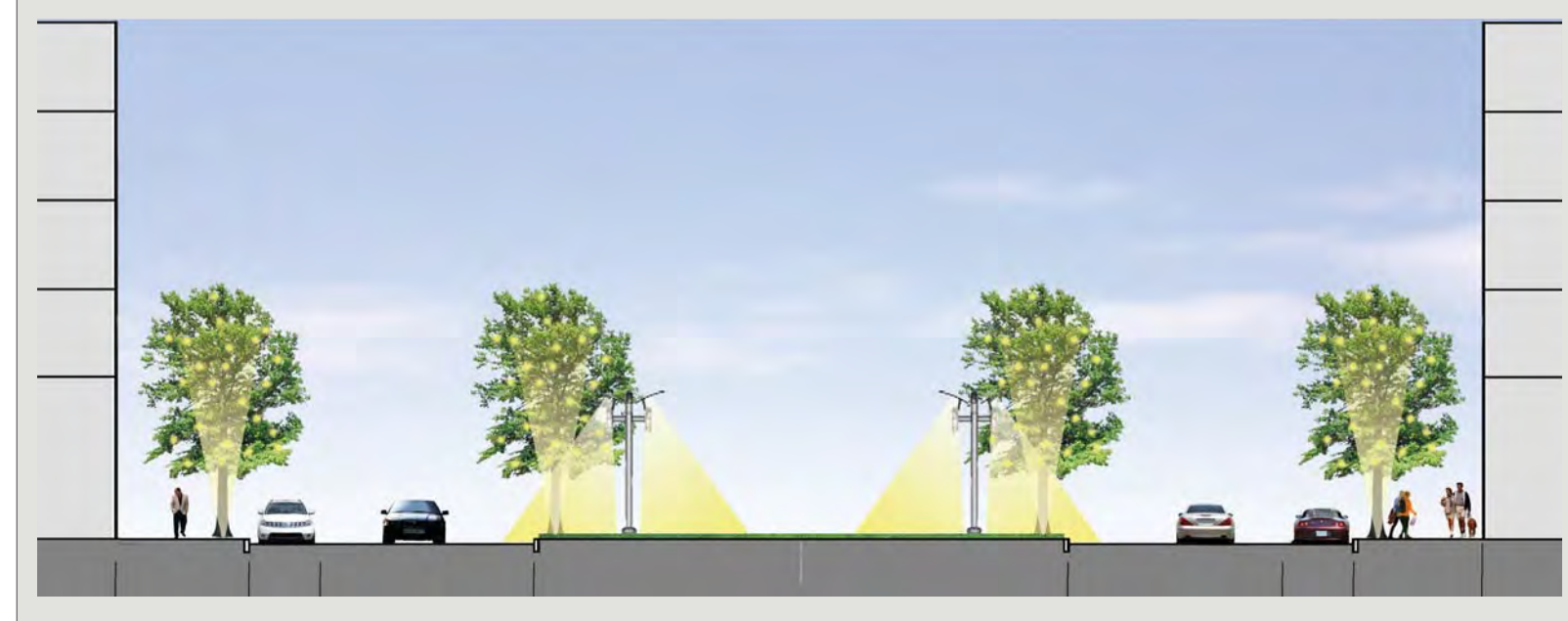
Uplight: Erco Tesis



Location map for reference



Plan showing typical fixture locations and light levels for the Promenade / Center Street Couplet and Fashion Place



Section D1: Showing typical design approach for the Promenade / Center Street Couplet and Fashion Place



Type SC-3 - Option 1



Type SC-3 Option 2



# 7.6 Promenade / Town Square

### 7.6.2 Promenade & Town Square Pedestrian

**Light Level Summary**

D-1

Promenade & Town Square - Pedestrian

Roadway Cross Section at Promenade Vehicular Street (168' wide x 300' section).

Average (fc) values across Promenade Interior (1.13 fc).

Canopy Tree uplights located in median for all trees and extend to include all canopy trees at intersections. Contribution from supplemental landscape lighting fixture, will represent an additional 15-20 % of the total in this area.


Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

The light levels are based on the recommended IES minimum.

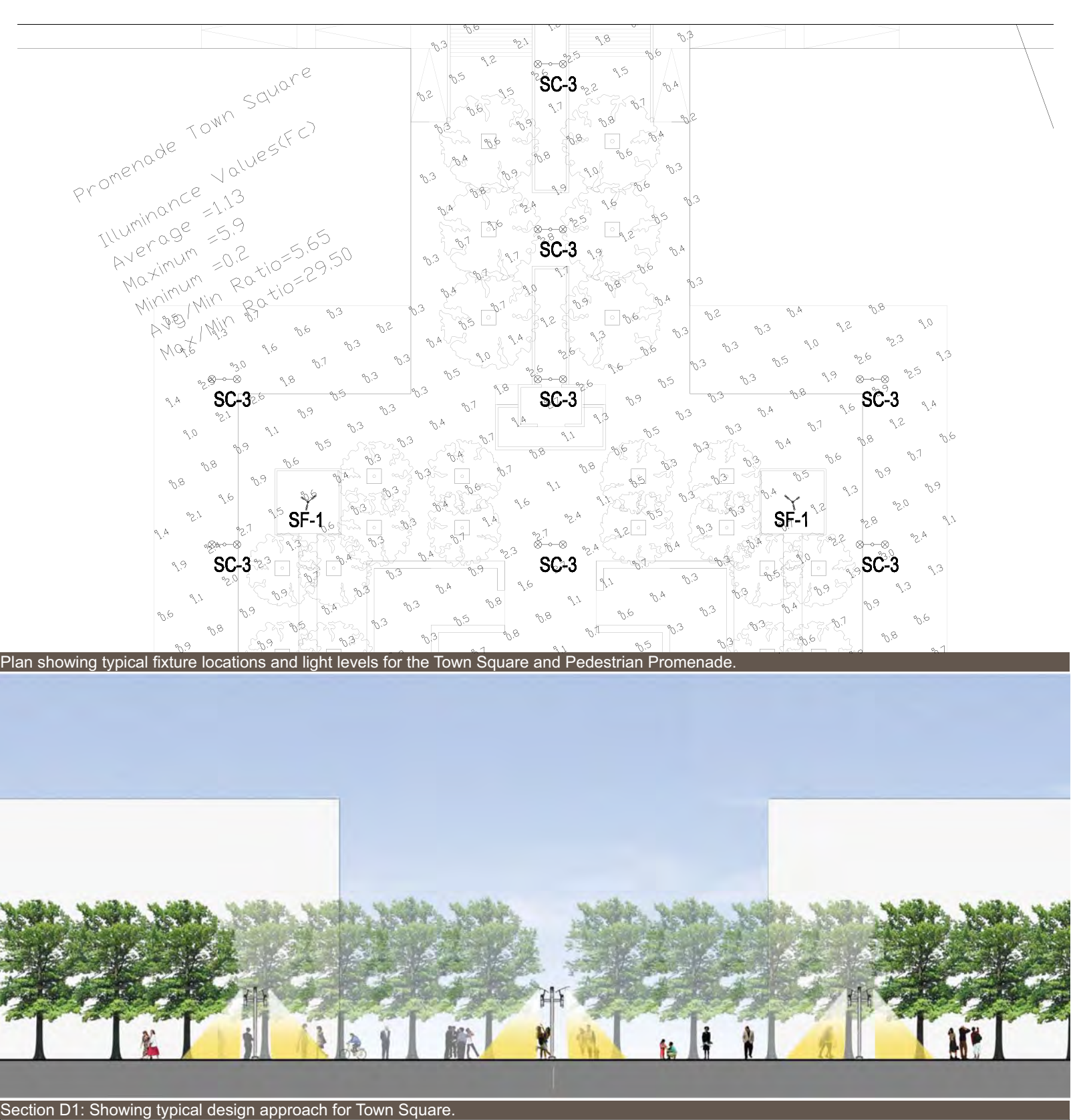
**Reference Design Criteria:**

Type SF-1, Special Event Pole

Type SC-3, Double Head, 14' Pole, 45' o.c.  
Lamp: 100W/120V White Son HID



Location map for reference





# 7.7 Pedestrian Passage

### 7.7.1 Pedestrian Passage

**Light Level Summary**

**E Pedestrian Passage**

Roadway Cross Section at Office and Hotel Street ( 45' wide x 550' section).

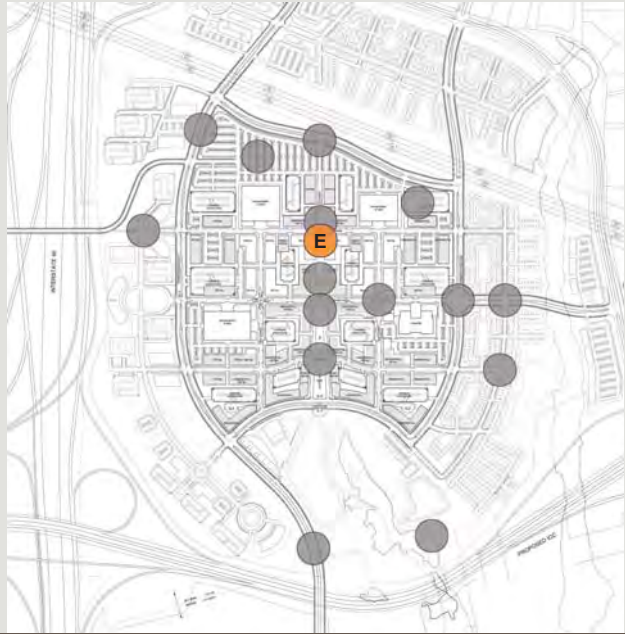
Average (fc) values across street (2.18 fc).

**Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.**

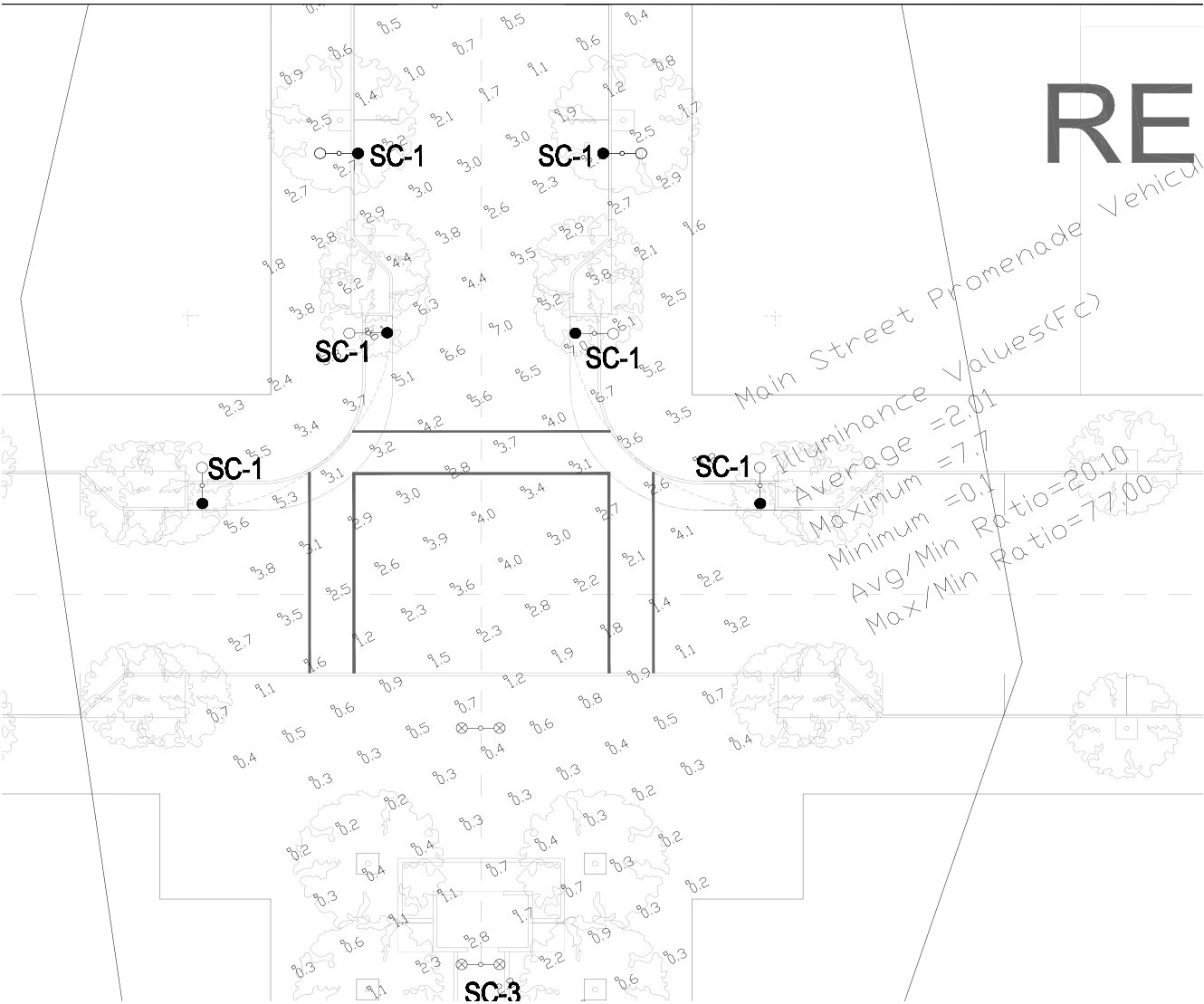
**The light levels are based on the recommended IES minimum.**

**Reference Design Criteria:**

Type SC-1, Double Head 12'/22' Pole, 120' o.c.  
Lamp: 100W/250W/120V



Location map for reference





# 7.8 Parking

## 7.8.1 Parking Structure

### Light Level Summary

**H** Parking Structure

Average (fc) values in Parking Structure (4.05 fc).

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

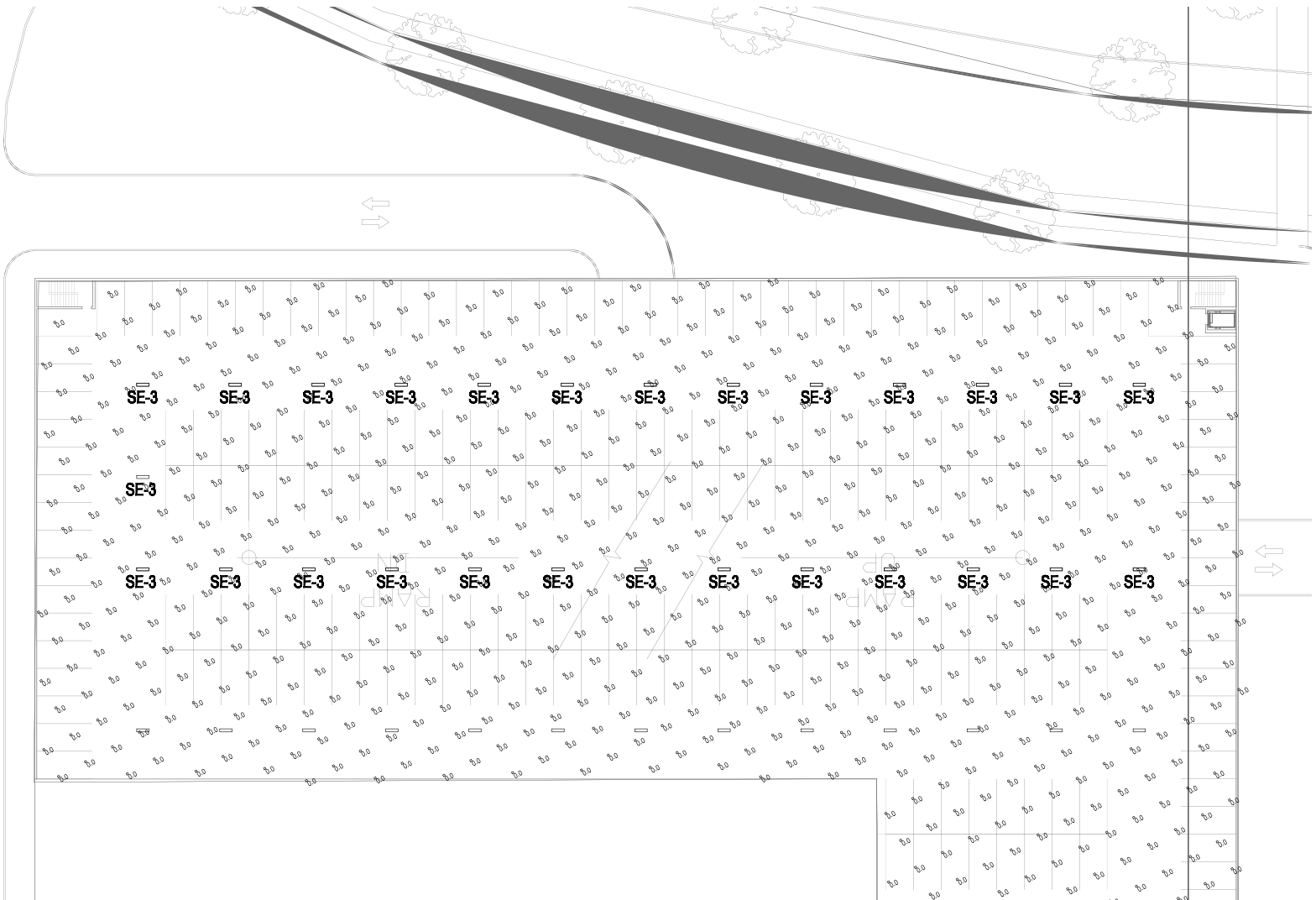
The light levels are based on the recommended IES minimum.

### Reference Design Criteria:

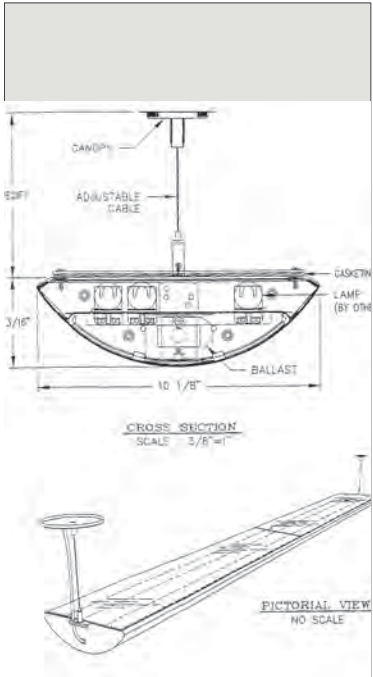
- Type SE-3 Indirect Pedestrian.
- Type SE-4 Direct Surface Mount



Location map for reference



Plan showing typical fixture locations and light levels for parking structures.



Type SE-3



Type SE-4



# 7.8 Parking

## 7.8.2 Parking Lots

### Light Level Summary

**I Surface Parking**

Average (fc) values in Parking Structure (4.27 fc).

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

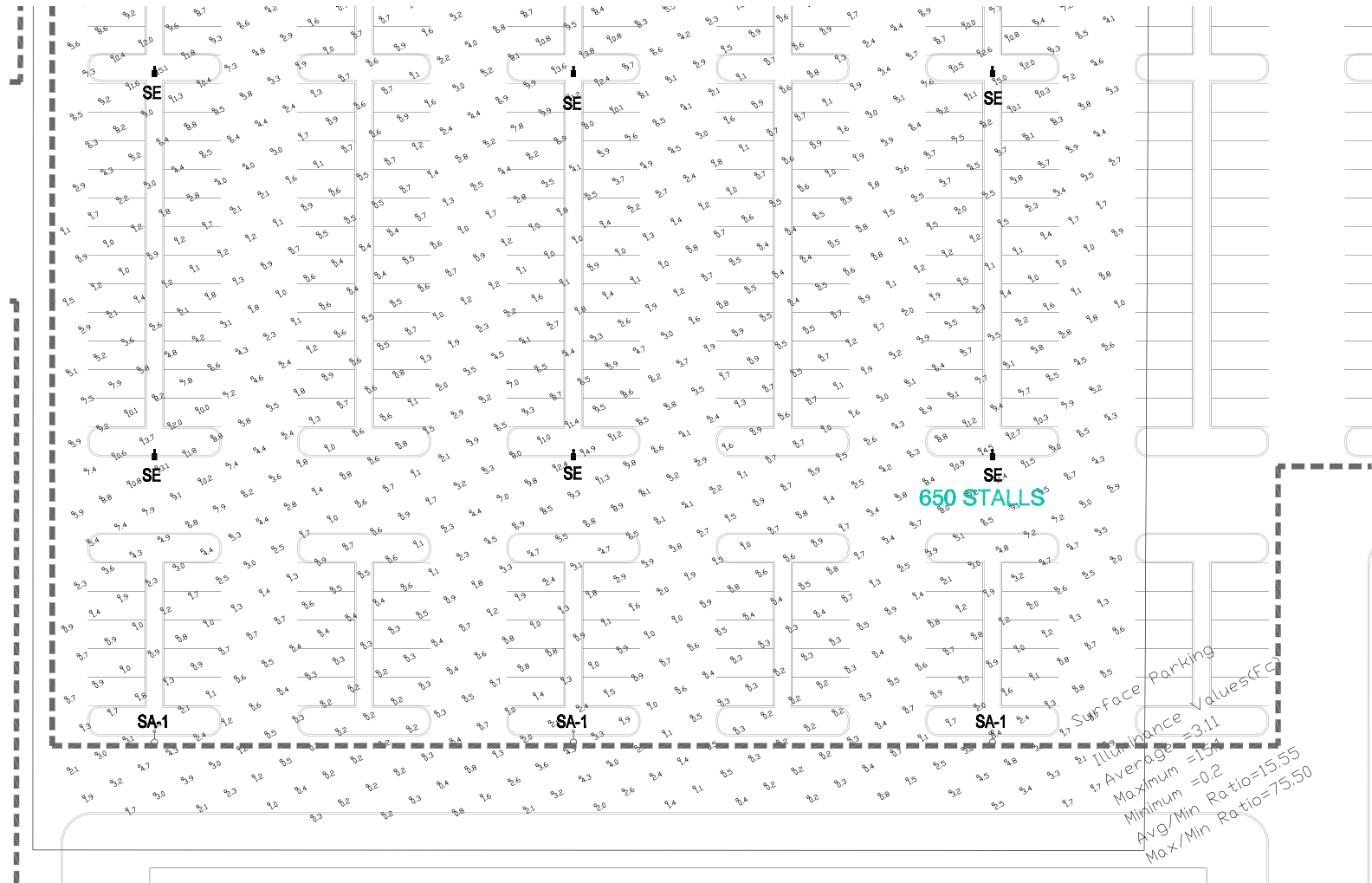
The light levels are based on the recommended IES minimum.

### Reference Design Criteria:

Type SE, Double Head 20' Pole.  
Lamp: 250W/120V, HPS.



Location map for reference



Plan showing typical fixture locations and light levels for the surface parking lots.



Section Q: Showing typical design approach for surface parking lots.





# 7.9 Environ Areas

## 7.9.1 Office and Hotel Street

### Light Level Summary

#### **F** Office and Hotel Street

Roadway Cross Section at Office and Hotel Street ( 45' wide x 550' section).

Average (fc) values across street (2.65 fc).

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

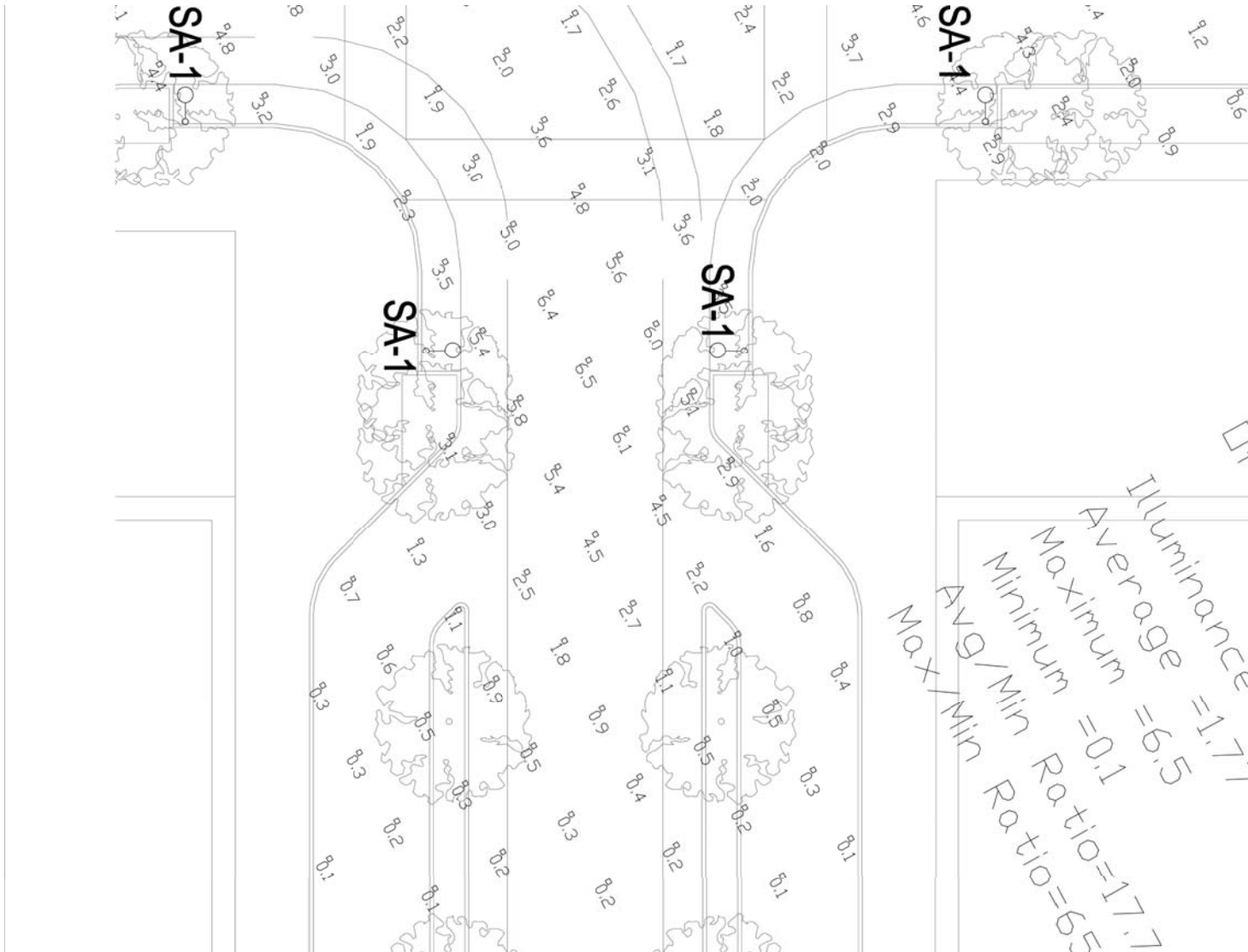
The light levels are based on the recommended IES minimum.

### Reference Design Criteria:

Type SA-1, Single Head 22' Pole, 160 o.c.  
Lamp: 250W/120V



Location map for reference



Plan showing typical fixture locations and light levels for a typical office environ street.



Section F: Showing typical design approach for office environ street.



Type SA -1 - Option 1



Type SA -1 - Option 2



# 7.9 Environ Streets

### 7.9.2 Pedestrian Park Path

#### Light Level Summary

G

Park Pedestrian Path

Roadway Cross Section at Park Pedestrian Path (15' wide x 650' section).

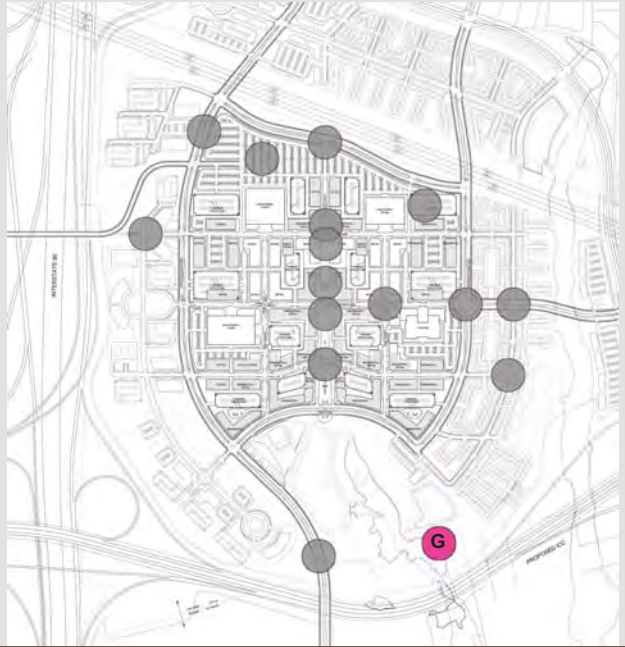
Average (fc) values across the pedestrian path (0.92 fc).

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

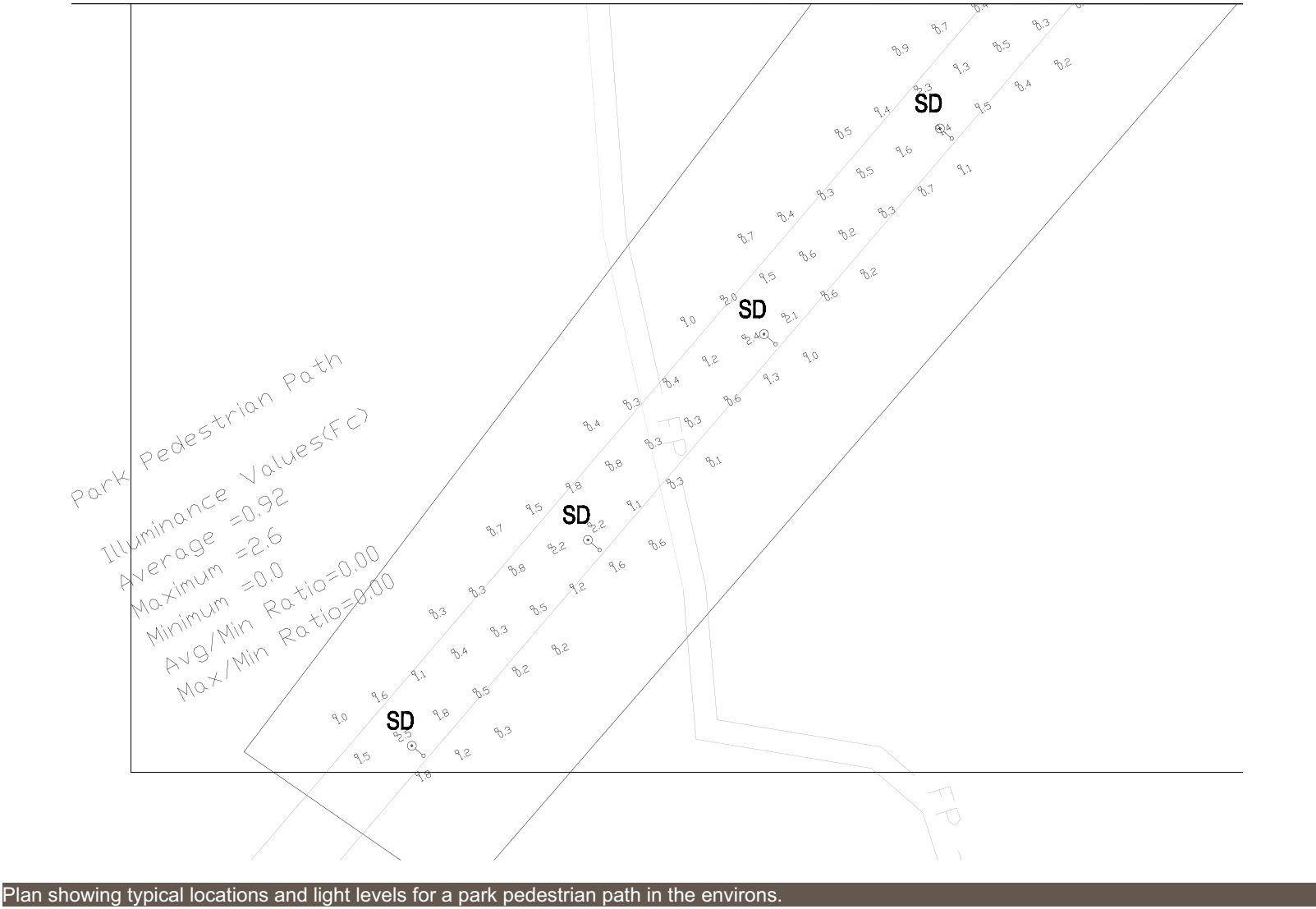
The light levels are based on the recommended IES minimum.

#### Reference Design Criteria:

Type SD-2, 14' Pole, 60' o.c., Poulsen/Selux.



Location map for reference





# 7.9 Environ Streets

### 7.9.3 Residential Street Intersection

**Light Level Summary**

**J Residential Street Intersection**

Roadway Cross Section at North Collector Street ( 98' wide x 300' section).

Average (fc) values across Across Collector Street intersection (1.91 f.c)

Uniformity ratio of 4:1 based upon the Standard for Prince George's County, Maryland.

The light levels are based on the recommended IES minimum.

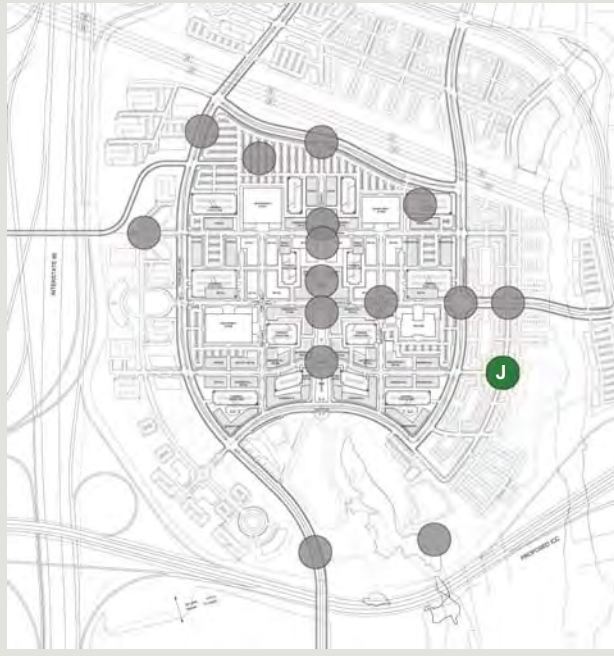
**Reference Design Criteria:**

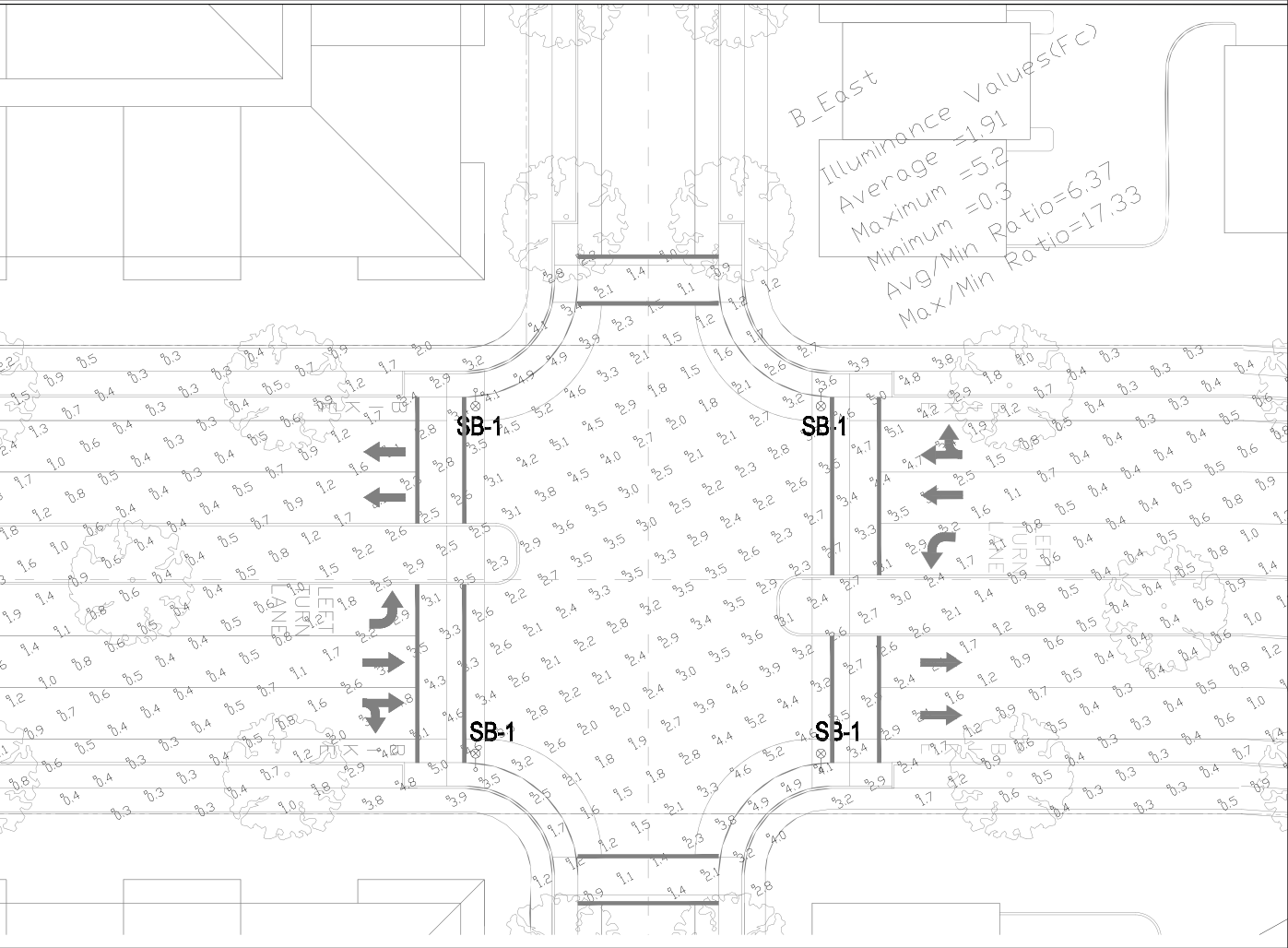
Type SB-1 (30' Single head pole, 4x @ intersection.)

Lamp: 250w. HPS, 120V

1.0-1.3 fc Average / Street

1.8 fc Average / Intersection

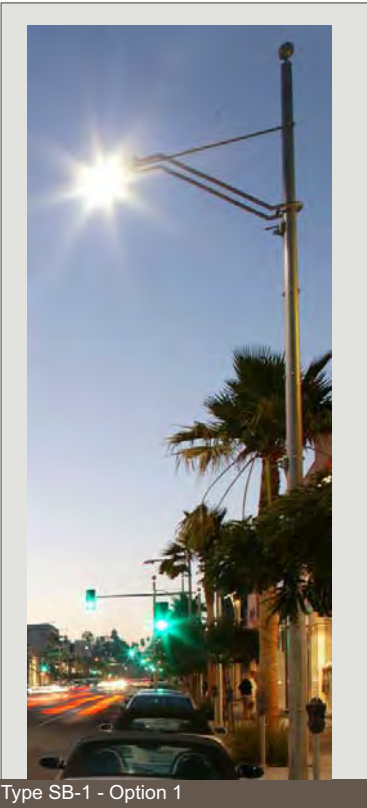




Plan showing typical fixture locations and light levels for a typical residential environ street.



Section J: Showing typical design approach for residential environ street.





# 7.10 Additional Fixture Options



OPTION 3  
Type: SA-1 / Single Head 22' Pole  
Lamp: 250W / HPS



OPTION 3  
Type: SB / Double Head 30' Pole  
Lamp: 250W HPS / 100W Metal Halide



OPTION 3  
Type: SB-1 / Single Head 30' Pole  
Lamp: 250W HPS



OPTION 3  
Type: SC-1 / Double Head 22' Pole  
Lamp: 100W Metal Halide



OPTION 3  
Type: SC-3 / Double Head 14' Pole  
Lamp: 100W White Son HID



OPTION 1  
Type: SF / SF-1 / Special Event Pole  
Lamp:



OPTION 4  
Type: SA-1 / Single Head 22' Pole  
Lamp: 250W / HPS



OPTION 4  
Type: SB / Double Head 30' Pole  
Lamp: 250W HPS / 100W Metal Halide



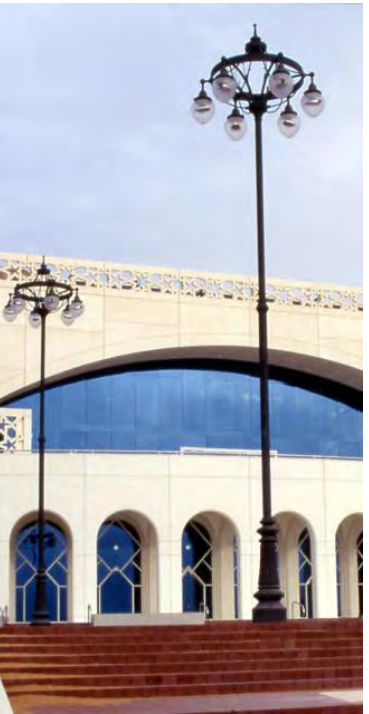
OPTION 4  
Type: SB-1 / Single Head 30' Pole  
Lamp: 250W HPS



OPTION 4  
Type: SC-1 / Double Head 22' Pole  
Lamp: 100W Metal Halide



OPTION 4  
Type: SC-3 / Double Head 14' Pole  
Lamp: 100W White Son HID



OPTION 4  
Type: SF / SF-1 / Special Event Pole  
Lamp:



# Konterra Town Center East - Design Standards

Approval Sheet:

M-NCPPC APPROVALS			
PROJECT NAME: <i>Konterra Town Center East</i>			
PROJECT NUMBER: <i>DSP-08011</i>			
For Conditions of Approval see Site Plan Cover Sheet or Approval Sheet Revisions Listed Below Apply to This Sheet			
Approval or Revision #	Approval Date	Reviewer's Initials	Certification Date
	<i>7-21-09</i>	<i>H2</i>	<i>7-6-10</i>

RECEIVED  
APR 26 2010  
RECEIVED  
DEVELOPMENT REVIEW  
DIVISION



# Konterra Town Center East - Design Standards

## Conditions listed in the July 29, 2009 NOTICE OF FINAL DECISION OF THE DISTRICT COUNCIL

Affirmance of the Planning Board's decision is subject to the following conditions:

1. Prior to certificate approval of this detailed site plan, the applicant shall:

a. Obtain certification of Conceptual Site Plan CSP-07003. Any additional conditions attached to the final approval of CSP-07003 that impact the physical layout of the downtown core area as shown on the subject DSP shall be reflected on the site plans.

b. Obtain signature approval of Preliminary Plan of Subdivision 4-07108; however, the approval of VP-07108 must be stricken from the approval.

c. Revise the design standards in accordance with the recommended revisions in this report, and include the design standards book in the certification package for final certification approval.

d. Expand the existing section on The Promenade in the design standards book to include additional design standards that establish design parameters for the architecture surrounding the central park, including, but not limited to, design, materials, color, street furniture, signage, lighting, shadow and wind analysis, detailed planting schedule, water features, if any, and hardscape; or obtain a separate DSP approval for the central park.  
[see page 38 - 55](#) (2.6 Center Street Corridor)  
[see page 108 - 121](#) (5.8 Plaza Landscape)

e. Provide evidence demonstrating that the Health Department approves the applicant's environmental site assessment and testing that is required because of prior sand and gravel mining on this site.

f. Provide evidence from the utility companies that have jurisdiction over the subject site that all utilities in the core area will be provided within the right-of-way of streets.

g. Revise the design standards book as follows:

(1) Pedestrian pathways or connections through permanent surface parking lots, for which the farthest point exceeds four hundred (400) feet, shall be designed in "Allee" style. A typical design detail shall also be provided in Chapter 5, Landscape Standards.  
[see page 58](#) (2.9 Pedestrian Pathways)  
[and page 123](#) (5.10 Street Landscape - Parking Lot)

(2) Provide a note in Chapter 7, Lighting Standards, indicating that pole mounted perimeter street and interior parking area fixtures, excepting light fixtures provided by and subject to BG&E requirements, are to be light fixtures with full cut-off optics. All light fixtures located within service court areas are also to be fixtures with full cut-off optics to the extent practicable. Decorative internal street and plaza poles, decorative building-mounted fixtures, and other fixtures used for aesthetic effect or security, including those that may be submerged in water features, will be light fixtures with cut-off optics, to the extent practicable and consistent with the District Council finding above.  
[see page 150](#) (7.1 Design Intent)

On page 103, the Design Intent for Lighting Architecture shall be restated as follows: Light will enhance, feature and complement the distinct architectural components within each district, street, park and plaza to create landmarks.  
[See page 150](#) (7.1 Design Intent)

(3) Provide an infrastructure chapter to demonstrate how utilities serving the downtown core area will be provided and where the utility easements will be located. All utilities shall be underground. A utility exhibit to scale shall be provided.  
[see page 28 - 29](#) (1.9 Infrastructure)

(4) Provide a section on transit access and transportation-friendly initiatives, which shall include:  
[see page 26 - 27](#) (1.8 Transit Access and Transportation-friendly Initiatives)

(a) Provision of two or more proposed bus stops within one-quarter mile of the project and provide a discussion on transit and TDM measures in the Sustainability Chapter;  
[see page 27](#) (1.8 Transit Access and Transportation-friendly Initiatives)

(b) Implementation of infrastructure within appropriate areas of the site for multiple support programs promoting shared vehicle usage such as car pool drop off areas, designated parking for vanpools or car share services, ride boards, and shuttle service to mass transit;  
[see page 26](#) (1.8 Transit Access and Transportation-friendly Initiatives)

(c) Options for preferred parking for car share programs;  
[see page 26](#) (1.8 Transit Access and Transportation-friendly Initiatives)

(d) Shared parking;  
[see page 26](#) (1.8 Transit Access and Transportation-friendly Initiatives)

(e) A discussion of how the Applicant will implement the statements on page 22 with respect to the provision of bike lockers and showers for bike users; and  
[see page 26](#) (1.8 Transit Access and Transportation-friendly Initiatives)

(f) Provision for secure bicycle racks in accordance with Leadership in Energy and Environmental Design (LEED) standards, to the extent possible or provide sufficient information on a more appropriate requirement for mixed-use developments. Usage of the bicycle racks shall be monitored throughout the course of development and the number or percentage may be increased or decreased as utility dictates. Modifications of the numbers and/or locations of bicycle racks should be documented with each permit application.  
[see page 26](#) (1.8 Transit Access and Transportation-friendly Initiatives)

(5) Revise the standards to state that Project Landmark Signs in excess of 50 feet and Vertical Beacon Signs in excess of 30 feet shall require approval of a separate detailed site plan.  
[see page 134](#) (6.2 Perimeter Project Signs)

(6) Revise the standards to clarify that crosswalks shall have distinctive features such as pavers.  
[see page 130](#) (5.12 Paving)

(7) On Page 4 after the sentence discussing Metro extension, add: Transit connections to the MARC line and/or Metro are important aspects to successful urban development of the Town Center and to reducing vehicle dependency.  
[see page 18](#) (1.1 Context of Konterra Town Center East)

(8) On pages 12 and 14 the reference to "bike lanes" in the Recommendations section shall be changed to "bike demarcation" and the term "where appropriate" in the same references shall be changed to "except where infeasible".  
[see page 32](#) (2.3 Perimeter Roads), [page 34](#) (2.4 Main Streets East/West)  
[and page 36](#) (2.5 Main Streets North/South)

(9) On page 29, Section 3.5.1 shall be restated as follows: Materials appropriate to the desired overall character of the development will be used, and there shall be an emphasis on traditional regional materials such as brick, stone and masonry.  
[see page 64](#) (3.5.1 Exterior Buildings Materials and Finishes)

(10) Provide a new Sustainability Chapter of the Design Standards for the development and include the narrative submitted with the special purpose DSP in the new chapter. The Sustainability Chapter of the Design Standards, as submitted, shall be amended to include narratives of the following issues:

(a) The provision of sustainable roofing systems, inclusive of:

(1) Design technologies that incorporate vegetative material on roofs or portions thereof;  
[see page 24 -25](#) (1.7 Sustainability)

(2) The use of reflective / "white roof" technology; and  
[see page 24 -25](#) (1.7 Sustainability)

(3) The technologies used to collect and convey rain water for irrigation purposes.  
[see page 24 -25](#) (1.7 Sustainability)

The new chapter shall also specify the minimum solar reflectance index (SRI) value for sidewalks and building roofs. The applicant shall seek to incorporate sustainable roofing systems on the roofs or top levels of structures of no less than five percent of the roof area of residential and commercial buildings.

(b) A narrative on the potential use of Photovoltaic and Geo-thermal installations. As this technology evolves, the applicant shall seek opportunities to incorporate this technology into the project where practicable, particularly seeking to utilize County based labor and materials.  
[see page 24 -25](#) (1.7 Sustainability)

(c) A narrative on phasing the implementation of the collection of recyclable materials from commercial and multifamily properties. Collections shall commence not later than six months after opening of the initial phase.  
[see page 24 -25](#) (1.7 Sustainability)

(d) A commitment to using energy efficient consumer appliances where practicable. Residential development shall include the provision of energy efficient consumer appliances as standard or as optional features.  
[see page 24 -25](#) (1.7 Sustainability)

(e) A narrative on transit and TDM measures consistent with the discussion of the 1990 Subregion I Master Plan. The known limitations of the regional and local transportation network make it clear that TDM measures in Konterra Town Center East are critical and need to be addressed at the beginning of the project, and not incorporated in later phases. Similarly, the adequate provision of transit opportunities shall be addressed, including the provision of shuttle busses, coordination with regional providers and key destinations. Konterra Town Center East is intended to be a high density regional center in the Baltimore Washington Corridor that is transit serviceable. To justify the density proposed, the Applicant bears some of the burden of addressing the transit needs and TDM measures to reduce traffic impacts. This may include provision of a broader shuttle bus service or subsidy to regional providers to support the level of service that is warranted.  
[see page 26 - 27](#) (1.8 Transit Access and Transportation-friendly Initiatives)

(11) Provide a note in each chapter indicating that the first permit plans for sites located within the adjoining blocks that front on the main streets shall establish standards for a complementary array of street trees, furniture, and lighting fixtures along each applicable main street. Subsequent permit plans for those main streets shall provide sufficient reference to the approved species of street trees, street furniture, and lighting fixtures for that street on which the project site is fronting.  
[see pages 17, 30, 59, 75, 96, 132, 149](#)